

To: Councillor Maskell (Chair)  
Councillors Brock, Emberson, Gavin,  
Hopper, McEwan, Page, Robinson,  
Rowland, Singh, Vickers, J Williams & R  
Williams

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19 June 2018

Your contact is: **Simon Hill - Committee Services**

### NOTICE OF MEETING - PLANNING APPLICATIONS COMMITTEE - 27 JUNE 2018

A meeting of the Planning Applications Committee will be held on **Wednesday 27 June 2018 at 6.30 pm in the Council Chamber**, Reading. The Agenda for the meeting is set out below.

Please note that with regard to the planning applications, the order in which applications are considered will be at the Chair's discretion, and applications on which members of the public have requested to speak are likely to be considered first.

#### AGENDA

	<u>ACTION</u>	<u>WARDS AFFECTED</u>	<u>PAGE NO</u>
1. MINUTES OF THE PLANNING APPLICATIONS COMMITTEE HELD ON 30 MAY 2018		-	1
2. DECLARATIONS OF INTEREST	-	-	-
3. QUESTIONS	-	-	-
4. POTENTIAL SITE VISITS FOR COMMITTEE ITEMS	DECISION	BOROUGHWIDE	10
5. PLANNING APPEALS	INFORMATION	BOROUGHWIDE	13
6. APPLICATIONS FOR PRIOR APPROVAL	INFORMATION	BOROUGHWIDE	17

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## Planning Applications to be determined

Item(s)	Action	Ward(s)	Page
7-11	DECISION	ABBEY	29
12	DECISION	NORCOT	115
13	DECISION	PARK	133
14	DECISION	PEPPARD	141
15	DECISION	REDLANDS	151
16	DECISION	SOUTHCOTE	171
17	DECISION	THAMES	183
18	DECISION	TILEHURST	199
19	DECISION	WHITLEY	227

### WEBCASTING NOTICE

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Members of the public who participate in the meeting will be able to speak at an on-camera or off-camera microphone, according to their preference.

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## BOROUGH WIDE

### SCHEDULE OF PLANNING APPLICATIONS TO BE DETERMINED

#### Planning Applications Committee - 27<sup>th</sup> June 2018

Item:	7
Page No:	29
Ward:	Abbey
Application Number	180410
Application Type	Advertisement Consent
Applicant	Maxx Media Limited
Address	Pedestrian Zone, Station Approach, Reading, RG1 1LY
Proposal	Double sided LED digital smartscreen
Recommendation	Application Permitted
Item:	8
Page No:	44
Ward:	Abbey
Application Number	171808
Application Type	Full Planning Approval
Applicant	Bangladesh Association of Greater Reading (charity number 1039747)
Address	18 Waylen Street, Reading
Proposal	Retrospective planning application for the demolition of a pre-existing extension and the construction of a two storey rear extension, and the demolition of the boundary wall adjacent to the highway
Recommendation	Application Permitted
Item:	9
Page No:	63
Ward:	Abbey
Application Number	172259
Application Type	Full Planning Approval
Applicant	Vertu Motors PLC
Address	Mercedes Centre, Richfield Avenue, Reading, RG1 8EQ
Proposal	The development will see the extension of the existing Mercedes-Benz showroom on Richfield Avenue with two smaller buildings erected purely for valet and MOT services. The works to the building will see new cladding and glazing to the showroom and site facing workshop areas, re allocation of parking, resurfacing of the site, new boundary treatments, a dedicated covered service drop off area installed, new signage and an overhaul of the interiors to provide contemporary facilities for both customers and employees.
Recommendation	Permitted subject to Legal Agreement
Item:	10
Page No:	82
Ward:	Abbey
Application Number	180739
Application Type	Non Material Amendment
Applicant	Fynecast Ltd
Address	114 Oxford Road, Reading
Proposal	Non-material amendments to permission 150721 [Erection of part 4, part 5 storey building providing 16 (2x1, 13x2 & 1x3-bed) residential units (Class C3) with associated parking and landscaping, following demolition of existing buildings (Class A1 / B8 / nil use) (amended description)], namely to raise the height of the building by 0.5m, to omit the mezzanine floor to flats 13 and 14 and change the configuration of the front entrance door to the flats.
Recommendation	Agree

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## BOROUGH WIDE

### SCHEDULE OF PLANNING APPLICATIONS TO BE DETERMINED

#### Planning Applications Committee - 27<sup>th</sup> June 2018

Item:	11
Page No:	99
Ward:	Abbey
Application Number	172264
Application Type	Variation of Condition
Applicant	Hayward Nevitt
Address	County Arms Ph, Watlington Street, Reading, RG1 4RT
Proposal	Extension and conversion of existing building to create 1 x studio, 6 x 1-bed flats, 3 x 2-bed flats and associated works including parking, amenity space and landscaping without complying with conditions 2 (approved plans), 3 (materials), 4(landscape), 6 (landscape maintenance), 7 (boundary treatment), 8 (Code for Sustainable Homes) and 11 (construction method statement)of planning permission ref. 141416/VARIAT to increase the number of units from 9 to 10 (as secured previously under planning permission ref. 11/00110), alterations to fenestration, relocation of cycle store to car park and alterations to car parking and landscaping layout.
Recommendation	Permitted subject to Legal Agreement
Item:	12
Page No:	115
Ward:	Norcot
Application Number	161507
Application Type	Outline Planning Approval
Applicant	Mrs K Fielden
Address	2-6 Water Road and 158 Dee Road
Proposal	Demolition of 4 existing dwelling houses 2,4,6 Water Road and 158 Dee Road and erection of 11 new four bedroom residential dwellings and car parking.
Recommendation	Permitted subject to Legal Agreement
Item:	13
Page No:	133
Ward:	Park
Application Number	180786
Application Type	Full Telecommunications Approval
Applicant	H3G UK Ltd
Address	Fire Station, Wokingham Road, Reading, RG6 1JU
Proposal	Telecommunications application for replacement of 1 no. existing flagpole antenna to top of fire training tower with 1 no. new tri- sector antenna. Installation of 1 no. equipment cabinet within the existing cabin, plus associated ancillary development.
Recommendation	Application Permitted
Item:	14
Page No:	141
Ward:	Peppard
Application Number	180720
Application Type	Regulation 3 Planning Approval
Applicant	Reading Borough Council Property Services
Address	11 Knights Way, Emmer Green, Reading, RG4 8RJ
Proposal	Single storey rear extension
Recommendation	Application Permitted



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## BOROUGH WIDE

### SCHEDULE OF PLANNING APPLICATIONS TO BE DETERMINED

#### Planning Applications Committee - 27<sup>th</sup> June 2018

Item:	15
Page No:	151
Ward:	Redlands
Application Number	180144
Application Type	Full Planning Approval
Applicant	Mr Paul Kilshaw
Address	25 Redlands Road, Reading, RG1 5HX
Proposal	Demolition of a single-storey rear projection, followed by the construction of a single-storey rear extension, internal modifications and refurbishment to facilitate change of use from a single dwelling house with detached garage (C3a) to 5no. self- contained flats (C3a) with associated car parking, bin and cycle storage.
Recommendation	Permitted subject to Legal Agreement
Item:	16
Page No:	171
Ward:	Southcote
Application Number	180704
Application Type	Full Planning Approval
Applicant	Ms Lorna Tee
Address	1 Kenilworth Avenue, Reading, RG30 3DL
Proposal	Erection of 1no. four bedroom detached dwelling
Recommendation	Application Refused
Item:	17
Page No:	183
Ward:	Thames
Application Number	180556
Application Type	Full Planning Approval
Applicant	Education and Skills Funding Agency
Address	Caversham Primary School, Hemdean Road, Caversham, Reading, RG4 7RA
Proposal	Demolition of existing single storey extension to school building and replacement with new single storey extension
Recommendation	Application Permitted
Item:	18
Page No:	199
Ward:	Tilehurst
Application Number	180171
Application Type	Regulation 3 Planning Approval
Applicant	Reading Borough Council
Address	Moorlands Primary School, Church End Lane, Tilehurst, Reading, RG30 4UN
Proposal	School expansion from a two form of entry (420 pupils) to a three form of entry (630 pupils) to include two, two-storey double modular units (with new cladding), one single storey modular building (with new cladding) and two single storey extensions, demolition of single temporary classroom, retention of 2 double modular units, external landscaping works and increase in car parking numbers including off- site parking on adjacent Recreation Ground.
Recommendation	Permitted subject to Legal Agreement

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## BOROUGH WIDE

### SCHEDULE OF PLANNING APPLICATIONS TO BE DETERMINED

#### Planning Applications Committee - 27<sup>th</sup> June 2018

Item:	19
Page No:	227
Ward:	Whitley
Application Number	180691
Application Type	Full Planning Approval
Applicant	St. Edward Homes Limited
Address	Green Park Village, Longwater Avenue
Proposal	A planning application for a 2 Form Entry Primary School, associated playing space, car parking, pedestrian and cycle routes, services & infrastructure, landscaping and other associated works.
Recommendation	Permitted subject to Legal Agreement

## KEY TO CODING OF PLANNING APPLICATIONS

1. Planning application reference numbers are made up of 2 parts.
  - 1.1 The number begins with the year e.g. 15
  - 1.2 This is followed by a consecutive number, showing what number the application is in any year (e.g. 150128).
  
2. The following is a key to existing officers with their direct dial telephone numbers.

GF1	-	Giorgio Framalico	9372604
KAR	-	Kiaran Roughan	9374530
LEB	-	Lynette Baker	9372413
JW6	-	Julie Williams	9372461
RJE	-	Richard Eatough	9373338
JPM	-	Johnathan Markwell	9372458
SDV	-	Steve Vigar	9372980
CR2	-	Claire Ringwood	9374545
CJB	-	Christopher Beard	9372430
SGH	-	Stephen Hammond	9374424
MDW	-	Mark Worringham	9373337
AJA	-	Alison Amoah	9372286
SEH	-	Sarah Hanson	9372440
BXP	-	Boja Petkovic	9372352
MJB	-	Mathew Burns	9373625
HB3	-	Heather Banks	9374175
EH1	-	Ethne Humphreys	9374085
SKB	-	Sarah Burr	9374227
TRH	-	Tom Hughes	9374150
SFB	-	Susanna Bedford	9372023
NW2	-	Nathalie Weekes	9374237
TF1	-	Tom French	9374068

**GUIDE TO USE CLASSES ORDER**  
and Permitted Changes of Use (England)

Use Classes (Amendment) Order 2005	Use Classes Order 1972	Description	General Permitted Development (Amendment) Order 2005
<b>A1</b> Shops	Class I	<ul style="list-style-type: none"> <li>Shops, retail warehouses, hairdressers, undertakers, travel and ticket agencies, post offices, dry cleaners, internet cafes, etc.</li> <li>Pet shops, cat-meat shops, tripe shops, sandwich bars</li> <li>Showrooms, domestic hire shops, funeral directors</li> </ul>	No permitted changes
<b>A2</b> Financial and Professional Services	Class II	<ul style="list-style-type: none"> <li>Banks, building societies, estate and employment agencies</li> <li>Professional and financial services, betting offices</li> </ul>	Permitted change to A1 <i>where a ground floor display window exists</i>
<b>A3</b> Restaurants and Cafes		Restaurants, snack bars, cafes	Permitted change to A1 or A2
<b>A4</b> Drinking Establishments		Pubs and bars	Permitted change to A1, A2 or A3
<b>A5</b> Hot Food Take-Aways		Take-Aways	Permitted change to A1, A2 or A3
<b>Sui Generis</b>		Shops selling and/or displaying motor vehicles, retail warehouse clubs, laundrettes, taxi or vehicle hire businesses, amusement centres, petrol filling stations	No permitted change
<b>B1</b> Business	Class II  Class III	(a) Offices, not within A2 (b) Research and development, studios, laboratories, high tech (c) Light industry	Permitted change to B8 <i>where no more than 235m</i>
<b>B2</b> General industry	Class IV-IX	General industry	Permitted change to B1 or B8 <i>B8 limited to no more than 235m</i>
<b>B8</b> Storage or Distribution	Class X	Wholesale warehouse, distribution centres, repositories	Permitted change to B1 <i>where no more than 235m</i>
<b>Sui Generis</b>		Any work registrable under the Alkali, etc. Works Regulation Act, 1906	No permitted change
<b>C1</b> Hotels	Class XI	Hotels, boarding and guest houses	No permitted change
<b>C2</b> Residential Institutions	Class XII Class XIV	<ul style="list-style-type: none"> <li>Residential schools and colleges</li> <li>Hospitals and convalescent/nursing homes</li> </ul>	No permitted change
<b>C2A</b> Secure residential institutions		Prisons, young offenders institutions, detention centres, secure training centres, custody centres, short-term holding centres, secure hospitals, secure local authority accommodation or use as military barracks.	No permitted change
<b>C3</b> Dwelling houses		<ul style="list-style-type: none"> <li>Single occupancy or single households (in the family sense);</li> <li>No more than six residents living as a single household where care is provided;</li> <li>No more than six residents living as a single household where the building is managed by a local housing authority, a registered social landlord, a police authority, a fire authority, or a health service body.</li> </ul>	Permitted to change to C4
<b>C4</b> Houses in multiple occupation		Use of a dwellinghouse by between three and six residents, who do not form a single household (in the family sense) and share basic facilities (toilet, bathroom or kitchen).	Permitted to change to C3
<b>Sui Generis</b>		<ul style="list-style-type: none"> <li>House in multiple occupation with more than six residents</li> <li>Hostel</li> </ul>	No permitted change

<b>D1</b> Non-Residential Institutions	Class XIII Class XV Class XVI	<ul style="list-style-type: none"> <li>• Places of worship, church halls</li> <li>• Clinics, health centres, creches, day nurseries, consulting rooms</li> <li>• Museums, public halls, libraries, art galleries, exhibition halls</li> <li>• Non-residential education and training centres</li> </ul>	No permitted change
<b>D2</b> Assembly and Leisure	Class XVII Class XVIII	<ul style="list-style-type: none"> <li>• Cinemas, music and concert halls</li> <li>• Dance, sports halls, swimming baths, skating rinks, gymnasiums</li> <li>• Other indoor and outdoor sports and leisure uses, bingo halls, casinos</li> </ul>	No permitted change
<b>Sui Generis</b>	Class XVII	Theatres, nightclubs	No permitted change



Present: Councillor Maskell (Chair);

Councillors Brock, Emberson, Gavin, Hopper, McEwan, Page, Rowland, Singh, J Williams and R Williams.

Apologies: Councillors Robinson and Vickers.

## RESOLVED ITEMS

### 1. MINUTES

The Minutes of the meeting held on 25 April 2018 were agreed as a correct record and signed by the Chair.

### 2. SITE VISITS

The Director of Environment and Neighbourhood Services submitted, at the meeting, a schedule of applications to be considered at future meetings of the Committee to enable Councillors to decide which sites, if any, they wished to visit prior to determining the relevant applications.

Resolved -

- (1) That the under-mentioned application, together with any additional applications which the Head of Planning, Development and Regulatory Services might consider appropriate, be the subject of an unaccompanied site visit with briefing note:

#### 180693/FUL - LAND ADJACENT 300 KINGS ROAD

Construction of a part five part three storey building of 14 residential apartments (C3) and associated under croft car parking.

- (2) That the under-mentioned application, together with any additional applications which the Head of Planning, Development and Regulatory Services might consider appropriate, be the subject of an accompanied site visit:

#### 180144/FUL - 25 REDLANDS ROAD

Demolition of a single-storey rear projection, followed by the construction of a single-storey rear extension, internal modifications and refurbishment to facilitate change of use from a single dwelling house with detached garage (C3a) to 5no. self-contained flats (C3a) with associated car parking, bin and cycle storage.

### 3. PLANNING APPEALS

#### (i) New Appeals

The Director of Environment and Neighbourhood Services submitted a schedule giving details of notification received from the Planning Inspectorate regarding two planning appeals, the method of determination for which she had already

expressed a preference in accordance with delegated powers, which was attached as Appendix 1 to the report.

(ii) Appeals Recently Determined

The Director of Environment and Neighbourhood Services submitted details of two decisions that had been made by the Secretary of State, or by an Inspector appointed for the purpose, which were attached as Appendix 2 to the report.

(iii) Reports on Appeal Decisions

There were no reports on appeal decisions.

Resolved -

- (1) That the new appeals, as set out in Appendix 1, be noted;
- (2) That the outcome of the recently determined appeals, as set out in Appendix 2, be noted.

4. APPLICATIONS FOR PRIOR APPROVAL

The Director of Environment and Neighbourhood Services submitted a report giving details in Table 1 of 14 pending prior approval applications, and in Table 2 of six applications for prior approval decided between 13 April and 16 May 2018.

Resolved - That the report be noted.

5. PLANNING APPLICATIONS

The Committee considered reports by the Director of Environment and Neighbourhood Services.

Resolved -

- (1) That, subject to the conditions now approved, permission be granted under planning legislation and, where appropriate, under the Advertisement Regulations, as follows:

180273/FUL - 109B OXFORD ROAD

Amended Description: Change of use from sui generis (betting shop) to A3 restaurant with ancillary A5 takeaway and replacement shopfront (revised elevation details).

An update report was tabled at the meeting that gave further details of the proposed extraction system and the shopfront.

Granted as recommended.

Conditional planning permission and informatives as recommended in the original report, with the following additions:

Additional condition to ensure takeaway use is only ancillary to A3 restaurant use.



The Extraction System details in Condition 4 to be agreed in consultation with Ward Councillors.

Comments and objections received and considered.

Objector Anthony Ihringer, Gulraiz Siddique on behalf of the applicant, and Ward Councillor Karen Rowland attended the meeting and addressed the Committee on this application.

(Councillor Rowland declared a prejudicial interest in this item on the grounds of predetermination, made a statement as Ward Councillor then sat in the public gallery and took no part in the debate or decision. Nature of interest: Councillor Rowland lived in the road next to the application site and had led a campaign against the proposal.)

180204/HOU - 79 HENLEY ROAD, CAVERSHAM

First floor rear extension (resubmission of 171302).

Granted as recommended.

Conditional planning permission and informatives as recommended.

Comments and objections received and considered.

(2) That the following applications be refused for the reasons indicated:

172192/PNN - READING WEST STATION FOOTBRIDGE, OXFORD ROAD

Prior Approval under Part 18 Class A to Schedule 2 of the Town and Country Planning (General Permitted Development) (England) Order 2015 (the GPDO) for reconstruction of the footbridge to provide the necessary clearance for the OLE which is to run underneath the structure.

An update report was tabled at the meeting which gave details of an additional objection received. It also had appended a full copy of a letter from the applicant which had been attached at Appendix 3 to the original report but had had a page missing in error.

Refused for the reasons set out in the original report.

Informatives as recommended in the original report.

Comments and objections received and considered.

Objectors Rex Hora, Alan Overton, Alexander Price and Mily Touzet, and Colin Field and Mike Foss on behalf of the applicant, attended the meeting and addressed the Committee on this application.

171740/FUL - 62-79 ARMADALE COURT

Extension of existing flat block with two additional storeys to accommodate 12 new apartments and provision of lift.

Refused for the reasons set out in the report.

Informatives as recommended.

Comments and objections received and considered.

- (3) That consideration of the following applications be deferred for the reason indicated:

180144/FUL - 25 REDLANDS ROAD

Demolition of a single-storey rear projection, followed by the construction of a single-storey rear extension, internal modifications and refurbishment to facilitate change of use from a single dwelling house with detached garage (C3a) to 5no. self-contained flats (C3a) with associated car parking, bin and cycle storage.

An update report was tabled at the meeting.

Deferred for an accompanied site visit.

(Councillor Rowland declared an interest in this item. Nature of interest: Councillor Rowland had contributed to the comments submitted by the Conservation Area Advisory Committee.)

180171/REG3 - MOORLANDS PRIMARY SCHOOL, CHURCH END LANE

School expansion from a two form of entry (420 pupils) to a three form of entry (630 pupils) to include two, two-storey double modular units (with new cladding), one single storey modular building (with new cladding) and two single storey extensions, demolition of single temporary classroom, retention of 2 double modular units, external landscaping works and increase in car parking numbers including off-site parking on adjacent Recreation Ground.

Deferred for further information on traffic surveys.

- (4) That, subject to the requirements indicated, the Head of Planning, Development and Regulatory Services be authorised to determine the following application under planning legislation:

171814/FUL - COX & WYMAN SITE, CARDIFF ROAD

Demolition of existing site buildings and boundary treatments and erection of 96 no. dwellings including associated surface car parking, public realm and landscaping on land at the former Cox & Wyman building, Cardiff Road.

An update report was tabled at the meeting which stated that the applicant had confirmed their agreement to financial contributions for road closures and off site leisure improvements and set out the amended Heads of Terms for the Section 106 legal agreement.

The issue of planning permission to be dependent on the completion of a Section 106 legal agreement by 25 July 2018 (unless a later date be agreed by the Head of Planning, Development and Regulatory Services), to secure the Heads of Terms set

out in the update report, subject to the following:

The Head of Terms regarding the financial contribution of £30,000 to undertake formal road closures and associated legal costs to be amended so that the contribution could also be used for removal or insertion of width restrictions if required.

In the event of the requirements set out not being met, the Head of Planning, Development and Regulatory Services be authorised to refuse permission.

Conditional planning permission and informatives as recommended in the original report.

Comments and objections received and considered.

- (5) That, pursuant to Regulation 3 of the Town and Country Planning General Regulations 1992, the carrying out of the following developments be authorised, subject to the conditions now specified:

**171108/REG3 - LAND BETWEEN THAMES VALLEY BUSINESS PARK AND NAPIER ROAD**

Construction of a segregated fast-track public transport, pedestrian and cycle bridge and viaduct, comprising concrete bridge structure with a river span of 59.5m and a land span of 316m, supported by concrete columns, steel beams and reinforced soil embankment, together with new footpath links and existing footpath alterations, replacement supermarket car parking provision, junction improvements and landscaping.

An update report was tabled at the meeting, also covering the identical application 171662/ADJ, which addressed the following matters:

- Air quality update
- Update on alternatives considered
- Further effect on trees
- Flood risk update (including stating that the Environment Agency had withdrawn their objections on flood risk, biodiversity and navigation grounds)
- Trees and landscaping update (including a technical note on tree retention, loss and planting from the applicant, which was appended to the update report)
- Additional consultation responses received
- Additional objections received
- An amended red line site boundary plan, to address the Network Rail objection
- List of plans

The recommendation had been amended accordingly, with a number of alterations to conditions proposed. The update report also proposed amendments to the Section 106 heads of terms, to confirm that all Section 106 management controls (landscaping, ecology etc) be carried out for a minimum of ten years, and that the Construction Method Statement be via the Section 106 agreement, not by condition

(currently Condition 15).

Details of further objections received which had not been included in the update report were given at the meeting, along with officer comments.

It was explained at the meeting that the Network Rail objection had been a result of an inadvertent over-run of Network Rail land in the Kennetmouth area, and the objection had now been withdrawn following a slight amendment to the red line site boundary plan.

It was stated at the meeting that, as well as the list of plans to be approved, elevations from the South were also needed and there might need to be further technical plans, so it was recommended that the Head of Planning, Development & Regulatory Services be authorised to receive any additional plans, as necessary.

The issue of planning permission to be dependent on the completion of a Section 106 legal agreement by 27 July 2018 (unless a later date be agreed by the Head of Planning, Development and Regulatory Services), to secure the Heads of Terms set out in the original report, with the amendments set out in the update report.

In the event of the requirements set out not being met, the Head of Planning, Development and Regulatory Services be authorised to refuse permission.

The Head of Planning, Development & Regulatory Services to be authorised to receive any additional plans, as necessary.

Conditional planning permission and informatives as recommended in the original report, with the amendments to conditions set out in the update report.

Comments and objections received and considered.

Objectors John Booth, Tamzin Morphy and John Mullaney, Scott Witchalls and Luke Fay on behalf of the applicant and Ward Councillor Brenda McGonigle attended the meeting and addressed the Committee on this application.

(Councillor Page declared a prejudicial interest in this item on the grounds of predetermination. He made a statement to the Committee, left the room and took no part in the debate or decision. Nature of interest: Councillor Page was the Lead Councillor for Strategic Environment, Planning & Transport and had been closely involved in developing the scheme).

180552/REG3 - THE HEIGHTS PRIMARY SCHOOL, 82 GOSBROOK ROAD, CAVERSHAM

Extension to the existing planning approval ref 151283 until 31st August 2020. Erection of a new build 2 storey, 6 classroom modular unit on part of the St. Anne's School site, to allow the school to expand towards a capacity of 325 pupils on the temporary school site until 31st August 2020. Associated external works. [including the temporary annexation of a portion of the adjacent Westfield Road Recreation Ground for pupils' outdoor play area during school hours] (amended description).

An update report was tabled at the meeting, which addressed the following matters:

- Flooding update
- Traffic and parking
- Update on consultation responses received
- Additional objections received
- Additional information on hours of use of classrooms and on contaminated land
- The existing site location plan which had been missing from the original report
- The previous update report to the Committee from 9 September 2015 on existing planning application 151283

The recommendation had been amended accordingly, with a number of alterations to conditions proposed. The update report also proposed amendments to the Section 106 heads of terms.

Details of two further responses received which had not been included in the update report were given at the meeting, one an objection from the Parks Agency and one a letter of support from parents of children at the school with Special Educational Needs.

The applicant agreed at the meeting to the amended description of the planning application, to remove the temporary annexation of a portion of the adjacent Westfield Road Recreation Ground for pupils' outdoor play area during school hours.

The Committee requested that the applicant discuss the possible location of the pupils' outdoor play area further with Ward Councillors to see if a more acceptable location could be found, and submit a new application for this element of the scheme.

The issue of planning permission, for a reduced version of the planning application without the temporary annexation of a portion of the adjacent Westfield Road Recreation Ground for pupils' outdoor play area during school hours, to be dependent on the completion of a Section 106 legal agreement/unilateral undertaking by 7 June 2018 (unless a later date be agreed by the Head of Planning, Development and Regulatory Services), to secure the Heads of Terms set out in the original report, with the amendments set out in the update report.

In the event of the requirements set out not being met, the Head of Planning, Development and Regulatory Services be authorised to refuse permission.

Conditional planning permission and informatives as recommended in the original report, with the amendments to conditions set out in the update report and any necessary amendments to remove conditions pertaining to the outdoor play area.

Comments and objections received and considered.

Objector Alex Vugler, Karen Edwards, Myles Milner and Nick Walden on behalf of the applicant and Ward Councillor Adele Barnett-Ward attended the meeting and addressed the Committee on this application.

- (6) That the following observations be made to the statutory body in question in respect of the under-mentioned proposal referred to the Council for consultation purposes:

171662/ADJ - LAND BETWEEN THAMES VALLEY BUSINESS PARK AND NAPIER ROAD

Construction of a segregated fast-track public transport, pedestrian and cycle bridge and viaduct, comprising concrete bridge structure with a river span of 59.5m and a land span of 316m, supported by concrete columns, steel beams and reinforced soil embankment, together with new footpath links and existing footpath alterations, replacement supermarket car parking provision, junction improvements and landscaping.

An update report was tabled at the meeting also covering the identical application 171108/REG3 which addressed the following matters:

- Air quality update
- Update on alternatives considered
- Further effect on trees
- Flood risk update (including stating that the Environment Agency had withdrawn their objections on flood risk, biodiversity and navigation grounds)
- Trees and landscaping update (including a technical note on tree retention, loss and planting from the applicant, which was appended to the update report)
- Additional consultation responses received
- Additional objections received
- An amended red line site boundary plan, to address the Network Rail objection
- List of plans

Details of further objections received which had not been included in the update report were given at the meeting, along with officer comments.

It was explained at the meeting that the Network Rail objection had been a result of an inadvertent over-run of Network Rail land in the Kennetmouth area, and the objection had now been withdrawn following a slight amendment to the red line site boundary plan.

That Wokingham Borough Council be informed that Reading Borough Council raised no objection to the proposal.

That Wokingham Borough Council be informed of the decision regarding the identical application 171108/REG3.

That Wokingham Borough Council be sent a copy of the report for their information and use.

Objectors John Booth, Tamzin Morphy and John Mullaney, Scott Witchalls and Luke Fay on behalf of the applicant and Ward Councillor Brenda McGonigle attended the meeting and addressed the Committee on this application.

## PLANNING APPLICATIONS COMMITTEE MINUTES - 30 MAY 2018

(Councillor Page declared a prejudicial interest in this item on the grounds of predetermination. He made a statement to the Committee, left the room and took no part in the debate or decision. Nature of interest: Councillor Page was the Lead Councillor for Strategic Planning & Transport and had been closely involved in developing the scheme).

(The meeting started at 6.30 pm and closed at 10.22 pm).

## READING BOROUGH COUNCIL

### REPORT BY DIRECTOR OF ENVIRONMENT & NEIGHBOURHOOD SERVICES

TO:	PLANNING APPLICATIONS COMMITTEE		
DATE:	27 JUNE 2018	AGENDA ITEM:	4
TITLE:	POTENTIAL SITE VISITS FOR COMMITTEE ITEMS		
SERVICE:	PLANNING	WARDS:	BOROUGHWIDE
AUTHOR:	KIARAN ROUGHAN	TEL:	0118 9374530
JOB TITLE:	PLANNING MANAGER	E-MAIL:	kiaran.roughan@reading.gov.uk

#### 1. PURPOSE AND SUMMARY OF REPORT

- 1.1 To identify those sites where, due to the sensitive or important nature of the proposals, Councillors are advised that a Site Visit might be appropriate before the meeting of the next Committee (or at a future date) and to confirm how the visit will be arranged.

#### 2. RECOMMENDED ACTION

- 2.1 That you resolve to visit the sites which will be identified by officers in a paper in the update Agenda on the day of the forthcoming Planning Applications Committee and confirm if there are any other sites Councillors consider necessary to visit before reaching a decision on an application.
- 2.2 That you confirm how the site will be visited, unaccompanied or accompanied, and if accompanied agree the site visit date and time.

#### 3. THE PROPOSAL

- 3.1 The potential list of agenda items submitted since the last meeting of the Planning Applications Committee will be provided with the update Agenda on the day of forthcoming Planning Applications Committee. Where appropriate, I will identify those applications that I feel warrant a site visit by the Committee prior to formal consideration of the proposals.
- 3.2 Councillors may also request a site visit to other sites on that list if they consider it relevant to their ability to reach a decision on the application.
- 3.3 Officers may also recommend a site visit if they intend to report a normally delegated application to the Committee for a decision.
- 3.4 A site visit may also be proposed in connection with a planning enforcement issue which is before the Committee for consideration.



- 3.5 Site visits in the above circumstances should all take place in advance of a Committee decision and should only be used where the expected benefit is substantial.
- 3.6 A site visit is only likely to be necessary if the impact of the proposed development is difficult to visualise from the plans and any supporting material including photographs taken by officers (although, if this is the case, additional illustrative material should have been requested); or, there is a good reason why the comments of the applicant and objectors cannot be expressed adequately in writing; or, the proposal is particularly contentious.
- 3.7 Accompanied site visits consist of an arranged inspection by a viewing Committee, with officers in attendance and by arrangement with the applicant or their agent. Applicants and objectors however will have no right to speak but may observe the process and answer questions when asked. The visit is an information gathering opportunity and not a decision making forum.
- 3.8 Recently Councillors have expressed a preference to carry out unaccompanied site visits, where the site is easily viewable from public areas, to enable them to visit the site when convenient to them. In these instances the case officer will provide a briefing note on the application and the main issues to be considered by Councillors when visiting the site.
- 3.9 There may also be occasions where officers or Councillors request a post completion site visit in order to review the quality or impact of a particular development.

#### 4. CONTRIBUTION TO STRATEGIC AIMS

- 4.1 Planning services contribute to producing a sustainable environment and economy within the Borough and to meeting the 2015 -18 Corporate Plan objective for “Keeping the town clean, safe, green and active.” Under the heading, Neighbourhoods, the Corporate Plan aims to improve the physical environment - the cleanliness of our streets, places for children to play, green spaces, how we feel about our neighbourhood and whether we feel safe, have a sense of community and get on with our neighbours.

#### 5. COMMUNITY ENGAGEMENT AND INFORMATION

- 5.1 Statutory neighbour consultation takes place on planning applications.

#### 6. EQUALITY IMPACT ASSESSMENT

- 6.1 Officers when assessing an application and when making a recommendation to the Committee, will have regard to its duties Under the Equality Act 2010, Section 149, to have due regard to the need to—
- eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
  - advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;

- foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

## 7. LEGAL IMPLICATIONS

7.1 None arising from this report.

## 8. FINANCIAL IMPLICATIONS

8.1 The cost of site visits is met through the normal planning service budget.

## 9. BACKGROUND PAPERS

Reading Borough Council Planning Code of Conduct.

Local Safety Practice 2013 Planning Applications Committee site visits.

READING BOROUGH COUNCIL

REPORT BY DIRECTOR OF ENVIRONMENT & NEIGHBOURHOOD SERVICES

TO:	PLANNING APPLICATIONS COMMITTEE		
DATE:	27 JUNE 2018	AGENDA ITEM:	5
TITLE:	PLANNING APPEALS		
AUTHOR:	KIARAN ROUGHAN	TEL:	0118 9374530
JOB TITLE:	PLANNING MANAGER	E-MAIL:	Kiaran.roughan@reading.gov.uk

1. PURPOSE AND SUMMARY OF REPORT

- 1.1 To report notifications received from the Planning Inspectorate on the status of various planning appeals.

2. RECOMMENDED ACTION

- 2.1 That you note the appeals received and the method of determination as listed in Appendix 1 of this report.
- 2.2 That you note the appeals decided as listed in Appendix 2 of this report.
- 2.3 That you note the Planning Officers reports on appeal decisions provided in Appendix 3 of this report.

3. INFORMATION PROVIDED

- 3.1 Please see Appendix 1 of this report for new appeals lodged since the last committee.
- 3.2 Please see Appendix 2 of this report for new appeals decided since the last committee.
- 3.3 Please see Appendix 3 of this report for new Planning Officers reports on appeal decisions since the last committee.

4. CONTRIBUTION TO STRATEGIC AIMS

- 4.1 Defending planning appeals made against planning decisions contributes to producing a sustainable environment and economy within the Borough and to meeting the 2015 -18 Corporate Plan objective for “Keeping the town clean, safe, green and active.”

## 5. COMMUNITY ENGAGEMENT AND INFORMATION

- 5.1 Planning decisions are made in accordance with adopted local development plan policies, which have been adopted by the Council following public consultation. Statutory consultation also takes place on planning applications and appeals and this can have bearing on the decision reached by the Secretary of State and his Inspectors. Copies of appeal decisions are held on the public Planning Register.

## 6. EQUALITY IMPACT ASSESSMENT

- 6.1 Where appropriate the Council will refer in its appeal case to matters connected to its duties Under the Equality Act 2010, Section 149, to have due regard to the need to—
- eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
  - advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
  - foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

## 7. LEGAL IMPLICATIONS

- 7.1 Public Inquiries are normally the only types of appeal that involve the use of legal representation. Only applicants have the right to appeal against refusal or non-determination and there is no right for a third party to appeal a planning decision.

## 8. FINANCIAL IMPLICATIONS

- 8.1 Public Inquiries and Informal Hearings are more expensive in terms of officer and appellant time than the Written Representations method. Either party can be liable to awards of costs. Guidance is provided in Circular 03/2009 “Cost Awards in Appeals and other Planning Proceedings”.

## 9. BACKGROUND PAPERS

- 9.1 Planning Appeal Forms and letters from the Planning Inspectorate.

## APPENDIX 1

### Appeals Lodged:

WARD: MINSTER  
APPEAL NO: APP/E0345/Z/18/3193852  
CASE NO: 171582  
ADDRESS: Land at A33 Relief Rd, Rose Kiln Lane  
PROPOSAL: 48 sheet digital advertising board  
CASE OFFICER: Claire Ringwood  
METHOD: Written Representation  
APPEAL TYPE: REFUSAL OF ADVERTISING CONSENT  
APPEAL LODGED: 18<sup>TH</sup> May 2018

WARD: KENTWOOD  
APPEAL NO: APP/E0345/D/18/3202378  
CASE NO: 180057  
ADDRESS: 12 Lower Armour Road,  
PROPOSAL: First floor rear extension  
CASE OFFICER: Tom Hughes  
METHOD: Written Representation Householder  
APPEAL TYPE: REFUSAL  
APPEAL LODGED: 13<sup>th</sup> June 2018

WARD: REDLANDS  
APPEAL NO: APP/E0345/W/18/3198514  
CASE NO: 171954  
ADDRESS: 3-5 Craven Road  
PROPOSAL: Demolition of existing buildings and redevelopment to form 25 Retirement Living units (C3 use) for older persons with communal facilities, parking and associated landscaping.  
CASE OFFICER: Stephen Vigar  
METHOD: Informal Hearing  
APPEAL TYPE: REFUSAL  
APPEAL LODGED: 14<sup>th</sup> June 2018

## APPENDIX 2

### Appeals Decided:

WARD: ABBEY  
APPEAL NO: APP/E0345/W/17/3190722  
CASE NO: 161430  
ADDRESS: 179 Oxford Road, Reading  
PROPOSAL: Rear extension to second floor to enlarge 2x1 bedroom flats on approved application ref: 11/01564/FUL into 2x2 bedroom flats.  
CASE OFFICER: Ethne Humphreys  
METHOD: Written Representation  
DECISION: ALLOWED  
DATE DETERMINED: 25/05/2018

WARD: ABBEY  
APPEAL NO: APP/E0345/W/17/3189394  
CASE NO: 170975  
ADDRESS: 48 Watlington St  
PROPOSAL: Part single- part two-storey rear extension and conversion of enlarged existing HMO to form a six-person HMO above a self-contained basement flat.  
CASE OFFICER: Richard Eatough  
METHOD: Written Representation  
DECISION: DISMISSED  
DATE DETERMINED: 01.06.2018

## APPENDIX 3

Address Index of Planning Officers reports on appeal decisions.

No reports available this time.

READING BOROUGH COUNCIL

REPORT BY THE DIRECTOR OF ENVIRONMENT & NEIGHBOURHOOD SERVICES

TO:	PLANNING APPLICATIONS COMMITTEE		
DATE:	27 JUNE 2018	AGENDA ITEM:	6
TITLE:	APPLICATIONS FOR PRIOR APPROVAL		
AUTHOR:	LYNETTE BAKER & JULIE WILLIAMS		
JOB TITLE:	AREA TEAM LEADERS	E-MAIL:	<a href="mailto:Julie.williams@reading.gov.uk">Julie.williams@reading.gov.uk</a> <a href="mailto:Lynette.baker@reading.gov.uk">Lynette.baker@reading.gov.uk</a>

1. PURPOSE AND SUMMARY OF REPORT

- 1.1 To advise Committee of new applications and decisions relating to applications for prior-approval under the amended Town and Country Planning (General Permitted Development) Order (GPDO 2015).

2. RECOMMENDED ACTION

- 2.1 That you note the report.

3. BACKGROUND

- 3.1 At your meeting on 29 May 2013 a report was presented which introduced new permitted development rights and additional requirements for prior approval from the local planning authority for certain categories of permitted development. It was agreed then that a report be brought to future meetings for information and to include details of applications received for prior approval, those pending a decision and those applications which have been decided since the last Committee date.

4 TYPES OF PRIOR APPROVAL APPLICATIONS

- 4.1 The categories of development requiring prior approval under the Town and Country Planning (General Permitted Development)(England) Order 2015, or amended by the Town and Country Planning (General Permitted Development)(England)(Amendment) Order 2016 that are of most relevance to Reading Borough are summarised as follows:

- Householder development - single storey rear extensions. GPDO Part 1, Class A1(g-k).
- Change of use from A1 shops or A2 financial & professional, betting office, pay day loan shop or casino to A3 restaurants and cafes. GPDO Part 3 Class C.
- Change of use from A1 shops or A2 financial & professional, betting office or pay day loan shop to Class D2 assembly & leisure. GPDO Part 3 Class J.
- Change of use from A1 shops or A2 financial and professional or a mixed use of A1 or A2 with dwellinghouse to Class C3 dwellinghouse. GPDO Part 3 Class M\*
- Change of use from an amusement arcade or a casino to C3 dwellinghouse & necessary works. GPDO Part 3 Class N
- Change of use from B1 office to C3 dwellinghouse GPDO Part 3, Class O\*.
- Change of use from B8 storage or distribution to C3 dwellinghouse GPDO Part 3, Class P

- Change of use from B1(c) light industrial use to C3 dwellinghouse GPDO Part 3, Class PA\*
  - Change of use from agricultural buildings and land to Class C3 dwellinghouses and building operations reasonably necessary to convert the building to the C3 use. GPDO Part 3 Class Q.
  - Change of use of 150 sq m or more of an agricultural building (and any land within its curtilage) to flexible use within classes A1, A2, A3, B1, B8, C1 and D2. GPDO Part 3 Class R.
  - Change of use from Agricultural buildings and land to state funded school or registered nursery D1. GPDO Part 3 Class S.
  - Change of use from B1 (business), C1 (hotels), C2 (residential institutions), C2A (secure residential institutions and D2 (assembly and leisure) to state funded school D1. GPDO Part 3 Class T.
  - Temporary use of buildings for film making for up to 9 months in any 27 month period. GPDO Part 4 Class E
  - Development under local or private Acts and Orders (e.g. Railways Clauses Consolidation Act 1845). GPDO Part 18.
  - Development by telecommunications code system operators. GPDO Part 16.
  - Demolition of buildings. GPDO Part 11.
- 4.2 Those applications for Prior Approval received and yet to be decided are set out in the appended Table 1 and those applications which have been decided are set out in the appended Table 2. The applications are grouped by type of prior approval application. Information on what the estimated equivalent planning application fees would be is provided.
- 4.3 It should be borne in mind that the planning considerations to be taken into account in deciding each of these types of application are specified in more detail in the GDPO. In some cases the LPA will first need to confirm whether or not prior approval is required before going on to decide the application on its planning merits where prior approval is required.
- 4.4 Details of any appeals on prior-approval decision will be included elsewhere in the agenda.
5. CONTRIBUTION TO STRATEGIC AIMS
- 5.1 Changes of use brought about through the prior approval process are beyond the control or influence of the Council's adopted policies and Supplementary Planning Documents. Therefore it is not possible to confirm how or if these schemes will contribute to the strategic aims of the Council.
6. COMMUNITY ENGAGEMENT AND INFORMATION
- 6.1 Statutory consultation takes place in connection with applications for prior-approval as specified in the Order discussed above.
- 7 EQUALITY IMPACT ASSESSMENT
- 7.1 Where appropriate the Council must have regard to its duties under the Equality Act 2010, Section 149, to have due regard to the need to—
- eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
  - advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
  - foster good relations between persons who share a relevant protected characteristic and persons who do not share it.



7.2 There are no direct implications arising from the proposals.

## 8. LEGAL IMPLICATIONS

8.1 None arising from this Report.

## 9. FINANCIAL IMPLICATIONS

9.1 Since the additional prior notifications were introduced in May 2013 in place of applications for full planning permission, the loss in fee income is estimated to be £965,304

(Office Prior Approvals - £888,023: Householder Prior Approvals - £61,670:  
Retail Prior Approvals - £5836: Demolition Prior Approval - £2135: Storage Prior Approvals - £5350: Shop to Restaurant Prior Approval - £1886: Shop to Leisure Prior Approval - £305)

*Figures since last report*

Office Prior Approvals - £0: Householder Prior Approvals - £1030

9.2 However it should be borne in mind that the prior notification application assessment process is simpler than would have been the case for full planning permission and the cost to the Council of determining applications for prior approval is therefore proportionately lower. It should also be noted that the fee for full planning applications varies by type and scale of development and does not necessarily equate to the cost of determining them.

## 10. BACKGROUND PAPERS

The Town and Country Planning (General Permitted Development) (England) Order 2015

Town and Country Planning (General Permitted Development)(England)(Amendment) Order 2016.

Table 1 - Prior-approval applications pending @ 12 June 2018

Application type CLASS A - Householder

Application type	Application reference number	Address	Ward	Proposal	Date Received	Target Determination Date	Comments	Equivalent planning application fee
Householder Prior Approval - Class A, Part 1 GPDO 2015	180889	95 Thirlmere Avenue, Tilehurst, Reading, RG30 6XH	Kentwood	Rear extension measuring 5.13m in depth, with a maximum height of 2.65m, and 2.65m in height to eaves level.	25/05/2018	05/07/2018		£206
Householder Prior Approval - Class A, Part 1 GPDO 2015	180812	58 Donnington Road, Reading, RG1 5ND	Redlands	Rear extension measuring 4.5 metres in depth, with a maximum height of 3.85 metres and 3.0 metres in height to eaves level.	17/05/2018	27/06/2018		£206
Householder Prior Approval - Class A, Part 1 GPDO 2015	180844	47 Hexham Road, Reading, RG2 7UA	Redlands	Rear extension measuring 4.7m in depth, with a maximum height of 3.2m, and 2.85m in height to eaves level.	23/05/2018	03/07/2018		£206
Householder Prior Approval - Class A, Part 1 GPDO 2015	180853	47 Morlais, Emmer Green, Reading, RG4 8PQ	Thames	Rear extension measuring 3.5m in depth, with a maximum height of 3.2m, and 2.2m in height to eaves level.	23/05/2018	03/07/2018		£206
Householder Prior Approval - Class A, Part 1 GPDO 2015	180836	141 St Michaels Road, Tilehurst, Reading, RG30 4SB	Tilehurst	Rear extension measuring 4 metres in depth, with a maximum height of 3.3 metres, and 2.8 metres in height to eaves level.	22/05/2018	02/07/2018		£206

Office to Residential Prior Approval applications pending

Application type	Application reference number	Address	Ward	Proposal	Date Received	Target Determination Date	Comments	Equivalent planning application fee
Office use to dwelling house - Class O, Part 1 GPDO 2015	180722	Clarendon House, 59-75 Queens Road, Reading, RG1 4BN	Abbey	Change of use of building from Class B1(a) (offices) to C3 (dwelling houses) to comprise 49 dwellings.	27/04/2018	03/07/2018		£22080
Office use to dwelling house - Class O, Part 1 GPDO 2015	180658	Land to the rear of, 223 Oxford Road, Reading, RG1 7PX	Battle	Change of use of ground floor from Class B1(c) (light industrial) to C3 (dwelling house) to comprise of a 1 bed flat.	18/04/2018	20/06/2018		£366
Office use to dwelling house - Class O, Part 1 GPDO 2015	180654	14 Arkwright Road, Reading, RG2 0LS	Katesgrove	Change of use of office building from Class B1(a) (offices) to C3 (dwelling houses) to comprise 37 dwelling units.	18/04/2018	13/06/2018		£16536

Demolition Prior Approval applications pending

Application type	Application reference number	Address	Ward	Proposal	Date Received	Target Determination Date	Comments
Demolition Prior Approval	180217	20 Hosier Street, Reading, RG1 7JL	Abbey	Application for prior notification of proposed demolition.	02/02/2018	02/03/2018	
Demolition Prior Approval	180725	40 Silver Street, Reading, RG1 2ST	Katesgrove	Application for prior notification of proposed demolition.	01/05/2018	29/05/2018	

Retail Prior Approvals applications pending - None

Prior Notification applications pending - None

Shop to Restaurant Prior Approval applications pending - None

Shop to Assembly & Leisure Prior Approval applications pending - None

Telecommunications Prior Approval applications pending - None

Storage to Residential Prior Approval applications pending - None

Table 2 - Prior-approval applications decided 16 May 2018 to 12 June 2018

Application type CLASS A - Householder

Application type	Application reference number	Address	Ward	Proposal	Date Received	Decision Date	Decision
Householder Prior Approval - Class A, Part 1 GPDO 2015	180589	276 Tilehurst Road, Reading, RG30 2NE	Norcot	Rear extension measuring 5m in depth, with a maximum height of 3.625m, and 3m in height to eaves level.	04/04/2018	17/05/2018	Prior Approval NOT REQUIRED
Householder Prior Approval - Class A, Part 1 GPDO 2015	180621	53 Culver Road, Reading, RG6 1QA	Park	Rear extension measuring 6m in depth, with a maximum height of 3.9m, and 2.2m in height to eaves level.	09/04/2018	17/05/2018	Prior Approval NOT REQUIRED
Householder Prior Approval - Class A, Part 1 GPDO 2015	180791	38 Crawshay Drive, Emmer Green, Reading, RG4 8SY	Peppard	Rear extension measuring 3.5m in depth, with a maximum height of 3.7m, and 2.7m in height to eaves level.	11/05/2018	06/06/2018	Application Withdrawn
Householder Prior Approval - Class A, Part 1 GPDO 2015	180687	10 De Beauvoir Road, Reading, RG1 5NS	Redlands	Rear extension measuring 1.9m and 6m in depth, with a maximum height of 2.86m, and 2.86m in height to eaves level.	20/04/2018	21/05/2018	Agree

Application type	Application reference number	Address	Ward	Proposal	Date Received	Decision Date	Decision
Householder Prior Approval - Class A, Part 1 GPDO 2015	180622	20 Underwood Road, Reading, RG30 3LR	Southcote	Rear extension measuring 6m in depth, with a maximum height of 2.7m, and 2.6m in height to eaves level.	10/04/2018	17/05/2018	Prior Approval NOT REQUIRED
Householder Prior Approval - Class A, Part 1 GPDO 2015	180584	5 Ilkley Road, Caversham, Reading, RG4 7BD	Thames	Rear extension measuring 8m in depth, with a maximum height of 4m, and 2.5m in height to eaves level.	05/04/2018	25/05/2018	Prior Approval Notification - Refusal
Householder Prior Approval - Class A, Part 1 GPDO 2015	180620	119 Callington Road, Reading, RG2 7QF	Whitley	Rear extension measuring 5m in depth, with a maximum height of 3m, and 2.65m in height to eaves level.	09/04/2018	17/05/2018	Prior Approval NOT REQUIRED
Householder Prior Approval - Class A, Part 1 GPDO 2015	180640	65 Whitley Wood Lane, Reading, RG2 8PW	Whitley	Rear extension measuring 4m in depth, with a maximum height of 3.6m, and 2.52m in height to eaves level.	13/04/2018	18/05/2018	Prior Approval NOT REQUIRED
Householder Prior Approval - Class A, Part 1 GPDO 2015	180727	85 Spencer Road, Reading, RG2 8TP	Whitley	Rear extension measuring 4m in depth, with a maximum height of 3m, and 3m in height to eaves level.	01/05/2018	31/05/2018	Prior Approval Notification - Approval

Retail to Residential applications decided

Application type	Application reference number	Address	Ward	Proposal	Date Received	Decision Date	Decision
Retail Prior Approval	180572	180 Wantage Road, Reading, RG30 2SJ	Norcot	Notification for Prior Approval for a Proposed Change of Use of a Building from Class A1 (shops) to C3 (dwellinghouses) to comprise two dwellings on the ground floor.	29/03/2018	23/05/2018	Prior Approval Notification - Approval

Prior Notification applications decided

Application type	Application reference number	Address	Ward	Proposal	Date Received	Decision Date	Decision
Prior Notification	172192	Reading West Footbridge, Reading West Station, Oxford Road, Reading	Battle	Prior Approval for reconstruction of the footbridge to provide the necessary clearance for the OLE which is to run underneath the structure.	07/12/2017	31/05/2018	Application Refused

Office to Residential Prior Approval applications decided - None  
 Telecommunications Prior Approval applications decided - None  
 Demolition Prior Approval applications decided - None  
 Storage to Residential Prior Approval applications decided - None  
 Shop to Assembly & Leisure Prior Approval applications decided - None  
 Shop to Restaurant Prior Approval applications decided - None





# ABBEY



## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT CULTURE & SPORT  
READING BOROUGH COUNCIL  
PLANNING APPLICATIONS COMMITTEE: 27<sup>th</sup> June 2018

ITEM NO. 7

Ward: Abbey

App No: 180410/ADV

Address: Reading Train Station

Proposals: Double sided LED digital smart screen

Target decision date: 17<sup>th</sup> May 2018 Extension of time date: 29<sup>th</sup> June 2018

### RECOMMENDATION

GRANT Advertisement Consent subject to conditions

Conditions to include:

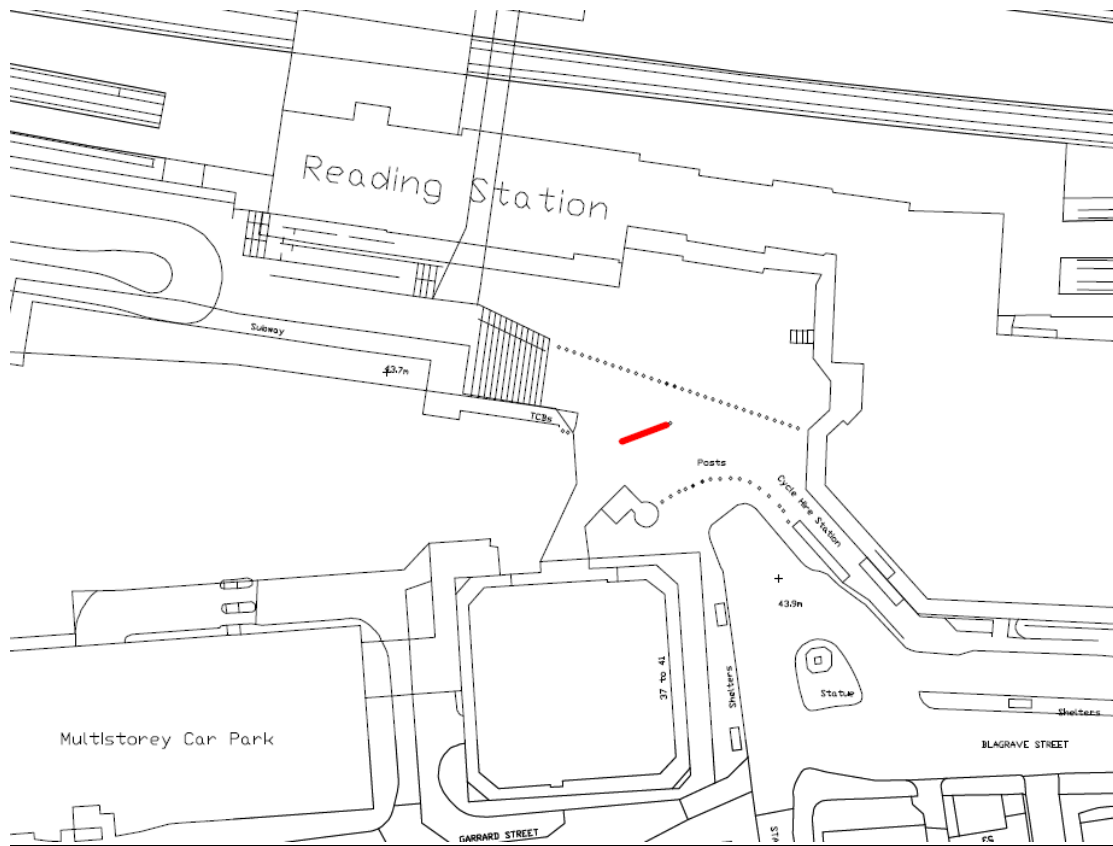
1. In accordance with approved plans
2. Time Limit - grant of 5 years advertisement consent only
3. Standard Advertisement Conditions
4. Construction Method Statement
5. Hours of operation - 0600 to 2400 hours only
6. Luminance not to exceed 500 cdm<sup>2</sup> during the evening (1700 to 2400 hours)

Informatives to include:

1. Terms and Conditions
2. Positive and Proactive

### 1. INTRODUCTION

- 1.1 This application for advertisement consent relates to land to the south of Reading Train Station adjacent to the main entrance to the station building. The train station building continues to the east, to the west there are a series of steps leading down to Station Hill whilst to the south there is Thames Tower, a recently refurbished 15 storey commercial building. To the north-east of the site and to the east of the modern main station entrance is the Three Guineas Public House which is a grade II listed building.
- 1.2 The site is located within the Reading Central Area and Central Core as defined by the Reading Central Area Action Plan (2009).
- 1.3 Reading Borough Council is the landowner of the application site but is not the applicant.



Location Plan

## 2. PROPOSAL & BACKGROUND

- 2.1 The application seeks advertisement consent for a double sided LED smart screen to be located in the pedestrian zone to the south of the southern station entrance.
- 2.2 The proposed screen would 8m in width and 4m in height and would be set upon a 2.4m high supporting stand with a 0.3m frame. Altogether the structure would have a total height of 7m, width of 8.6m and would be 0.4m in depth.
- 2.3 It is proposed that the LED screen would display static and motion image advertisements which would be in place for 10 seconds at a time before merging into a new image at a speed of around 1.8 seconds. The screen is proposed to operate between 0600 and 2400 hours daily.
- 2.4 The luminance of the screen would adapt depending on the time of day and light conditions to make the display visible. The screen would operate at low luminance (approximately 500 cdm<sup>2</sup>) during evenings and night times and approximately 5000 cdm<sup>2</sup> during bright sunny days.

## 3. PLANNING HISTORY

3.1 None relevant to this advertisement application.

#### 4. CONSULTATIONS

4.1 RBC Transport - No objection, subject to a condition to control the screen's luminance during evening/night time.

4.2 RBC Environmental Protection - No objection.

4.3 RBC Emergency Planning - No objection following submission of additional information concerning impact on views from CCTV cameras.

4.4 RBC Abbey Quarter Team - Concern that the screen would block directional and interpretational signage in the station pedestrian zone regarding the Abbey Quarter and its impact on the setting of the Grade II listed Three Guineas PH.

4.5 National Rail - No objection following submission of additional information concerning emergency vehicle access and impact on CCTV cameras.

4.6 British Transport Police - No objection subject that concerns regarding impact on RBC CCTV and impact of the construction compound upon emergency vehicle access can be addressed.

4.7 RBC Conservation Consultant - Object, the proposal would harm the significance of the Grade II listed Three Guineas PH and fail to preserve its setting.

4.8 Conservation Area Advisory Committee - Object, the proposal will detract from the setting of the Grade II listed heritage assets around the station. The public benefit of the screen would not outweigh the harm to the heritage assets.

#### 4.6 Public consultation:

Two site notices were displayed at the application site. Six letters of objection have been received raising the following issues:

- Harm to the visual amenity of the street-scene and public realm by reason of screens scale, height, clutter and location
- Block views to and from the station
- Block views of the ground floor signage of Thames Tower
- Harm to the setting of the grade II listed Three Guineas PH building and statue of King Edward VII
- Detract from the function and legibility of the public realm area to the south of the station

#### 5. RELEVANT PLANNING POLICY AND GUIDANCE

- 5.1 The Town and Country Planning (Control of Advertisements) Regulations 2007 apply.
- 5.2 Section 16(2) of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires the local planning authority to have special regard to the desirability of preserving a listed building or its setting or any features of special interest which it possesses.
- 5.3 National Planning Policy Framework  
Part 7: Requiring Good Design  
Part 12: Conserving and Enhancing the Historic Environment
- 5.4 National Planning Practice Guidance  
Advertisements
- 5.5 Reading Borough Local Development Framework Core Strategy (2008) (altered 2015)  
CS7: Design and the Public Realm  
CS33: Protection and Enhancement of the Historic Environment
- 5.6 Sites and Detailed Policies Document (2012) (Altered 2015)  
DM12: Access, Traffic and Highway-Related Matters  
DM22: Advertisements
- 5.7 Reading Central Area Action Plan (adopted 2009)  
RC5: Design in the Centre  
RC14: Public Realm
- 5.8 Reading Station Area Framework (2010)
- 5.9 Station Hill South Planning and Urban Design Brief (2007)
- 6. APPRAISAL - Application for Advertisement Consent
  - (i) Legal context
    - 6.1 Regulation 3 of the Town and Country Planning (Control of Advertisements) (England) Regulations 2007 requires the Local Planning Authority to exercise its powers under these regulations in the interests of amenity and public safety taking into account the provisions of the development plan, so far as they are material; and any other relevant factors. Regulation 3 states that factors relevant to amenity include the general characteristics of the locality, including the presence of any feature of historic, architectural, cultural, or similar interest.
    - 6.2 Factors relevant to public safety include highway safety and whether the advert would hinder security or surveillance devices, including speed cameras.

(ii) Main Issues

6.3 Therefore the main issues are:

- a) The effect upon visual amenity, the public realm and heritage assets
- b) The effect upon public safety

a) The effect upon visual amenity, the public realm and heritage assets

6.4 Policy DM22 (Advertisements) states that advertisements will respect the building or structure on which they are located and/or their surroundings and setting in terms of size, location, design, materials, colour, noise, lettering, amount and type of text, illumination and luminance. It also specifies that the cumulative impact of adverts will be taken into account, and a proliferation of advertisements that detrimentally affects visual or aural amenity will not be acceptable.

6.5 Policy CS7 (Design) seeks that all development must be of high design quality that maintains and enhances the character and appearance of the area of Reading within which it is located. Proposals should also make a positive contribution to urban design objectives including, character, quality of public realm, ease of movement and permeability, legibility, adaptability and diversity. In addition the policy states that developments will be assessed to ensure that they respond positively to their local context and create or reinforce local character and distinctiveness, including protecting and enhancing the historic environment of the Borough and providing value to the public realm; create safe and accessible environments and are visually attractive as a result of good high quality built forms and spaces.

6.6 Policy CS33 (Protection and Enhancement of the Historic Environment) seeks that all development proposals should preserve or enhance the historic character of heritage assets.

6.7 Policy RC5 (Design in the Centre) seeks that development should provide appropriate relationships between buildings and spaces and movement within the central area, appropriate public spaces and public realm including squares and open spaces, and utilise high quality architectural details and materials. It also indicates that any development associated within public realm should contribute to the diversity of the central area, be capable of adaptation over time and be designed to enhance community safety.

6.8 Policy RC14 (Public Realm) states that new development will need to make a positive contribution towards the quality of the public realm including imaginative uses of open space and public realm.

6.9 In terms of adopted policies directly related to development of the station area Policy RC1d (Station & Interchange) states that the listed station building should be retained and its setting enhanced and areas of open

space will be provided at the northern and southern entrances to the station. The Reading Station Area Framework and the Station Hill South Planning and Urban Design Brief seek that the area adjacent to the station entrance should be a high quality multi-functional public space - 'new town squares'.

- 6.10 The proposed screen structure would be located centrally within the public realm area to the south of the train station entrance. The proposed plan shows the screen with a slight north-west orientation addressing people exiting the main station building to the north, people arriving at the top of the stairs which connect to Station Hill to the west and people approaching the station from the south from Station Road and Blagrave Street.
- 6.11 The area of public realm to the south of the station where the structure would be located contains some street furniture, which aside from a number of lamp posts, is low level including a row of safety bollards and some benches.
- 6.12 The screen and its stand by its nature is contemporary in its design and appearance and would be in keeping with the modern station building to the north. The updated façade of Thames Tower also now appears as a more contemporary elevation bordering the station public realm area.
- 6.13 Whilst the screen is larger in scale than the existing street-furniture the pedestrian area itself is not insignificant in size. Located relatively centrally within this area, surrounded by two storey buildings together with Thames Tower at fifteen storeys to the south, and set against the modern Station building, it is considered the screen would not unduly detract from the spaciousness of this area, quality of the public realm or frontages of surrounding buildings.
- 6.14 The screen would represent a modern addition to the public realm area to the south of the station, an area where the contrast between old and new buildings is already present. As a freestanding structure located centrally within the large pedestrian area it is considered that the proposal would contribute to the visual interest of the public realm and would not be detrimental to the permeability, accessibility, adaptability or legibility of this area.
- 6.15 Concern has been raised regarding the impact of the screen upon the setting of the Grade II listed Three Guineas Public House to the north of the station pedestrian area and also the statue of King Edward VII on the small roundabout on Station Approach to the south. Officers acknowledge that the screen would be a sizeable structure within the station public realm area. However, it is noted that the siting of the screen is to the south west of centre of this area. This assists in the screen being viewed, when looking north towards the station, against the back drop of the modern station building rather than directly or primarily against the backdrop of the listed public house. There is also a significant separation between the screen and



the public house (28m). In the context of the spaciousness of the square and the scale of the surrounding large buildings, officers consider that the proposal will not adversely impact upon the setting of the listed building. Moving the structure westwards, away from the listed building, has been explored. However, the area under the west of the square comprises voids and it would be impractical to anchor the structure within this area.

- 6.16 The details submitted as part of the application also indicate that the luminance of the screen would be controlled via sensors which automatically adjust depending upon prevailing natural light conditions whilst ensuring the luminance levels reduce during evenings which would assist in minimising light spillage that might have an impact on the heritage assets, particularly during the evenings. The screen is proposed to operate between 0600 and 2400 only. This could be controlled by way of a suitably worded condition to ensure the screen is turned off outside of these hours.
- 6.17 Sited over 40m from the Grade II listed statue of King Edward VII it is not considered that the screen would adversely impact upon the setting of the statue. Again due to the screens siting, key views of the statue from the station would be retained, with the screen predominantly viewed against the backdrop of the north east corner of Thames Tower.
- 6.18 A condition is recommended to grant advertisement consent for a 5 year period. This is in accordance with Regulation 14 (7) of the Town and Country Planning (Control of Advertisements) Regulations 2007 and would allow the Local Planning Authority to review the impact of the advertisement scheme after this period.

#### b) The effect upon public safety

- 6.19 The relevant considerations for this application with regard to public safety are highway safety and crime prevention, including whether granting consent could block the view of CCTV cameras, or whether illumination from an advertisement would cause glare on such cameras.
- 6.20 Policy DM22 (Advertisements) states that advertisements will not have detrimental impact on public safety. It also specifies that the cumulative impact of adverts will be taken into account.
- 6.21 Policy CS7 (Design) seeks that all development should create safe and accessible environments.
- 6.22 Policy DM12 (Highway Matters) states that development will only be permitted where it is not detrimental to highway safety.
- 6.23 Policy CS20 (Reading Transport Strategy) seeks to promote transport safety.
- 6.24 The proposal has been reviewed by RBC Transport Officers who have advised that as the screen would be located within a pedestrian area and is not

located on a major transport corridor, it is considered that the proposed advertisement would not unduly distract highways users and give rise to safety issues. This is subject to a condition to ensure that the screen does not exceed 500 cd/m<sup>2</sup> during the evening (from 1700 to 2400 hours) when the screen illumination is likely to be more prominent from longer distance views.

- 6.25 The base of the screen would also be elevated 2.4m from ground level which would not impede pedestrian and cycle movements within the station public realm area.
- 6.26 RBC Emergency Planning, National Rail and British Transport Police originally objected to the application based upon concerns regarding impact upon CCTV (blocking sightlines and glare) and emergency vehicle access to the station.
- 6.27 Further to these objections, and following a meeting with the applicant, additional material was submitted which includes more detailed information on the specification of the screen and how this can be managed remotely to reduce luminance and avoid glare. Details of vehicular access were also provided indicating that the screen would not obstruct the existing route via the dropped kerb from Station Approach and the two pairs of lowering bollards.
- 6.28 Following the submission of the additional information the concerns raised by RBC Emergency Planning, National Rail and British Transport Police are considered to have been largely overcome.
- 6.29 With regard to CCTV impact further review of existing cameras has been undertaken by RBC Emergency Planning. They have advised the screen would have only a minimal impact upon the view from one existing CCTV camera which is not classed as essential. The information regarding ability to remotely control the brightness of the screen is considered to address concerns regarding glare to the existing CCTV.
- 6.30 Although RBC Emergency Planning now raises no objection it has been requested that the developer undertakes to provide a new or replacement camera should the glare from the screen prove to be a problem for existing cameras. The developer has agreed to this if needed in the future. As this is a matter that would be more appropriately addressed by the Council, as landowner of the site, by making it a requirement of the lease. Officers advise that it is not necessary to use a planning condition to secure this commitment.
- 6.31 In terms of emergency vehicle access, it has been demonstrated that the existing route would not be affected by the proposed screen. British Transport Police remain concerned that the Construction Compound for when the screen is installed should not impinge on emergency vehicle routes. Details of Construction Methodology have been provided as part of

the application information but it is reasonable to require a more detailed construction method statement to be submitted and agreed with the Local Planning Authority by way of a suitably worded condition, prior to the commencement of any works. This would include a requirement to provide an assessment of the impact of the construction compound upon emergency vehicle routes.

- 6.32 Whilst land ownership is not usually a planning consideration, it is relevant to note that the land to which this application relates is controlled by the Council and as such there is an additional level of control of the signage and the applicant would also be required to sign up to the Council's advertising and sponsorship policy.

## 7 CONCLUSION

- 7.1 When assessed for impact on amenity, the setting of listed buildings and public safety officers are satisfied that, subject to recommended conditions, the proposal complies with national and local policy.

### Recommendation

GRANT advertisement consent for the reasons given above

Case Officer: Matt Burns

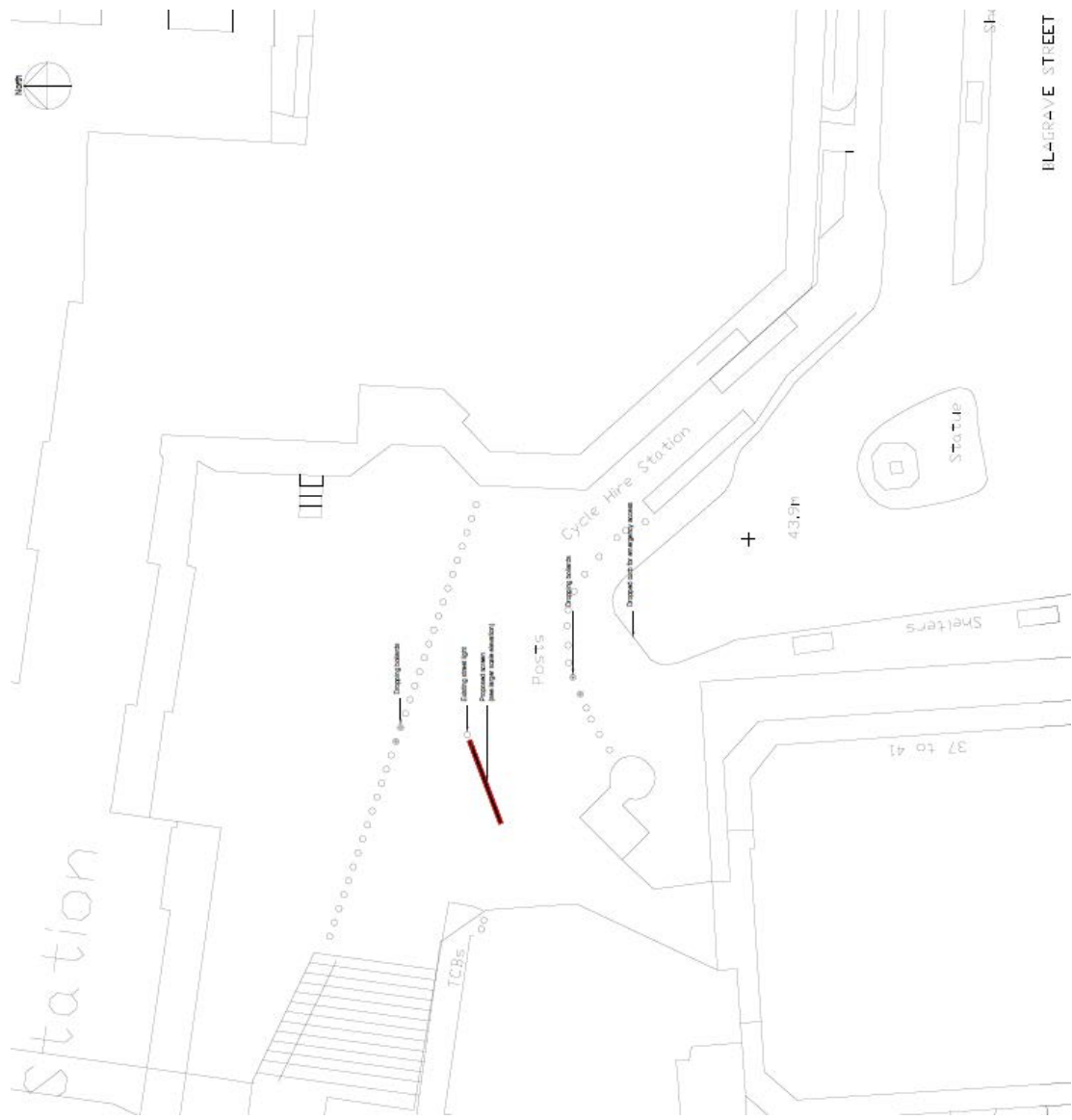
### Plans and Documents Considered:

Drawing no. L01.B - Location Plan, Block Plan and Elevations dated March 18  
Received by the Local Planning Authority on 25<sup>th</sup> April 2018

Orion Heritage Statement dated March 2018  
Received by the Local Planning Authority on 22<sup>nd</sup> March 2018

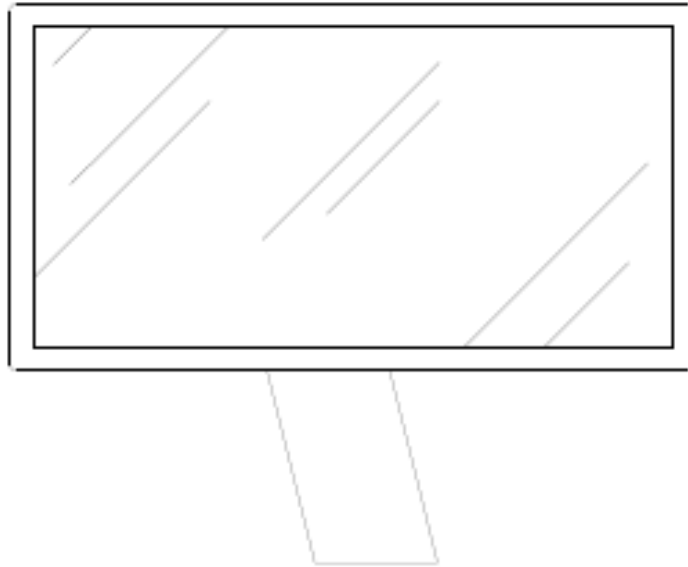
'ECE Planning' Planning Statement dated March 2018  
SIS Digital Reading Lights Maxx Media Ltd Structural Information  
Maxx Media Ltd Visuals  
Received by the Local Planning Authority on 7<sup>th</sup> March 2018

UNILED - Brightness Settings and Management Letter  
SIS Digital Method of Construction Proposal  
'ECE Planning' letter ref. RH/Let/P1489 dated 25<sup>th</sup> April 2018  
Received by the Local Planning Authority on 25<sup>th</sup> April 2018



Proposed Site Plan

Location Plan 1:1250



8000mm x 4000mm digital screen  
Front elevation 1:50



8000mm x 4000mm digital screen  
Side elevation 1:50

Proposed Elevations



Proposed visual - view north from Station Approach 1



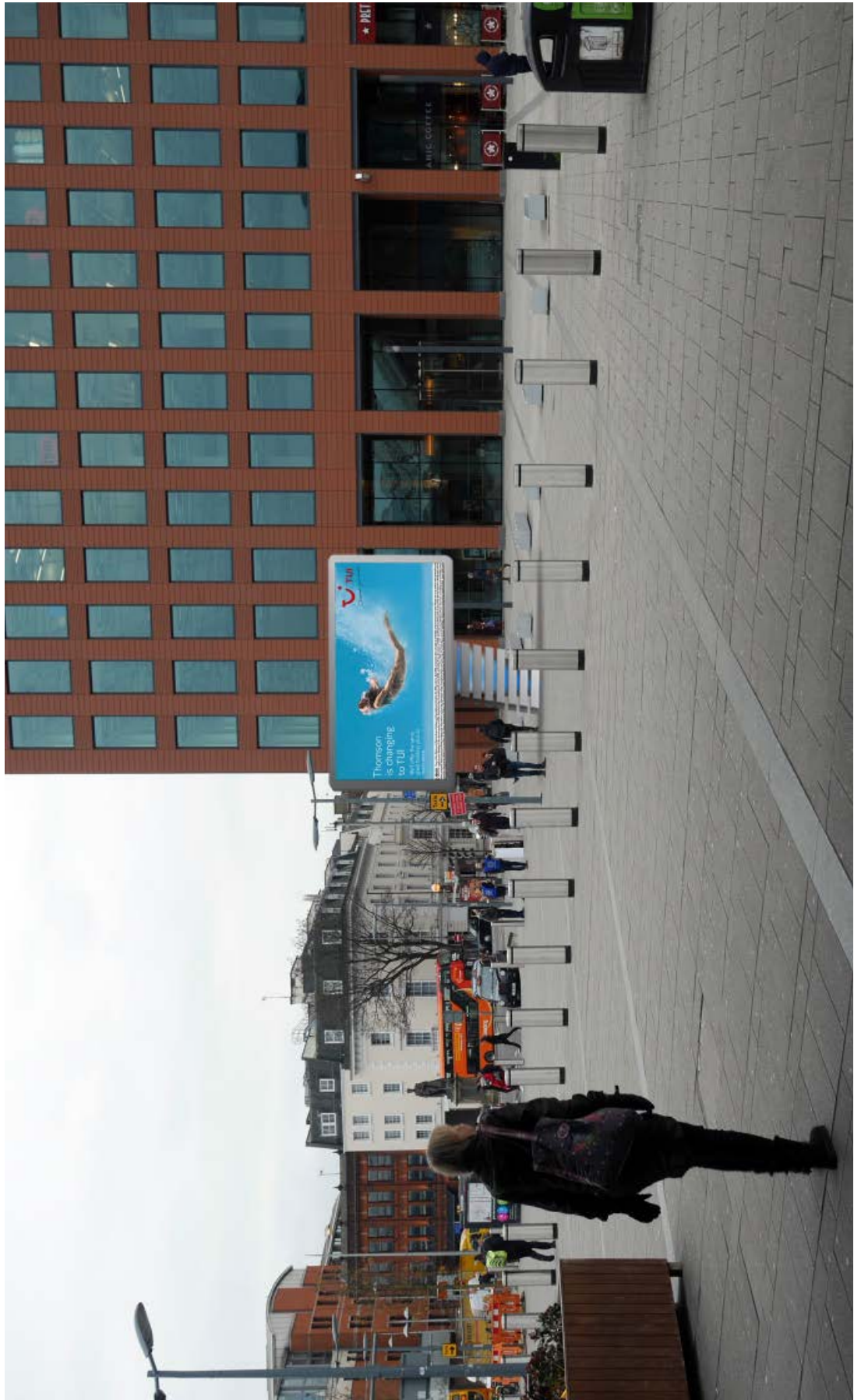
Proposed visual - View north from Station Approach 2





Proposed Visual - View West from Old Station Building





Proposed Visual - View South from Modern Station Building

## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT AND NEIGHBOURHOOD SERVICES  
READING BOROUGH COUNCIL  
PLANNING APPLICATIONS COMMITTEE: 27 June 2018

ITEM NO. 8

Ward: Abbey

App No.:171808/FUL

Address: Central Jamme Mosque, 18/18a Waylen Street, Reading

Proposal: Retrospective planning application for the demolition of a pre-existing extension and the construction of a two storey rear extension, and the erection of a boundary wall adjacent to the highway (amended description).

Applicant: Bangladesh Association of Greater Reading (charity number 1039747).

Date received: 6 December 2017

Minor Application PPA decision date: 4 July 2018

**RECOMMENDATION: GRANT retrospective planning permission.**

Conditions to include:

1. AP1 Approved plans.
2. No later than three months from the date of this planning permission, details of the measures to block up of windows and substitution of glazed doors for solid doors, as shown on the approved plans in Condition 1, shall have been submitted to the Local Planning Authority. The details for the windows shall include all materials and plans and sections of not less than 1:20 scale showing how the works will be carried out. The details for the doors will show full specifications of the doors to be used. The above works shall be undertaken in full not later than three months following written approval of the details by the Local Planning Authority. The development shall be retained in accordance with the approved details thereafter.  
Reason: to ensure that the unauthorised development is regularised in terms of the harm caused to privacy/overlooking of neighbouring properties, in accordance with Policy DM4.
3. No later than three months from the date of this planning permission, detailed scaled elevations and plans shall have been submitted to the Local Planning Authority, which shall show a the reinstatement of the front boundary wall which has been demolished. These details shall be based on the wall as shown in the approved plans approved by Condition 1 above. Following approval, the wall shall be completed no later than three months following approval of the approval of the details.  
Reason: to provide a suitable means of enclosure, in the interests of the streetscene and the Conservation Area, in accordance with policies CS7 and CS33.
4. No later than three months from the date of this planning permission, a scheme for hard and soft landscaping scheme for the area enclosed to the front of the site, shall have been submitted to the Local Planning Authority. The scheme should include hard landscaping details and full details of cycle parking and bin storage provision. The approved landscaping works as described above shall be carried out in full compliance with the approved scheme no later than three months following

approval of the approval of the details and the development retained with such facilities thereafter.

Reason: to ensure that the unauthorised development is regularised in terms of the harm caused to the streetscene and to ensure that a suitable level of cycle parking and servicing is provided in accordance with policies CS7, CS24 and CS33.

5. No later than four weeks from the date of this planning permission, the extract/ventilation systems shall have been installed in accordance with the approved plans and specifications and thereafter the extract/ventilation systems shall be permanently retained and maintained in accordance with the approved specifications. Thereafter, the specific sound level of the plant/equipment hereby approved,  $LA_{eqr,Tr}$  as measured at a point 1 metre external to sensitive facades, shall be at least 10dB below the existing background sound level,  $LA_{90,T}$  when all plant/equipment (or any part of it) is in operation. The noise rating level of the plant/equipment hereby approved,  $LA_{eqr,Tr}$  (specific sound level plus any adjustment for the characteristic features of the sound) as measured at a point 1 metre external to sensitive facades, shall not exceed the existing background sound level,  $LA_{90,T}$  when all plant/equipment (or any part of it) is in operation.

Reason: To safeguard the amenities of the adjoining premises and the area generally, in accordance with Policy CS34 of the Reading Borough LDF Core Strategy 2008 (Altered 2015) and Policy DM4 of the Reading Borough LDF Sites and Detailed Policies Document 2012 (Altered 2015).

6. Notwithstanding the provisions of Schedule 2, Part 3 of the Town and Country Planning (General Permitted Development) (England) Order 2015, the premises shall be used as a D1 Mosque offering space for a combination of worship, training, education and meetings activities for a maximum of 300 people only and for no other purpose (including any other purpose in the same Use Class of the Schedule to the Town & Country Planning (Use Classes) Order 1987 (as amended), or in any provision equivalent to that Class in any statutory instrument revoking or re-enacting that Order with or without modification.
7. Basement ablutions area to remain ancillary only.
8. No amplified music at the premises at any time.
9. All openings (windows, doors) shut during services.
10. Submission of a travel plan.

#### Informatives

- The provisions of the extant Enforcement Notice continue to apply.
- Separate approval under the Building Regulations and Fire Safety Regulations is required.
- Terms and conditions
- Conditions precedent
- Positive and proactive requirement
- No parking permits to be issued

## 1. INTRODUCTION

- 1.1 The application site consists of Nos. 18/18a Waylen Street, just to the west of central Reading. It is situated within a street of predominantly large terraced Victorian residential properties and within the Russell Street/Castle Hill Conservation Area.
- 2.2 The site is long and narrow and generally flat. It accommodates a frontal building which has two distinct elements and was formerly the Elim Family Church and a house. Little is known about the history of the buildings and there is no detailed information on this site in the relevant Conservation Area Appraisal. But the right-hand side (northerly) building probably started off as a handsome 1840s-1860s Italianate style villa in its own relatively generous garden. It has some nice decorative features, including exposed rafters at the eaves, coloured string courses, stone window surrounds and arched windows. The building that then abuts it, 18a, appears (according to historic maps) to have been added at roughly the same time as the rear church hall, so given the style and the fact that it would appear to be of cavity wall construction, somewhere between 1930-1950.
- 2.3 The site has been the home of the Central Jamme Mosque (also known as the Central Jamme Masjid) for around the last 20 years. The application site area measures some 400 sq.m in area.



## 2. PROPOSAL

- 2.1 The planning application primarily seeks to retain a large, two storey extension which replaced an earlier extension. The application also includes other related works which consist of filling in and adjusting various openings on both flank ground floor elevations of the extension and the rebuilding of a front boundary wall, which

may have been removed in order to facilitate the building works which have taken place.

2.2 The development currently on site does not benefit from a valid planning permission. Your officers have been encouraging the applicant/owner to submit a planning application to attempt to regularise the planning situation for a number of years. An Enforcement Notice was eventually served on the owners in May 2017. This application was submitted in response to that Notice.

2.3 This application is being referred to your meeting for a number of reasons.

- There has recently been an Enforcement appeal decision and this is a large structure which the appeal Inspector considers is not acceptable;
- it is a non-residential assembly and leisure use in a residential conservation area; and
- a similar proposal in 2001 was also reported to the Planning Applications Committee and at that time, the Committee granted the planning permission.

2.4 Religious buildings are not chargeable for the Community Infrastructure Levy (CIL) under the Council's adopted CIL Charging Schedule.

2.5 Supporting documents submitted with the application include:

- Planning and Design and Access Statement
- Heritage Statement
- Acoustic report
- BREEAM Pre-estimator

### 3. PLANNING HISTORY

3.1 Relevant planning history is as follows:

00/01355/FUL (later planning reference: 990726)	First floor rear extension and internal alterations to existing Mosque and part change of use of four bedroom house to offices	PERMISSION 7/3/2001. LAPSED.
140288/PREAPP	Pre-application advice for proposed amendments.	Advice supplied 30/4/2014.
170154/CLE	1st Floor rear extension and internal alterations to existing Mosque.	CERTIFICATE REFUSED 13/4/2017
E0345/C/17/3178555	Enforcement Notice served 12/5/2017. Enforcement appeal received, concerning: Without planning permission, the erection of a two storey rear extension and removal of a boundary wall.	APPEAL DISMISSED 4/4/2018, planning permission refused, Enforcement Notice upheld, subject to minor variation concerning compliance period.

### 4. CONSULTATIONS

(i) Statutory:

None.

(ii) Non-statutory:

RBC Transport Strategy has raised the following concerns:

- Clarification is required as to how the increase in floor space has affected congregation numbers.
- Proposed mode of transport split would be required as to how attendees travel to the Mosque. This could be achieved by undertaking surveys from the existing attendees.
- It would appear that the demolition of the boundary wall has caused damage to the public highway. *Officer comment: this matter has been passed to RBC Environment and Neighbourhood Services to assess separately.*
- The Mosque is situated in a CPZ area, there are shared user bays directly outside the mosque; surveys of shared user bays is required to ascertain use of bays during hours the mosque is at its busiest i.e. Friday prayer times.

RBC Environmental Protection has raised issues with noise from the congregation and the plant noise and has proposed detailed condition wording. Full discussion is provided in the Appraisal below.

RBC Building Control advises that there is no Building Regulations approval for the works which have been undertaken, although a Building Regulations application was submitted in 2012 and is still a live application. Building Control's principal concerns are means of escape in the event of a fire.

Berkshire Archaeology advises that there are no archaeological issues with the application. Given the scale of the extension and the previous impacts on site, Berkshire Archaeology would not have recommended that any archaeological investigations would have been required prior to construction.

### Public consultation

Letters were sent to the following addresses in Waylen Street in December 2017: 16, 19 (Flats 1-4), 20, 23, 25. No letters received.

## 5. RELEVANT POLICY AND GUIDANCE

5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that proposals be determined in accordance with the development plan unless material considerations indicate otherwise. Material considerations include relevant policies in the National Planning Policy Framework (NPPF) - among them the 'presumption in favour of sustainable development'.

5.2 The following local and national planning policy and guidance is relevant to this application:

National Planning Policy Guidance  
National Planning Policy Framework (NPPF)

The Reading Borough Local Development Framework: Core Strategy (2008, as amended, 2015)

CS1 Sustainable Construction and Design  
CS3 Social Inclusion and Diversity

CS4 Accessibility and the Intensity of Development  
CS5 Inclusive Access  
CS7 Design and the Public Realm  
CS17 Protecting the Existing Housing Stock  
CS20 Implementation of the Reading Transport Strategy  
CS22 Transport Assessments  
CS23 Sustainable Travel and Travel Plans  
CS24 Car/Cycle Parking  
CS31 Additional and Existing Community Facilities  
CS33 Protection and Enhancement of the Historic Environment  
CS34 Pollution and Water Resources

The Reading Borough Local Development Framework: Sites and Detailed Policies Document (2012, as amended, 2015)

SD1 Presumption in Favour of Sustainable Development  
DM4 Safeguarding Amenity  
DM12 Access, Traffic and highway-Related Matters  
DM19 Air Quality

Supplementary Planning Documents

Revised Parking Standards and Design (2011)  
Sustainable Design and Construction (2011)

Other documents: Russell Street/Castle Hill Conservation Area Appraisal (2004)

## 6. APPRAISAL

### 6.1 The main issues are:

- a) Principle of the use
- b) Noise and disturbance
- c) Rear extension: design and impact on neighbours
- d) Loss of the wall and impact on the Conservation Area
- e) Transport
- f) Equalities and disabled access issues

#### a) Principle of the use

6.2 The former Elim Church hall, which was to the rear of the site and then included the front (left) building in an L-shape, appears to be present on old maps going back as far as WWII, therefore the principle of a D1 Place of Worship use on this site is accepted as being established. The hall to the Elim church, which was known to have been single storey with a vaulted ceiling and pitched roof and which covered the majority of the rear of the site, is likely to have been in the region of 200 square metres in size and therefore capable of potentially accommodating a large number of people, for which there were no planning restrictions. Therefore, although essentially a non-conforming use in a residential area, it is accepted that this has been a long-established situation.

6.3 At this point it is worth noting that the Planning Applications Committee approved a similar proposal (against an officer recommendation to refuse permission) to that

which is now under consideration, in 2001. However, as indicated in the Planning History section above, that permission was not implemented and lapsed. The appeal Inspector considered that the appeal should be considered on its own merit, leading officers to advise that no weight should be given to the earlier permission. However, references will be made to that permission where relevant and the differences between the two schemes will feature in this assessment as a comparison.

- 6.4 At the time of the original planning application's consideration in 2000/1, the Committee Report indicates that the site then consisted of a house at the front of the site and church hall to the rear. The report agreed to the change of use of the house to become part of the mosque and retention of part of it as, almost, a self-contained flat on the first floor. The current plans (as built) show an office, bedroom and shower-room for the Imam and then a walk along a landing to the main kitchen/dining area of the mosque. Officers suspect that whilst these internal changes may have taken place, perhaps 10-15 years ago, this was not of itself an implementation of the 2001 planning permission. RBC Council Tax has advised that this building ceased to pay Council Tax in May 2000. Whilst the new layout would be technically contrary to Policy CS17 (which seeks to retain dwellings), officers consider that there would continue to be an ancillary residential function/presence associated with the mosque and the situation in practice is unlikely to be greatly different from that which was considered suitable in the 2001 permission and may of itself become immune from enforcement in any event. Officers therefore consider that in this case, although a separate residential unit is technically lost, a residential purpose is maintained.
- 6.5 It is also noted that the proposal involves improvements to an existing community facility and there is support for this in principle in policies CS31 and CS3 and for this in a generally sustainable location in terms of Policy CS4. On the basis of the above, officers advise that the principle of an extension of the mosque is acceptable, subject to the issues identified below.

b) Noise and disturbance

- 6.6 The application site is in a residential street near Central Reading. Waylen Street is a Victorian street which is characterised by narrow terraced townhouses of a variety of similar styles, typically 2 and 3 storeys. The application site is different, featuring a large villa with what appears to be a large 2½ storey side extension and the whole is then a detached structure, rather than being adjoined to other buildings. This is a dense, residential part of the Conservation Area and officers consider that the opportunity for disturbance from the use, if not suitably controlled, is potentially severe. Policy DM4 seeks to ensure that development will not cause a significant detrimental impact to the living environment of existing residential properties through, *inter alia*, noise and disturbance. As discussed above, the issues raised in this application need to be considered on their individual planning merits.
- 6.7 The current situation on site is that the large rear extension is considered to be 'substantially complete' although there are multiple areas where concluding and - as this report will go on to explain - remedial works are necessary. Although it is noted that the current extension does not have a planning permission or thus no planning controls on its use, there are no recorded complaints to Planning Enforcement over the use of the site/extension. This is likely due to the fact that whilst there will at times be large numbers in the congregations, the prayer



services are quiet and often, largely silent. However, the size of the congregations, the fact that a PA system is used and that a central air conditioning system is being installed and may (or may not) already be operational; are all areas to consider for control via conditions.

- 6.8 The application includes a noise survey report which assessed the PA noise, break-out noise and noise from the air conditioning plant equipment. This concluded that in all cases, the use of the mosque, as extended did not give rise to amenity concerns. The Council's Environmental Protection (EP) Team advises that the noise assessment has been carried out satisfactorily. Their only recommendation is to ensure that the air conditioning plant is supplied with the necessary acoustic enclosure, as advised in the report, in order to protect the amenity to the nearest noise-sensitive receptor (in this case, the nearest habitable room window at No. 16 Waylen Street). This condition is set out in full in the Recommendation. However, your officers consider that further conditions are required.
- 6.9 Given the wide range of uses which can be covered by the D1 Use Class and various combinations of disturbance, traffic, etc. which such uses can create, whenever granting new planning permissions involving D1, the LPA will normally seek to restrict the use to that which is being applied for. Whilst the D1 use itself is lawful, Members may recall the principle established in the *Harbidge case*, that where the local planning authority is faced with an unauthorised use to which it does not in itself take exception but is aware that a change could take place in its operation it must take enforcement action or seek to suitably control it. Otherwise, if there has been no application for planning permission, there is the risk of uncontrolled, undesirable change. This supports your officers' position that whilst there is no in principle concern for the presence of an extension for mosque purposes, this cannot take place in a completely uncontrolled fashion. Therefore a condition is recommended that this be a D1 mosque only. Officers have also considered the need for a capacity restriction. The Council's Building Control section advises that given the floorspace now provided, ~~an occupancy of~~ some 700 persons could be present on site at any one time. The applicant at the time of the enforcement appeal stated that it was unlikely than more than 300 persons could use the accommodation. In the absence of any other information (for example a fire limit on the premises), officers advise that a capacity of 300 could be covered by the condition which seeks to control the D1 use. This is considered to be a reasonable approach, given the residential area, the instances of disturbance which could be caused through the operation of the mosque itself and any related comings and goings and the increase in floorspace over the previous situation with the Elim Church hall.
- 6.10 Consideration has also been given to the ancillary uses of these premises. Mosques, like many religious buildings or assembly and leisure-type uses, come in various shapes and sizes and their associated functions vary. It is notable that this mosque has been operating for some time now (possibly 20 years) in the community and with relatively few issues of disturbance during that time. This is likely to be because there is no amplified music and the mosque is quiet, with an extensive library, so this is a place which tends to be for serene prayer, rather than loud religious services. Such services occur at all hours and more so during Ramadan (16 May - 14 June this year) and officers have checked with the EP team as to whether any complaints have been received recently and there have been none. At the time of writing, officers are not suggesting the need for an hours of use condition on the site, given that the site has an established use for D1 at all hours and no disturbance appears to have been recorded.

6.11 Attendant issues of sound attenuation, control, air-conditioning etc. need to be properly and comprehensively controlled and given the potential for noise and disturbance arising from the use in the future officers recommend conditions that any other uses, including the basement ablutions area, remain ancillary; and there should be no amplified music or external speakers. Also, the noise report has considered amplified speech. Clearly, if any windows or doors are open, this will be detrimental to neighbours. Therefore Officers suggest a condition requiring that these remain shut during all services.

6.12 With the range of noise-related conditions discussed above, adopted planning policies (CS34, Pollution and Water Resources, DM4, Safeguarding Amenity and DM12, Access, Traffic and Highway-Related Matters) are considered to be complied with.

c) Rear extension: design and impact on neighbours

6.13 The rear extension which has been constructed is unauthorised. In your officers' opinion, the applicant/owner oversaw the construction of this structure and was fully aware of the differences between what was constructed and the 2001 planning permission. There are significant variations in what has been built and the planning permission 00/01355/FUL, i.e. the new floor plans are significantly shorter, the roof shape is higher and angles are different and this affects long lengths of roof, and openings on both flank elevations are very different.

6.14 The applicant's submitted DAS is relatively poor and is essentially a re-working of points made in their statement for the enforcement appeal, so as a consequence it is not comprehensive. The statement and conclusions made in the Inspector's appeal decision letter are important material considerations to be taken into account in the determination of this application for retrospective planning permission. Importantly, the Inspector states that the two storey rear extension which is on site presents a 'new chapter' in the planning history of the site and must thus be assessed on its individual planning merits. The Inspector remarks that '...with regard to outlook, I consider that the extension is excessively large'. He found that it is disproportionate and out of scale with the site's rear curtilage and the height and massing close to the boundaries with Nos. 16 and 20 results in an intrusive and noticeably dominating presence which adversely and unacceptably affects the outlooks of both properties, causing them significant harm. He did not seek to reduce it in size, he decided that it should be removed, as the extant Notice requires.

6.15 However, at this point, it is worth noting why the Notice asked for the wholesale removal of the extension. In short, it is because that was the only option open to your officers. The use could not be stopped, it is established. The Notice could not have asked for the extension to have been reduced back down to some agreed reference point, because there was none. Therefore the Notice concentrated on seeking the removal of the extension and the reinstatement of the front wall (discussion below) and was upheld. With the appeal now having been dismissed, it is up to the Local Planning Authority to look afresh at the development and decide whether this planning application - which was on-going at the time of the Inspector's decision - is now capable of approval. There are two main issues to consider in design terms: whether the design and its impact on the Conservation Area is suitable; and the impact on neighbouring properties.

### *Design and impact on the Conservation Area*

- 6.16 The design consists of a large rearward extension to the frontal buildings and is made up of a wide and long single-storey prayer hall, covering the majority of the site, with a first floor set in from the flank elevations. This is achieved with lean-to roofs on the sides and a simple pitch roof of similar angles at the ridge. The first floor is a smaller galleried/mezzanine space providing a smaller prayer hall, reached by internal staircases from the front and rear. The extension is similar in appearance to that approved in 2001. As can be seen from the photo below, immediate impacts on Waylen Street are extremely limited, given the narrow views possible and the fact that the first floor bulk is set in, behind the frontal buildings. Whilst views from within conservation areas are also important, in this case, the main public view from the rear is a private car park, accessed off Russell Street and the photo below shows this. The stepped gable-end wall is presented immediately on the boundary with the car park, but this was also the intention of the 2001 permission. (The word 'intention' is used because the approved plans in that planning permission included significant errors, as the length of that extension was in fact some five metres longer than the site itself and therefore not capable of construction within the application site in any event).
- 6.17 The design is in a sympathetic style, with brick and slate roofs and reconstituted stone window surrounds in arches and decorative brick details. This is a generally pleasant blend of materials which reflect the majority of the conservation area, whilst signalling the purpose of the building as a mosque. Windows would be adjusted on the flank elevations so as to block them up in a sympathetic style.

### *Impact on neighbouring properties*

- 6.18 The coloured-up section plan (not to scale) at the end of this report attempts to show how the various designs relate to each other. In the background is the Elim Church Hall. In green is the bulk approved in 2001 and in blue is the bulk as built. Officers are aware that the original Elim Church Hall had side-facing windows at close proximity to the neighbouring properties. Some of these may have been obscure glazed. However, the size of the openings in the unauthorised extension, particularly given the ground level change to No. 16, produces an overbearing and overlooking presence, of much greater intensity. The retrospective application initially applied to retain these openings 'as built' but given the appeal decision, these are now proposed to be infilled.
- 6.19 There is a slightly elevated floor level at No. 18 over No. 16 and there is a habitable side/rear room in No. 16 (a dining room) with a side-facing bay window and a large patio this side. In the appeal, the Local Planning Authority's statement offered that in order to be acceptable, the structure either needed to be adjusted (by removing the offending openings) or else be completely removed. The Inspector took the latter, more severe route. Officers have consistently sought to regularise the situation and have not intentionally sought the wholesale removal of the extension and this was the advice given in the pre-application response in 2014, when the extension by that point was erected and weathertight.
- 6.20 It is difficult to accurately present to the Committee the difference in massing between the 2001 permission and what has been built, due to the original plans not being scalable, but officers estimate that the variance in dimensions appears to be under a metre (save for the overall length of the structure, which as discussed above, is significantly shorter such that it now fits on the site). The structure is

indeed bulky when viewed from the properties either side, but not dissimilar in shape and form/massing to the 2001 permission. Officers' primary concern has been for the impact of the side windows.

- 6.21 On the South elevation (towards the garden of No. 20) high-level arched windows are currently situated on the boundary. The applicant's original contention was that the former Elim Church Hall presented itself to this neighbour in a similar manner. However, this is not an acceptable situation in terms of overlooking/presence to a residential garden and at officers' suggestion, the plans now show these high-level arched windows to be carefully bricked up, with a half-brick 'reveal', to provide some relief to this elevation. In terms of the massing, the building runs for 15 metres (the length of the garden) at single storey only - some 3 metres in height - and then the lean-to roof to the first floor gallery and then up to the ridge. There are no side-facing windows in the first floor, but the gallery level is served by four large rooflights facing south, although the roof angle means that these are not particularly noticeable. The extension is to the north of No. 20's garden, so there is no overshadowing concern, although it is accepted that there will be a degree of overbearing.
- 6.22 On the north elevation towards No. 16, the development currently has a number of openings and officers have advised that given the change in ground levels and the slight lay-off to that boundary (about a metre), actual and perceived overlooking to this property is unacceptable and the appeal Inspector agreed. Regarding light levels to No. 16, officers have assessed the light angles with specific reference to the side-facing bay window, which faces south. It appears that the extension as constructed obstructs the light angle from the window-pane of the bay window at around 40 degrees. The Inspector has attached significant harm to this. It is therefore accepted that in terms of location, height and massing of the development, it is visually dominant and overbearing on this property.
- 6.23 Officers consider that there are various matters to consider against this position. Firstly, is the fact that there is a separation distance to No. 16 and this is unusual in this street. Secondly, the bay window is to the side and this is not common. Presumably this was built around the same time as the original No. 18, so at that point, it would have enjoyed an outlook over No. 18's garden. However, the opportunity for doing so would already have been lost by at least WWII, by the time the original Elim Church Hall was constructed.
- 6.24 Officers also consider that the changes are not dissimilar to the situation which existed from the 1940s to approximately 2010 and the changes which have been made as deviations from the 2001 permission, whilst significant, are not considered so harmful as to warrant any draconian measures, such as seeking to reduce the bulk of what has been built; but it is up to the Committee to come to their own conclusions on this. Officers accept the sizeable bulk which has been created, but with the adjustments to remove lateral overlooking, officers are not advising, in this circumstance that any reduction in bulk should be required. As with the South elevation, there are no first floor windows, save for four further rooflights, which afford no overlooking.

d) Loss of the wall and impact on the Conservation Area

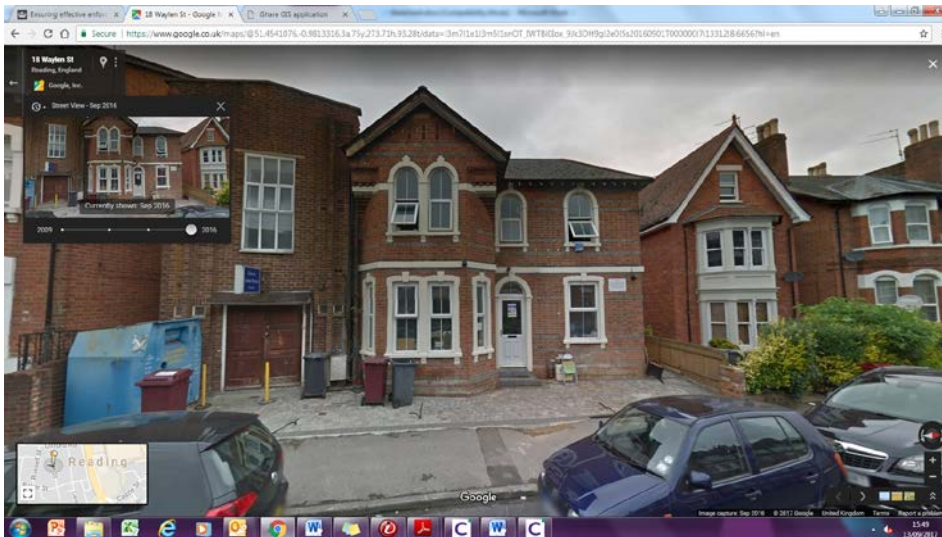
- 6.25 The most obvious concern in terms of the public realm and the Conservation area has been the complete removal of the front boundary wall. Planning permission should have been applied for to remove this wall, as it was over one metre in

height adjacent to the Highway. As can be seen from the Google Streetview photo below, the brick wall was a fairly plain but pleasant and sympathetic boundary treatment within the Conservation Area and such features generally make a positive contribution to streetscene, as in this case. The original building form was a grand house in the street, with its character slightly altered by the addition of 18a to its left; nevertheless, it should be framed by a suitable curtilage (physical boundary). It is accepted that the wall which was removed was non-original.

- 6.26 The Conservation Area Appraisal laments the loss of boundary treatments in this part of the Conservation Area and it is true that in various locations, such boundaries have unfortunately been lost. But as can be seen from the later Streetview photo below (September 2016), the loss of the wall is clearly harmful to the character of the street, through creation of a wide, open frontage, which is neither sympathetic to the character of the building or the streetscene. The building appears to be missing its enclosure (curtilage) to the street edge and this is uncharacteristic of the Conservation Area and the Inspector agreed that there was no convincing reasons for its removal which would outweigh the harm to the Conservation Area and conflict with Policy CS33.
- 6.27 In requiring a suitable replacement, the Enforcement Notice proposed that the boundary wall should be reinstated. Ideally, however, the wall should be a dwarf wall with railings, reflecting the character of No. 18, which was probably one of the grander houses in the street. Sadly, there are relatively few examples of walls and railings in Waylen Street. No. 37 has a very low dwarf wall and railings with Fleurs-de-lys spear-tops. This may have been the style of curtilage at the property. A couple of other properties also have dwarf walls but with simpler 'bow and spear' tops. Officers therefore advise that on the basis that the original style of curtilage is not clear, the proposed plans (as amended) which show a simple reinstatement of the brick wall, are supportable.
- 6.28 Given that the changes do not involve the front of the building itself and the extension has a minimal impact on the streetscene, there is no requirement to provide mitigating landscaping. However, the front courtyard area is small and during the works, a paving scheme has been constructed, to a generally satisfactory standard. However, the re-provision of the wall would be on an area where the applicant has inserted a linear French drain adjacent to the back of the pavement and this would need to be removed. Sustainable drainage to this area would then need to be re-provided. Accordingly a landscaping scheme condition is recommended, to adjust this area.



Streetview photo 2012



Streetview photo 2016

e) Transport

- 6.29 The site is in a sustainable location near the bus route (red route) on Oxford Road and within walking distance of the town centre. Uses such as this can have a wide draw, but the presence of the parking zone is likely to mean that most visitors will need to use the public car parks, such as Chatham Street. It is likely that visitors tend to access the mosque via public transport or by foot.
- 6.30 At the time of writing, the applicant has declined the Highway Authority's requests for further information, citing that they consider that there is no significant difference over the 2001 approval. However, it is material consideration that the 2001 approval has no weight, as confirmed by the appeal Inspector. Officers have no response to these thoughts from the Highway Authority at this time, but advise that subject to conditions for a travel plan and to provide cycle parking (there is currently none), the proposal is considered to be generally acceptable in transport terms and complies with Policy CS4.

f) Equalities and disabled access issues

- 6.31 As Members are aware, in determining this application, the Committee is required to have regard to its obligations under the Equality Act 2010. The key equality protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sexual orientation. The application raises concerns in terms both in terms of disability and gender discrimination. The issue is the first floor mezzanine/gallery level.
- 6.32 The 2001 permission required the inclusion of a lift to the mezzanine level, via a condition. Whilst at various points, the applicant/owner has claimed to be building in compliance with that planning permission, none of the necessary pre-commencement conditions were discharged, including the condition for a lift. Further, it appears that this level of the mosque may be for the use of women only. However, consultation with the Council's Policy Manager indicates that in instances where there is conflict between the workings of a religious organisation and the Equalities Act, the Act shall not take precedence. Nonetheless, the applicant has been asked to provide further clarification on how their policy on gender and disability matters is organised on the premises and this will be explained at your meeting, then officers will advise further. For the moment, however, officers have not identified a conflict with policies CS3 or CS5 or the Act.

#### Other matters

- 6.33 The construction quality of the extension appears to be generally reasonable, although inspections from RBC Building Control are on-going. The development does not therefore currently benefit from either Building Regulations approval or fire safety approval. Although these are not planning considerations, an informative reminding the owners of this is advised. Berkshire Archaeology's response is noted and nothing further is recommended.
- 6.34 The application has been submitted with a BREEAM Pre-estimator which proposes a low level to be achieved of 30%/'Pass'. This is a Minor level development, but officers consider that Part L of the Building Regulations must be achieved. A relevant condition is therefore recommended. This is considered to be a reasonable requirement and should not prove overly onerous, although it may involve retrofitting as necessary.

## 7. CONCLUSION

- 7.1 This planning application has been submitted as a result of the serving of a Planning Enforcement Notice. In its original form, the application was not considered to be acceptable and has since been adjusted following the decision to uphold the Notice.
- 7.2 The Inspector afforded no significance to the 2001 approval, but it is noted in the report above where relevant and Members will need to consider whether they wish to adopt a consistency of approach on the relevant matters, for instance, in considering matters of bulk and massing.
- 7.3 The serving of the Notice was, unfortunately, the last resort, officers having thoroughly exhausted all efforts at trying to reason with the owners and urging them to submit a retrospective application, which officers have continually advised that if the correct information is supplied, they would like to be able to support.



- 7.4 Officers are prepared, on balance and for the reasons above, to recommend the granting of retrospective planning permission, but with a range of carefully-worded planning conditions.
- 7.5 Were you to agree to grant permission, this becomes a situation where the planning conditions could be enforced in the normal manner, via a Breach of Condition Notice (BCN). Were you to refuse permission, the applicant could reapply and secure full compliance with any further approval and undertake such necessary works before the Enforcement Notice deadline of March 2019. Otherwise, the Notice takes Effect, which means that the Local Planning Authority would then be able to apply to the Courts for an Injunction to seek compliance with the Notice requiring that the extension be demolished in totality and the wall rebuilt.

Case Officer: Richard Eatough

Plans:

786/WA/SK - 100 B PROPOSED GROUND FLOOR PLAN IN CONTEXT

786/WA/PP - 100 BASEMENT GENERAL ARRANGEMENT PLAN (received 8/3/18)

786/WA/PP - 100 PROPOSED GROUND FLOOR PLAN AMENDMENTS (received x)

786/WA TI - 541 C WORKING DRAWING: PROPOSED 1<sup>ST</sup> FLR. PLAN FOR PHASE 3 (received x)

786/WA/TI - 543 C WORKING DRAWING: SECTION Y-Y FOR PHASE 3 [and Rear Elevation] (received 16/10/17)

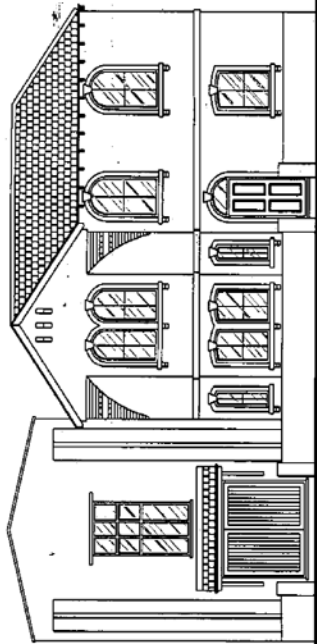
786/WA/PP - 101 PROPOSED SIDE ELEVATION AMENDMENTS (received x)



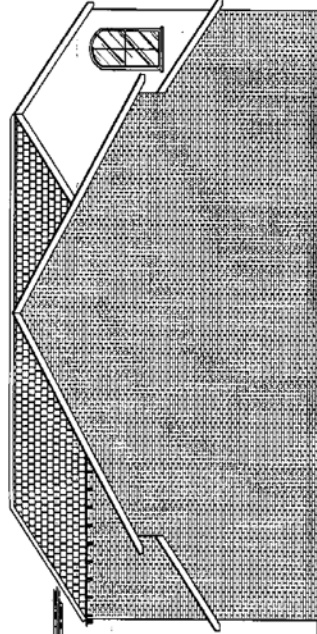
Site in current view from Waylen Street. Unauthorised extension is just visible in the red circle. Front boundary wall is missing.







FRONT ELEVATION (prop).



REAR ELEVATION (prop).

READING DISTRICT COUNCIL			
COUNCIL RESOLUTION			
DATE	BY	FOR	PH
1 JULY 2000			
LS	HS	SS	

**The KEEN Partnership**  
 Culham House, 122 Castle Street  
 Reading, Berks RG1 7RJ

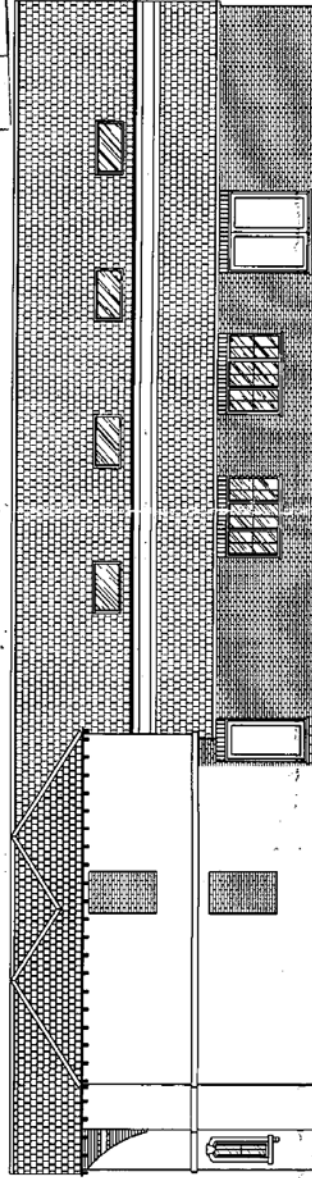
Drawn by: **M. SAOOD ESQ.**

Site Title: **Central Jamme Mosque,  
 18A Wayfen Street,  
 Reading.**

Prepared by: **PROP. ELEVATIONS.**

Scale: **1/100.** A3  
 Date: **July, 2000.** Drawn by: **P.J.M.**  
 Drawing No: **7242-00-04.**

PROJ. TEL: 0118 9510855 FAX: 0118 9510866  
 E-mail: info@keempartnership.co.uk



SIDE ELEVATION (prop).

2001 planning permission plans (elevations)



Comparison section plan (not to scale, officer estimates, for information):  
 Black outline: original Elim Church hall  
 Green outline: 2001 planning permission  
 Blue outline: current planning application



## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT AND NEIGHBOURHOOD SERVICES  
READING BOROUGH COUNCIL  
PLANNING APPLICATIONS COMMITTEE: 27<sup>th</sup> June 2018

ITEM NO. 9

Ward: Abbey

App No.: 172259

App Type: FUL

Address: Mercedes Centre 28-30 Richfield Avenue

Proposal: Extension of existing Mercedes-Benz showroom on Richfield Avenue with two smaller buildings erected purely for valet and MOT services. External alterations to building to include cladding and glazing to the showroom and site facing workshop areas. Re allocation of parking, resurfacing of the site, new boundary treatments, a dedicated covered service drop off area installed.

Applicant: Vertu Motors PLC

Date valid: 19/01/2018

Major Application: 13 week target decision date: 20/04/2018

Agreed Extension of time date: 31<sup>st</sup> July 2018

### RECOMMENDATIONS

Delegate to the Head of Planning, Development and Regulatory Services to GRANT Full Planning Permission, subject to the satisfactory completion of a S.106 legal agreement or (ii) to REFUSE permission should legal agreement not be completed by 27th July (unless the Head of Planning, Development and Regulatory Services agrees to a later date for completion of the Undertaking).

The S106 legal agreement to secure an Employment, Skills and Training Plan relating to the construction and operational phases of the development, or equivalent financial contribution, to be submitted to the Council at least one month prior to development commencing, in accordance with the requirements of the adopted Employment, Skills and Training SPD (2013).

### CONDITIONS TO INCLUDE:

1. Time limit - 3 years
2. Development to be carried out in accordance with approved drawings.
3. Details and samples of all external to be submitted and approved (including acoustic fence)
4. Details of design of fencing and height to be submitted.
5. Soft landscaping and tree pit specification in accordance with plan.
6. Standard landscaping maintenance.
7. Arboricultural method statement to be followed.
8. Submission of Construction Management Statement (highways, noise, dust and burning of waste) prior to commencement.
9. No development shall take place until sustainable drainage system undertaken in accordance with the approved details.
10. No development shall take place until a detailed sustainable drainage design has been submitted in accordance with the agreed run off rate reduction along with details of the implementation, maintenance and management plan of the sustainable drainage scheme have been submitted to and approved by the Local Planning Authority.
11. Hours of Working restriction
12. Travel Plan to be submitted
13. Noise assessment for mechanical plant before installation
14. The development hereby permitted shall not be occupied until all vehicle parking space(s) have been provided in accordance with the approved plan. The space(s) shall be kept available for parking at all times thereafter.

15. There shall be no obstructions of the parking spaces, access routes within the development and such areas shall not be used for any other purpose other than that shown on the approved plan.
16. All deliveries to the site including deliveries made a vehicle car transporter shall be undertaken in accordance with the tracking diagrams (Drawing no. 1871-100 P1). Development shall take place in accordance with the approved details unless otherwise agreed in writing by the local planning authority.
17. Details of secure, covered and lockable bicycle storage to be submitted for approval prior to occupation and provided, as approved prior to occupation.
18. Development shall not be occupied until all areas shown on the approved plans to be used by vehicles have been fully laid out, surfaced and drained such that surface water does not discharge or transfer onto the highway. These areas shall not be used for any other purpose thereafter.
19. Development as built to meet BREEAM Very Good standard with minimum score of 62.5 points. Post construction review demonstrating compliance with BREEAM Very Good standard with minimum score of 62.5 points to be submitted for approval prior to occupation.

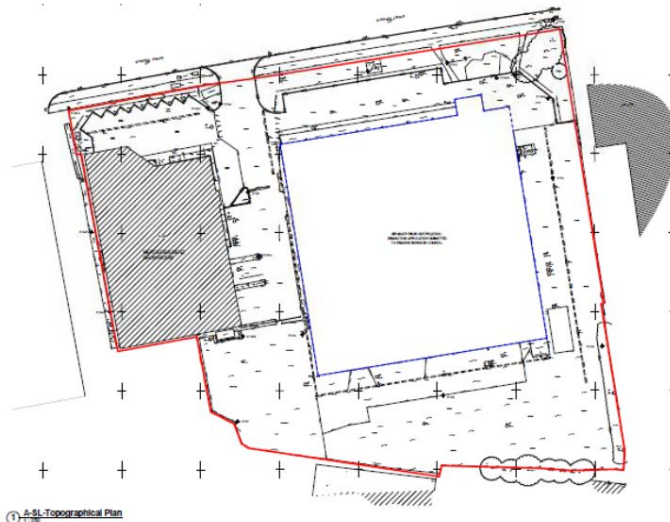
**INFORMATIVES TO INCLUDE:**

1. Terms and conditions.
1. Building control approval.
2. Pre-Commencement conditions
3. Environmental Protection advice
4. No other tree works approved
5. Record of adherence to approved AMS
6. Section 59 of highways act 1980
7. Works affecting highway
8. Separate advertisement consent
9. S106
10. Positive and proactive.

**1. INTRODUCTION**

- 1.1.1 The application site relates to the two car show rooms (Mercedes and Smart Car Dealership and Greenoaks Performance Cars) located on the southern side of Richfield Avenue. The site address incorporates 26 and 28-30 Richfield Avenue. Accordingly, part of the site (No.26 Richfield Avenue) is located within the designated Richfield Avenue Core Employment Area, whilst the other part of the site (No's 28-30) is outside of the area, but borders it.
- 1.1.2 The site is approximately 1.2 Ha and is bordered by Richfield Avenue to the north, flats at Caversham Place to the east, housing on Randolph Road and Denbeigh Place to the south-east and other business units to the west. Richfield Avenue itself is characterised by industrial and large scale commercial uses.
- 1.1.3 There are also protected trees to the north and east of the site. The site is within an air quality management area, potentially comprises contaminated land and is within Flood Zone 2 as designated by the Environment Agency.

## Location Plan (not to scale)



### 2.0 PROPOSAL AND SUPPORTING INFORMATION

- 2.1 The proposal is to extend and refurbish the Mercedes car dealership garage. The proposals also include the erection of a detached single storey valet building in place of 28-30 Richfield Avenue, reallocation of parking, resurfacing of the site, installation of new service drop off area and associated hard and soft landscaping.
- 2.2 Car Parking will be provided on-site for a total of 308 vehicles including a total of 31 customer parking spaces, 13 demonstrators, 84 used display and 180 service and storage spaces. Additional display vehicle spaces will be provided within the main building in addition to 9 service drop off spaces.
- 2.3 No change of use is to take place.
- 2.4 A more detailed soft landscaping proposal has been submitted, following discussion with officers, and now includes 5 trees to be planted alongside Richfield Avenue.
- 2.5 The following plans and supporting information have been considered:

Site Location Plan A 0100 S2-P2  
Topographical Survey A 0101 S2-P2  
Site Plan - Overall A 0102 S2-P3  
Valet Building Plan A 0112 S2-P2  
MOT Building Plan A 0113 S2-P2  
First Floor GA Plan A 0110 S2-P4  
Ground Floor GA Plan 0110 S2-P3  
Roof Plan GA Plan A 0110 S2-P3  
Context Site Sections A 0117 S2-P2  
Building Sections A 0118 S2-P4  
West and South Elevations A 0116 S2-P3  
East and North Elevations A 0115 S2-P3

First Floor Greenoaks Limited 9575.0003 (Existing)  
Ground Floor Greenoaks Limited 9575.0002 (Existing)  
Roof Plan Greenoaks Limited 9575.0004 (Existing)  
Elevations Greenoaks Limited 9575.0005 (Existing)

Planning Supporting Statement  
Design and Access Statement

Received 15<sup>th</sup> December 2017

Vehicle Tracking Car Transporter 1871-100 P1  
Vehicle Tracking Car Transporter 1871-101 P1

Supporting Letter re Transport/Employment Skills and Training/Sustainable  
Drainage and Natural Environment  
Received 25<sup>th</sup> May 2018

Proposed Site Plan A 0201 D5-P7  
Received 6<sup>th</sup> June 2018

General Arrangement Valet and Wash Building Elevations and Sections A 0331 D2-  
P3  
General Arrangement MOT Building Ground Floor Plan Elevations and Sections A  
0332 D2-P3  
Received 13<sup>th</sup> June 2018

Arboricultural Survey, Protection Plan and Method Statement 18212055/14/2018  
Received 14<sup>th</sup> June 2018

### 3. PLANNING HISTORY

- 3.1 172095 Application for prior notification of proposed demolition. Prior Approval  
12/04/2018

### 4. CONSULTATIONS Environment Agency

- 4.1 No response received. Any response received prior to the Committee meeting will  
be reported in an Update Report.

#### Lead Flood Authority (RBC Highways)

- 4.2 Further to revised information received, no objection subject to conditions.

#### RBC Natural Environment (Trees and Ecology)

- 4.3 Further to revised plans and submission of Arb report, no objection subject to  
conditions and informatives.

#### RBC Transport Development Control

- 4.4 The applicant was requested to submit a Transport Statement to assess the transport  
implications the development compared to the existing permission on the site.  
However, this document has not been included with the application submission. In  
terms of trip generation, the trips generated by the former car showroom will be lost  
as a result of the redevelopment of the site, therefore, proposed extension of the  
Mercedes-Benz showroom and valet and MOT services will not significantly increase  
trips above and beyond the current permitted use. In view of this, I am happy to  
assess the application without a Transport Statement.

#### Parking

The site is located within the Zone 2, the primary core area but on the periphery of  
the central core area which lies at the heart of Reading Borough, consisting primarily  
of retail and commercial office developments with good transport hubs. In  
accordance with the adopted Parking Standards and Design SPD, the development  
would be required to provide the following parking provision;

Land Use	Zone 2
Vehicle Sales	1 space/ 2 FTE staff + 1 space/ 10 display cars**



Vehicle Servicing	1 space/2 FTE staff + 3 per service bay
Vehicle Parts	1 space/ 2 FTE staff + 1 space per 50 sqm

\*\* This refers to parking for customers, display vehicles are not included in this provision, and would fall under sales space.

The Design and Access Statement states that current dealership directly employs 63 staff of which 11 are part-time positions. The existing parts department will be moved off site into a dedicated facility and will continue to operate with the existing staff resulting in the loss of 14 existing employees on the Richfield Avenue site. However, it is expected that the dealership will grow over the next 3-5 years to a total of 87 employees. 68 full time and 19 part time employees. Therefore, the proposed parking provision should meet this anticipated growth.

Car Parking will be provided on-site for a total of 308 vehicles including a total of 31 customer parking spaces, 13 demonstrators, 84 used display and 180 service and storage spaces. Additional display vehicle spaces will be provided within the main building in addition to 9 service drop off spaces.

Assuming the future growth of 68 full time and 19 part time employees (78 FTE), the development should provide 39 staff parking spaces. The proposed site plan (MER01-MCB-SL-SL-DR-A-0201-D5-P6) designates 39 spaces for staff parking which complies with the Council's standards and is acceptable.

In terms of customer parking, the Council's adopted standards requires 10 spaces associated with vehicles sales (new & used) and 36 spaces associated with vehicle servicing. The site plan allocates a total of 31 customer parking spaces plus an additional 9 service drop off spaces. Therefore, I am happy with the proposed customer parking provision.

As the existing parts department will be moved off site into a dedicated facility, I have not assessed the parking provision associated with this use.

#### Access & Servicing

The main vehicular access point to the site is via the bell mouthed access off Richfield Avenue. A secondary access point is taken from Milford Road where there is an existing gated access into the site.

The Design and Access statement includes diagrams to demonstrate that there a multiple options for accessing the site by a vehicle transporter via either from Richfield Avenue or the rear compound entrance off Milford Road. This will ensure no deliveries are undertaken from Richfield Avenue.

Gates are to be provided within the site boundary to the Richfield Avenue entrance for site security. The gates are positioned to ensure that a standard vehicle can pull off the highway and allow an operative to unlock and open the site. During operational hours the gate will remain fully open.

#### Cycle Parking

Cycle parking provision should be provided within the site at a ratio of 1 space per 6 FTE staff and 1 space per 20 display cars (vehicles sales). Applying these standards to the proposed staff numbers and display cars, the car dealership is required to provide a minimum of 18 cycle parking spaces to comply with the adopted standards, a total of 9 'Sheffield' type stands will be provided. It is not clear whether these spaces will be provided under a covered shelter but I am happy to deal with this by condition.

## RBC Environmental Protection

### 4.5 No objection subject to conditions and informatives

#### Public Consultation

4.4 Neighbours adjoining the site were consulted by letter and two site notices were displayed. Two neighbour letters of representation received, neither objecting to the proposal but commenting on:

- Hours of work
- Retention/replacement of trees
- Details of acoustic fence

## 5. RELEVANT PLANNING POLICY AND GUIDANCE

5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that proposals be determined in accordance with the development plan unless material considerations indicate otherwise. Material considerations include relevant policies in the National Planning Policy framework (NPPF) - among them the 'presumption in favour of sustainable development'.

5.2 The following national and local planning policy and guidance is relevant to this application:

#### Relevant Policies:

##### National Planning Policy Guidance

National Planning Policy Framework (NPPF)

##### Reading Borough Local Development Framework Core Strategy Document (2008, altered 2015).

Policy CS1 (Sustainable Construction and Design)

Policy CS2 (Waste Minimisation)

Policy CS7 (Design and the Public Realm)

Policy CS9 (Infrastructure, Services, Resources and Amenities)

Policy CS10 (Location of Employment Development)

Policy CS11 (Use of Employment Land for Alternative Uses)

Policy CS20 (Implementation of Reading's Transport Strategy)

Policy CS22 (Transport Assessments)

Policy CS23 (Sustainable Travel and Travel Plans)

Policy CS24 (Car/Cycle Parking)

Policy CS34 (Pollution and Water Resources)

Policy CS35 (Flooding)

Policy CS38 (Trees, Hedges and Woodland)

##### Reading Borough Local Development Framework: Sites and Detailed Policies Document (2012, altered 2015)

Policy SD1 (Presumption in Favour of Sustainable Development)

Policy DM1 (Adaption to Climate Change)

Policy DM3 (Infrastructure)

Policy DM4 (Safeguarding Amenity)

Policy DM12 (Access, Traffic and Highway-Related Matters)

Policy DM18 (Tree Planting)

Policy DM19 (Air Quality)

Policy SA12 (Core Employment Areas)

##### Supplementary Planning Guidance/Documents

Revised Parking Standards and Design (2011)

Revised Sustainable Design and Construction (2011)

6. APPRAISAL

6.1 The main issues to be considered are:

Principle of Development  
Transport and Accessibility  
Residential Amenity  
Environmental Effects  
Design & Appearance  
Natural Environment/Ecology  
Flood Risk/SuDS/Sustainability  
S106 Matters

Principle of Development

6.2 Part of the site, 28-30 Richfield Avenue is allocated for residential use in the emerging local plan. As this has not yet been adopted, it does not currently hold significant weight. It should be noted that this building has prior approval for its demolition.

6.3 Part of the site 26 Richfield Avenue is located within the designated Richfield Avenue Core Employment Area under Sites and Detailed Policies Document Policy SA12 (SA12g: Richfield Avenue). Core Strategy Policy CS11 explicitly states that within the Core Employment Area, the overall level of employment land should be maintained. Proposals that would result in a loss of such uses will not be permitted.

6.4 Whilst the proposals would see the loss of the building 28-30 Richfield Avenue, 68 full time and 19 part time employees are proposed, compared to the previous number of 63 full time and 11 part time employees.

6.5 There are also wider employment related benefits from the proposals as the company provide apprenticeships and graduate scheme.

6.6 Given the above, and that the use would remain the same, and that the building in the Core Employment Area itself has prior approval to be demolished, it is not considered that the proposals would result in any detrimental effect on the ongoing employment use of the site as a whole, nor set a precedent.

Transport and Accessibility

6.7 The comments of the Council's Transport section are set out above in detail in Section 4. It is considered that this is an appropriate assessment of the transport impacts of the proposal. The conditions suggested by Transport are considered to be necessary to make the development acceptable and are therefore included in the recommendation. It is considered that the proposal complies with Policies CS20 and CS24 of the Core Strategy and Policy DM12 of the Sites and Detailed Policies Document on this basis.

Residential Amenity

6.8 26-30 Richfield Avenue has prior approval for its demolition. The building proposed as part of this application in its place, would be considerably smaller in scale (height and footprint) and set further away from the boundaries with residential properties - indeed, the proposals move the working activities away from the residential boundaries. The acoustic fence would replace that which already sits in

the current location and the new fence would match the acoustic specification of the current version.

- 6.9 The Council's Environmental Protection Officer has assessed the proposal and recommended appropriate conditions regarding the control of noise from mechanical plant, control of dust, noise and working times during construction period. With such conditions in place the proposed development is considered acceptable in planning terms and in accordance with Policy CS34.

#### Environmental Effects

- 6.10 The site is located within an air quality management area. The proposals seek to mitigate against increase emissions including implementation of a travel plan, which can be secured via condition, inclusion of electrical vehicle charging point and provision of cycling facilities. These elements are considered to be beneficial to air quality and acceptable in terms of Policy DM19.
- 6.11 The development lies on the site of historic 'works' sites, and in the 250m buffer zone of a former landfill site (Richfield Avenue). Due to the nature of the historic land use there is the potential of contaminated land being present. The developers must be aware that any ground or enabling works may bring receptors into contact with the aforementioned contaminants. An informative will be attached stating that the applicant should satisfy themselves that the details of the construction proposals take the necessary account of the possibility of contaminants from that source.

#### Design & Appearance

- 6.12 The site is located within a prominent part of the road, with the existing buildings and parking areas directly fronting the pavement. 26-30 Richfield Avenue has prior approval for its demolition. The proposed valet and wash building would be small in scale and set further back in the site than the existing, resulting in a less prominent feature of the street scene. The proposed external alterations to the Mercedes Benz showroom would be acceptable in size, scale and appearance and a more contemporary design of the building is acceptable, subject to good quality materials, final details of which could be secured by way of a suitably worded condition. The proposed design of the cladding and proposed external alterations would be seen within the context of the site and are acceptable forms of development on such uses in and such context of light industrial and commercial sites. The angular forms of the new additions are intended to create a clean modern look to the building that reflects the premium quantities of the franchise brand. The resulting appearance would improve the building which whilst not unattractive in the realms of its purpose would be updated by the proposals and introduce visual interest on this part of Richfield Avenue.
- 6.13 The proposals to both the main building and the proposed valet building are considered suitable in scale and appearance and considered to be appropriate within their context. Indeed, the proposals are considered to be acceptable given the siting of the proposal, its relationship with neighbouring industrial units and the subservient role of the valet building in relation to the main showroom building.
- 6.14 Whilst signage is indicated, this would be subject to a separate advertisement consent application.
- 6.15 On this basis it is considered that the proposals would be in accordance with Policies CS7 and CS8 of the Core Strategy which relate to the effect on the character of the area.

#### Natural Environment/Ecology

- 6.16 Following initial concerns raised by the Council's Natural Environment Officer regarding the amount of soft landscaping proposed, revised plans with further clarification was submitted showing increased tree planting that has been assessed and considered acceptable. Subject to compliance with appropriate landscaping conditions the proposal is considered acceptable.
- 6.17 The Council's Ecologist has confirmed that the existing showroom is unlikely to be suitable for use by roosting bats - it is glass fronted with an open roof void and the eastern half of the building is flat-roofed and the works are unlikely to directly affect bats or other protected species. It is noted that a bat survey was submitted under application 172095 for the demolition of the building which stated that further to inspection of the building, no bats or signs of bats were found. The Ecologist considered that the risk of bats being affected by the proposals was minimal and raised no objection. There is no objection to the application on ecological grounds.
- 6.18 On this basis it is considered that the proposals comply with Policies CS7, CS8, CS36 and CS38 of the Core Strategy and Policy DM18 of the Sites and Detailed Policies Document.

#### Flood Risk/SUDS/Sustainability

- 6.19 The National Planning Practice Guidance recommends that "when applying the Sequential Test, a pragmatic approach on the availability of alternatives should be taken. For example, in considering planning applications for extensions to existing business premises it might be impractical to suggest that there are more suitable alternative locations for that development elsewhere." In this instance it the proposals relate to the extension and refurbishment of existing buildings and therefore there are very limited options for alternative siting. The whole site falls within Flood Zone 2 and as such the Sequential Test is considered to be passed on the basis that there are no reasonably available alternative sites.
- 6.20 The proposals do not involve a change of use and do not involve a significant alteration in the position of buildings relative to existing routes into and out of the site. The proposals would therefore have little impact in terms of the ability of persons within the site to escape, or the ability of emergency services to access the site in the event of a flood.
- 6.21 The submitted Flood Risk Assessment concludes that "the site is effectively on the edge of the floodplain of the River Kennet but is elevated above the main extent of inundation. The site is mapped as having an annual probability of flooding from rivers is between 1 in 100 and 1 in 1000, confirming its categorisation as flood zone 2. Climate change is expected to increase flood risk with time as rainfall intensity and volume increases". The FRA concludes that this would be a modest increase over the 30 year predicted lifetime of the development.
- 6.22 The proposal remains a 'Less Vulnerable' use in terms of national flood risk policy and it is considered that the proposal would not result in a change in the nature of the use which would result in an increased risk to life or property within the site from flooding.
- 6.23 In terms of SuDs, officers are satisfied with the revised information submitted during the course of the application, and it is considered that the proposals would not worsen the existing situation. This is subject to a pre-commencement condition for a final implementation, maintenance and management plan, as well as the measures proposed being implemented prior to the building being occupied.

- 6.24 Policy CS1 requires Major development such as the current proposal to meet the BREEAM 'Excellent' and 'Very Good' standards with at least half the development achieving 'Excellent'. The Council's Sustainable Construction and Design SPD explains that for practicality this is best achieved in many cases by securing an average score of halfway between Excellent and Very Good, which equates to 62.5 BREEAM points. A condition is recommended to secure this.
- 6.25 Sustainable design details have been provided as part of the application which includes the following:
- Building adaptability to enable flexible and modern design to enhance life span
  - Materials selected for their expected life cycle
  - Light weight construction
  - Building and facilities design to enhance well-being of staff and public
  - Good accessibility
  - Travel plan
  - Electrical vehicle charging points
  - Large areas of glazing to optimise daylighting
  - Lighting with energy efficient controls
- 6.26 This is considered to be reasonable and commensurate with the scope of the proposals which are fundamentally extensions to existing buildings.

#### S106 Matters

- 6.27 The proposal is classified as a Major development and as such the requirements of the Employment Skills and Training SPD (2013) apply. An Employment Skills and Training Plan is required to relate to both the construction phase and the ongoing operation of the site. The applicant agrees to the principle of an ESP and has been in discussions with the council with regard to the requirements. It is recommended that the detailed plan, or any appropriate financial contribution in lieu of a plan are secured by S106 agreement, to be finalised once full details of the construction process and operation of the development are known.

#### Equality

- 6.28 In determining this application the Council is required to have regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sexual orientation. There is no indication or evidence (including from consultation on the current application) that the protected groups have or will have different needs, experiences, issues and priorities in relation to this particular planning application.

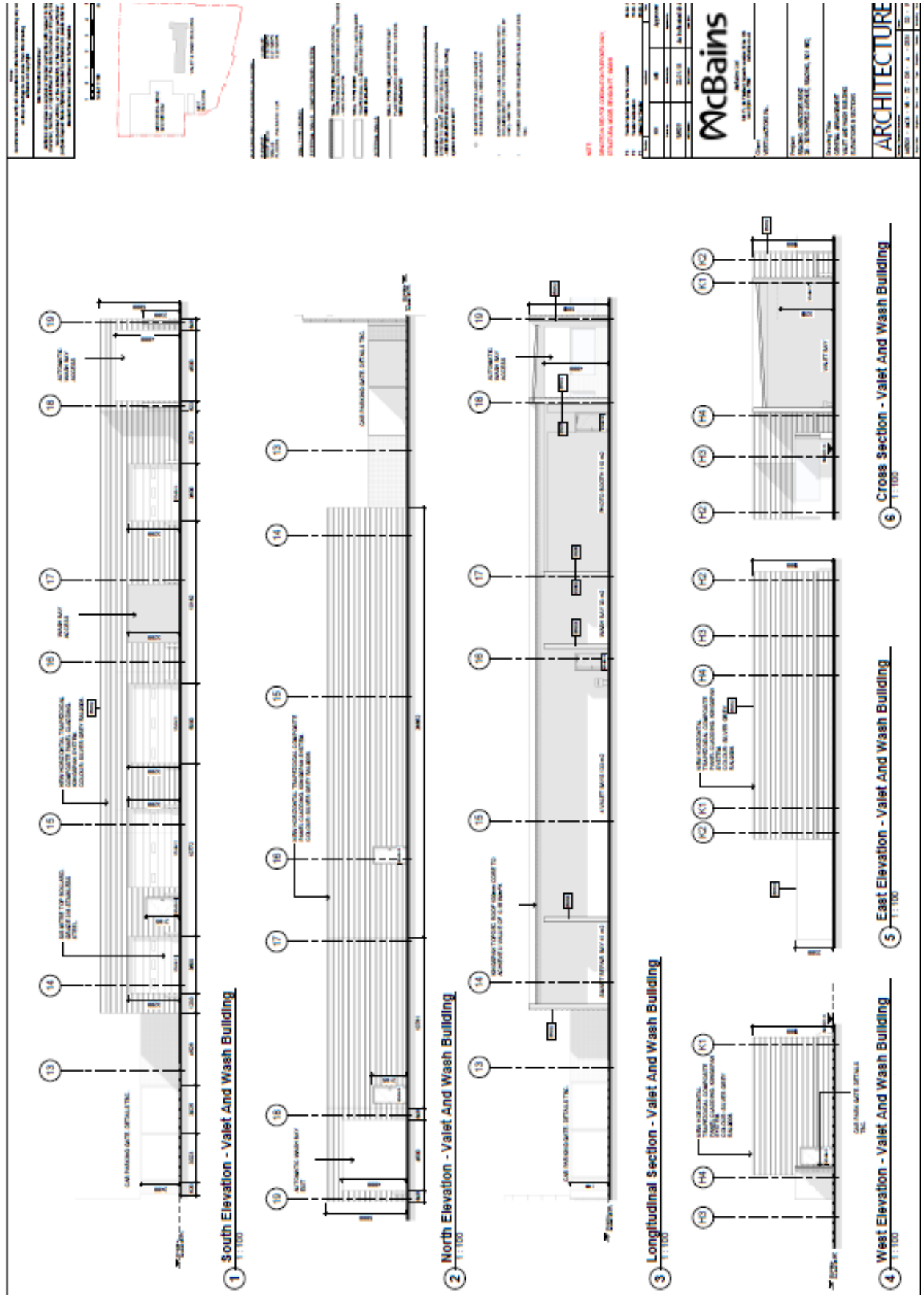
## 7. CONCLUSION

- 7.1 It is considered that the development would be acceptable for the reasons set out above. The proposals are recommended for approval, subject to appropriate conditions and subject to completion of the S106 legal agreement.

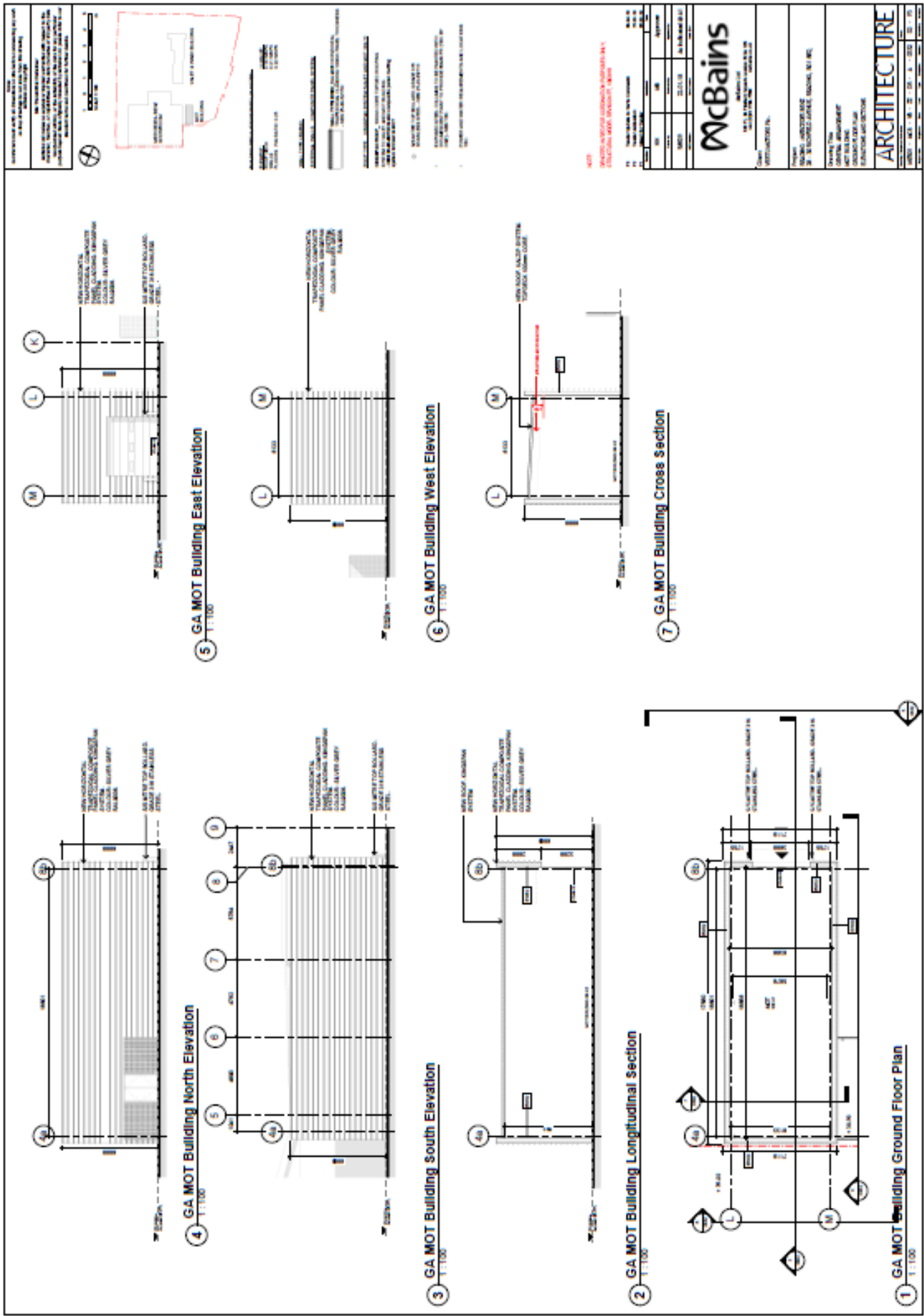
Case Officer: Ethne Humphreys



Proposed Valet Building



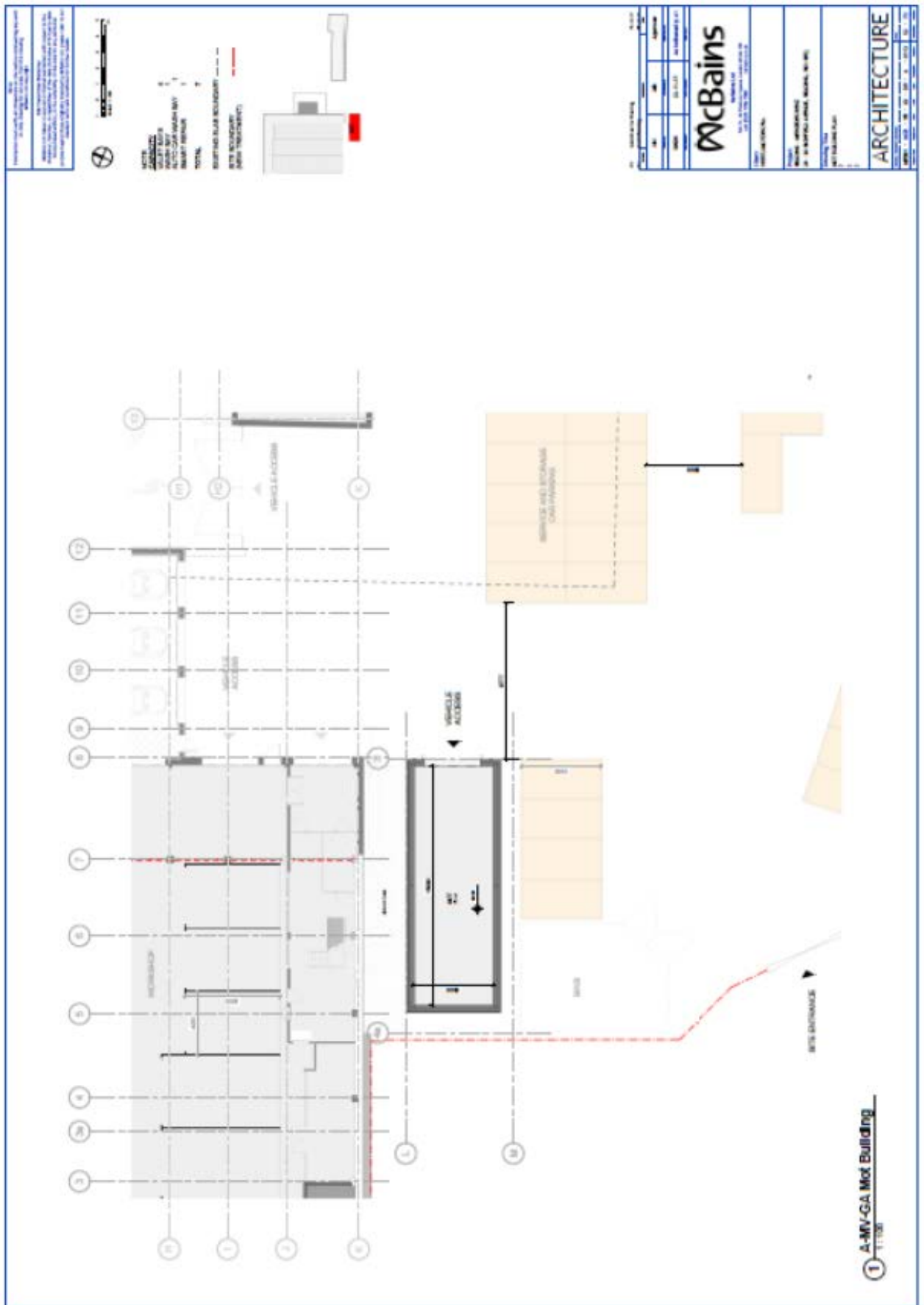




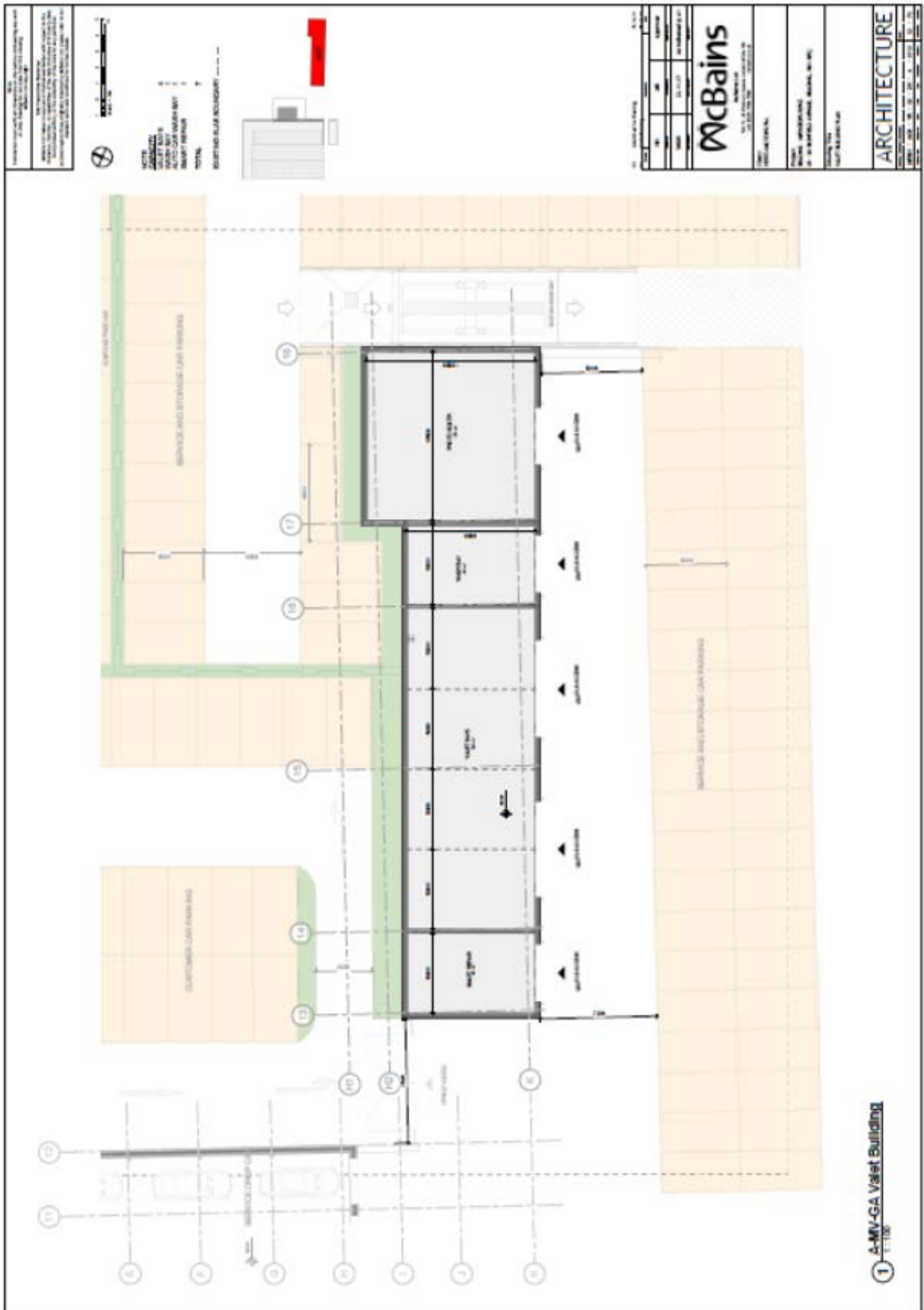
Proposed MOT Building







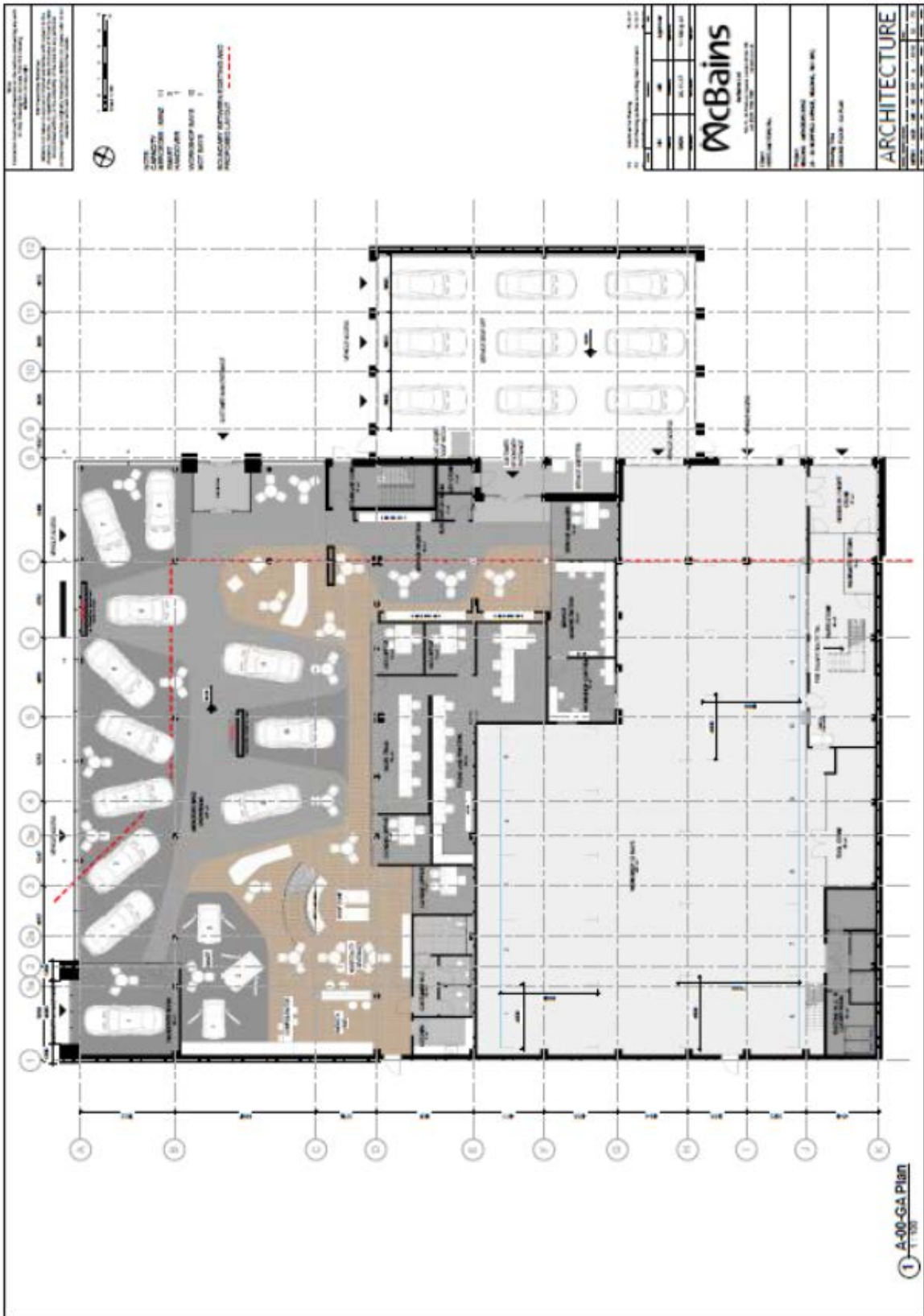
Proposed Floor Plan MOT Building



Proposed Floor Plan Valet Building







Proposed Ground Floor Plan

## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT AND NEIGHBOURHOOD SERVICES  
READING BOROUGH COUNCIL  
PLANNING APPLICATIONS COMMITTEE: 27<sup>th</sup> June 2018  
ITEM NO. 10

Ward: Abbey  
Application No.: 180739/NMA  
Address: 114-116 Oxford Road, Reading, RG1 7NE

Proposal: Non-material amendments to permission 150721 [Erection of part 4, part 5 storey building providing 16 (2x1, 13x2 & 1x3-bed) residential units (Class C3) with associated parking and landscaping, following demolition of existing buildings (Class A1 / B8 / nil use) (amended description)], namely to raise the height of the building by 0.5m, to omit the mezzanine floor to flats 13 and 14 and change the configuration of the front entrance door to the flats.

Applicant: Fynecast Ltd  
Date Valid: 02/05/2018  
Application target decision date: Originally 30/05/18, but an extension of time has subsequently been agreed with the applicant until 29/06/2018.  
26 week date: 31/10/18

### RECOMMENDATION

AGREE the non-material amendment to permission 150721 subject to the following condition:

1. The drawings hereby approved as non-material amendments to 150721 are as follows:

114:18:401 Rev B - Ground Floor, as received 02/05/18  
114:18:402 Rev B - Third Floor, as received 02/05/18  
114:18:403 Rev B - Front & Rear Elevations, as received 02/05/18  
114:18:404 Rev B - Side Elevations, as received 02/05/18  
114:18:405 Rev B - Inward Facing Rear Elevations, as received 02/05/18  
114:18:406 Rev A - Street Scene, as received 02/05/18  
114:18:408 - Mezzanine Plans & Sections, as received 02/05/18  
114:18:410 Rev A - Block & Location Plans, as received 02/05/18  
114:18:411 - Roof Plans, as received 02/05/18  
114:18:412 - Proposed Building Sections, as received 02/05/18  
114:18:413 Rev A - Block Plan, Drainage & Landscaping, as received 02/05/18

Reason: To clarify which plans are approved and in the interests of proper planning.

Informatives:

1. This permission relates only to the changes highlighted on the plans. Furthermore it shall only be read in the context of the planning permission 150721 and is bound by all the conditions and obligations attached to that permission. That permission still stands and this notice (and any other notices issued under section 96a for this development in the future) should be read together.

2. In reaching this decision, the local planning authority is satisfied that the changes proposed by this application do not constitute material changes to the approved planning



permission.

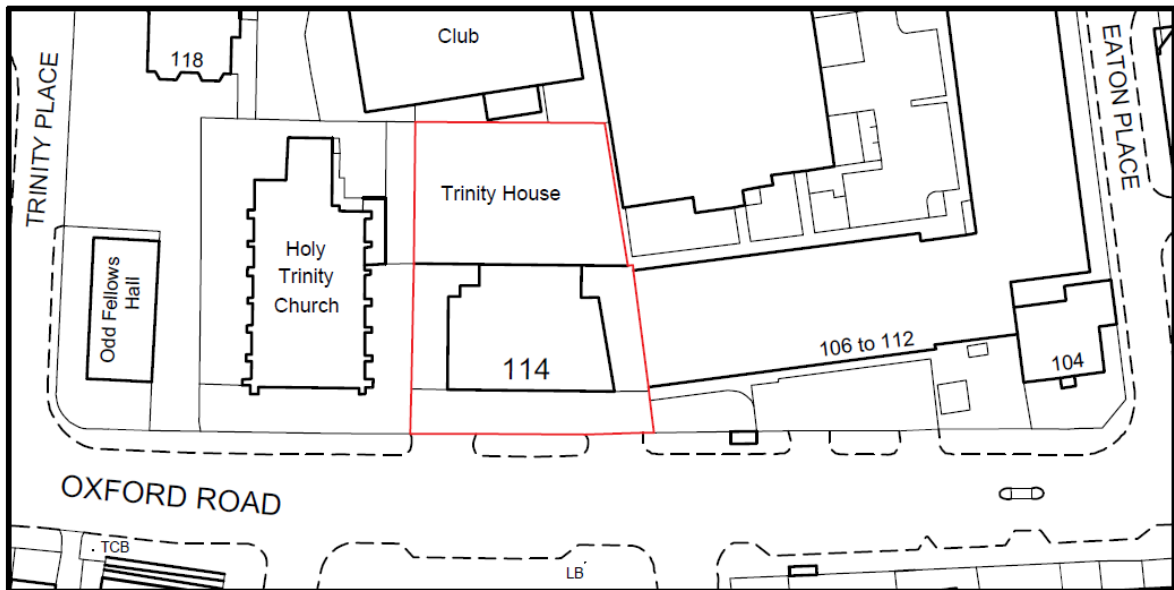
3. The Local Planning Authority has worked positively and proactively with the applicant to determine under the non-material amendment procedure within the agreed timeframes.

## 1. INTRODUCTION

- 1.1 The application site is located on the north side of Oxford Road, in-between the junctions with Eaton Place to the east and Trinity Place to the west. Based on an unaccompanied officer site visit on 31/05/18 it is evident that the site is vacant, with demolition nearing completion at that time. Until recently it comprised two buildings, one fronting Oxford Road and the other to the rear of the site. The Oxford Road fronting building was basement and three (ground, first and mezzanine floor level) storey building, with a formerly vacant retail use at basement, ground and part first floor level. Until August 2015 this unit was occupied by Richer Sounds. A front forecourt area provided some existing off-street parking facilities. Accessed from both side elevation access points were two former residential units, located at first and mezzanine floor level. It was apparent from the officer site visit on 16/09/15 (at the time of original application 150721) that the units had not been habitable for many years and were in a very poor state of disrepair. It is understood that the units had not been occupied since the early 1980s and were therefore considered to be an abandoned (nil) use in planning terms (at the time of original application 150721). The building to the rear is a single storey (but double height) warehouse building which the then applicant detailed at the officer site visit on 16/09/15 had been vacant since 2008. Previously the building had been occupied by Funeral Directors.
- 1.2 This application site is located within the designated Reading Central Area. Although located outside of the primary shopping area, office core and central core, the site does form part of a wider site designation within the Reading Central Area Action Plan. As part of site RC4a, which consists of 108-116 Oxford Road, 10 Eaton Place and 115-125 Chatham Street, the wider area is allocated for 'residential development with associated community uses (100-150 dwellings)'. The application site comprises circa. 20% of this designated area. The application site is also within an air quality management area and Oxford Road is a designated cycle route.
- 1.3 The application site is also located in close proximity to a number of listed buildings. Most visibly, immediately to the west of the site is the Grade II listed Holy Trinity Church. The False Acacia tree within the churchyard overhangs the application site is subject to Tree Preservation Order 168/01. To the south-west of the site No.s 139 & 141 Oxford Road are also Grade II listed, as is No. 118 Oxford Road to the north-west. Slightly further away, to the west is the Grade II listed terrace of No's 149-169 (odd) Oxford Road, while to the east No. 104 Oxford Road (Mannson House) is Grade II listed too.
- 1.4 Furthermore, the site is located directly to the north of the boundary to the Russell Street/Castle Hill Conservation Area, with the buildings opposite the site on the south side of Oxford Road being within the conservation area. The surrounding area comprises a mix of uses and building types. As well as those already noted, to the north of the site is the Face Bar Nightclub (which fronts onto Ambrose Place), to the east is a two-storey office building (No's 106-112 Oxford Road) with parking to the front and rear, while to the south are the retail and related uses of units on the

south side of Oxford Road (No's 127-141 - odd) between the junctions of Waylen Street and Russell Street.

- 1.5 The application is being considered at Planning Applications Committee at the request of Councillor Page (as per section 11 of the scheme of delegation).



Site Location Plan (not to scale).



Site photograph dated 05/03/2018.

Site photograph dated 31/05/18



Aerial view looking north.

## 2. PROPOSALS

### 2.1 Non-material amendments are sought to planning permission 150721, namely:

- An overall increase in height of the building by 0.5m;
- The omission of the mezzanine floor to flats 13 and 14 (resulting in the reduction from 16 to 4 rooflights on the pitched roof at this point - omission of 12 rooflights)
- A change in the configuration of the front entrance door to the flats (Oxford Road elevation).

## 3. PLANNING HISTORY

- 3.1 081462 - Demolition of existing commercial buildings. Construction of a new shop and 18 flats with associated parking. Refused 11/03/2009 following consideration at the Planning Applications Committee on 04/03/2009.
- 3.2 150721 - Erection of part 4, part 5 storey building providing 16 (2x1, 13x2 & 1x3-bed) residential units (Class C3) with associated parking and landscaping, following demolition of existing buildings (Class A1 / B8/ nil use). (amended description). Granted following completion of s106 legal agreement 17/02/16. This application was resolved to be granted by the Planning Applications Committee at the meeting on 10/02/2016.
- 3.3 171798 - Discharge of conditions 3 (materials), 13 (construction method statement), 17 (arboricultural method statement) and 24 (sustainable drainage) of planning permission 150721, as granted on 17/02/2016. Discharged 30/11/17.
- 3.4 180325 - Variation of condition 2 (approved plans) of planning permission 150721 [Erection of part 4, part 5 storey building providing 16 (2x1, 13x2 & 1x3-bed) residential units (Class C3) with associated parking and landscaping, following demolition of existing buildings (Class A1 / B8 / nil use). (amended description)], namely for various minor material amendments including replacement of approved balconies with Juliet balconies on front and rear elevations at first and second floor level, fenestration alterations, increase in height of the approved building by 0.5 metres and associated works. Withdrawn 27/04/18.

## 4. CONSULTATIONS

- i) Internal / External

4.1 Not considered to be required for this type of application.

ii) Public consultation

4.2 No statutory requirement for this type of application (Local Planning Authorities have discretion), as it is not an application for planning permission. None has been carried out for this application.

4.3 A letter has been received from a Churchwarden of the neighbouring church, stating:

*"The Churchwardens of the Most Holy Trinity Church have reviewed the planning application for a non-material amendment application No. 180739. The changes are very small and we understand and support the reasons for the application."*

## 5. LEGAL AND PLANNING POLICY CONTEXT

5.1 Whilst there is no statutory definition of what constitutes a 'non-material' amendment, Section 96A, part 2 of the Town and Country Planning Act 1990 (as amended) states that, "In deciding whether a change is material, a local planning authority must have regard to the effect of the change, together with any previous changes made under this section, on the planning permission as originally granted." The National Planning Practice Guidance clarifies that there is no statutory definition of 'non-material' "because it will be dependent on the context of the overall scheme - an amendment that is non-material in one context may be material in another." To clarify, a non-material amendment permission does not result in a new permission.

5.2 In terms of relevant policies, the proposed amendments are assessed for materiality - not on the basis of their planning merits. Planning policies therefore do not apply. The National Planning Practice Guidance confirms that "as this is not an application for planning permission, section 38(6) of the Planning and Compulsory Purchase Act 2004 does not apply."

## 6. APPRAISAL

6.1 The main issue is considered to be whether the proposed changes are non-material amendments to the original permission or not. Accordingly, each of the three changes are considered in turn:

i) Increase in height of the building by 0.5m;

6.2 In support of this element of the proposals, the applicant (who it is noted is a different applicant than at the time of the original permission) has provided commentary explaining the reasons behind the proposed increase in floor to floor heights of the building, which in-turn has resulted in a proposed overall increase in height of the building (in comparison with the original permission) of 0.5m:

*"From a construction point of view the existing planning consent is unbuildable because the Building Regulations require a minimum floor thickness to achieve acoustic separation between the units - this is in the region of 400mm depending on the construction method used - the approved plus have a floor thickness of only 291mm. The floor to ceiling height shown on the planning drawings is 2.33m which is the minimum floor*

*height required, so there is no opportunity to reduce ceiling heights to gain the aforementioned floor thicknesses required.*

*As such, Fynecast Limited propose to increase each floor height by 0.2m to achieve the necessary floor to floor separation required by the Building Regulations. On the face of it this would lead to an overall height increase of 0.6m to the height of the building, however, by lowering the height of the main and prominent gable ended section the overall height of the building will only rise by 0.5m. It is noted that the original permission was sensitive to the impact of the development on the neighbouring listed church and adjacent conservation area. The proposed building is still lower and subservient to the Church and the proposed increase in height only represents a 3.3% increase - a minimal increase.*

*It should also be borne in mind that when the adjacent property at 106-112 Oxford Road is re-developed, this will have floor to floor heights similar to those proposed at 114 Oxford Road, so the proposed height will not look out of place.*

*The whole situation has been exacerbated by two conditions which have been attached to the original consent. Specifically, Conditions No. 15 and No. 16, which have been expanded upon in greater detail below. Condition 15 relates to measures to protect future occupiers from the external noise environment. This condition deemed necessary due to the presence of a nightclub immediately to the rear of the site.*

*All dwellings require natural fresh air for obvious reasons, and to comply with the Building Regulations. This is usually in the form of trickle ventilations to windows, or extractor fans to bathrooms and kitchens. Both of these traditional forms of ventilation will 'let the sound through' and won't protect residents from the sound coming from the nightclub, nor comply with the Planning Condition.*

*As stated in the Noise Assessment Report which accompanied the original application, the solution is to fit a ventilation system which draws air in, circulates it around the apartment and expels the stale air. Acoustic baffles will be fitted to the intake and exhaust to prevent sound from the nightclub leaking into the dwelling.*

*Condition 16 similarly requires measures to mitigate the impacts of poor air quality. In tandem with the solution to Condition 15, the ventilation system will also be fitted with a carbon filter to remove pollutants from the air.*

*The ventilation system would distribute clean air around each apartment via ducts above the ceiling. Overlooked on the as approved drawings, the space above the ceiling where this ventilation would be fitted is not high enough to accommodate the ventilation.*

*By increasing the height of each floor by just 0.2m the necessary sound separation between units, as well as the two planning conditions attached to the original consent will be able to be implemented."*

- 6.3 Officers consider that, within the context of an approved part-four (totally 10.88m in height), part-five storey building (totalling 14.96m in height), a 0.5m increase falls within the realms of a non-material amendment to the original permission. At

the time of original permission (150721) the officer committee report at paragraph 6.10 that:

*"In terms of the general height, bulk and mass of the proposed development, it is considered that this element of the proposals has been carefully considered within the context of the previously refused scheme at the site, the wider site allocation, the neighbouring and nearby listed buildings, the adjacent conservation area, the general prevailing character and appearance of the area and, naturally, relevant planning policies. When all relevant factors are taking into account, it is concluded that the general mass, footprint and height of the proposed building is appropriate."*

6.4 The increase in height is not considered to alter any part of the conclusions reached previously, as agreed by members of the Planning Applications Committee on 10/02/16 when the original application was determined. The local context has not significantly altered in the intervening time to lead to a different overall conclusion. It is also evident from the commentary provided by the applicant that the changes are required as a result of design development and to enable compliance with relevant conditions attached to the original permission.

ii) Omission of the mezzanine floor to flats 13 and 14 being used as habitable floorspace (resulting in the omission of 12 rooflights)

6.5 In support of this element of the proposals, the applicant has provided commentary explaining the reasons behind the proposed removal of the mezzanine floor and resultant associated external works (removal of rooflights):

*"It was considered that the mezzanine floor did not really work internally due to the sloping ceilings compromising the internal space. The approved design was a little misleading and upon closer inspection, the sloping ceilings greatly inhibited the usable space. Fynecast Limited believe reformulating the internal layout of the unit on the third floor would provide better usage of the space. This would reflect the layouts of the ground, first and second floor flats. The two flats this affects are also slightly disproportionate as the living area is smaller than the bedroom areas, which is unusual. Externally there would be no change to the elevations."*

6.6 Officers consider that these changes are non-material amendments to the approved scheme. Internal alterations do not in themselves require planning permission, so these can only be described as non-material. Notwithstanding this, these internal layout changes are not considered to unduly compromise the quality of accommodation for future occupiers (these units would remain 2-bed flats). The omission of 12 rooflights, reducing the overall number on the pitched roof feature from 16 to 4 is not considered to alter the design idiom of the scheme.

iii) Change in the configuration of the front entrance door to the flats.

6.7 In support of this element of the proposals, the applicant has provided commentary explaining the reasons behind the proposed amendment, as follows:

*"We propose to slightly amend the arrangement of the front entrance to the flats by having full height side lights to the doors for aesthetic reasons to improve the appearance of the building, and offset the front door to*

*one side. Currently the doors are central into the building and this means the occupants are in danger of banging their heads on the underside of the stairs. By moving the door to the right-hand side, this will be avoided. Further, the proposed door and window configuration reflects the window lines from the upper floor common areas and the window lines will now "flow" better down the building."*

- 6.8 Officers consider that this proposed change is minor in nature and scale, not compromising the overall design / access / amenity intentions of the original permission.
- 6.9 Accordingly, in overall terms officers are content that the proposed changes are acceptable. Officers also consider that these changes can be made through the non-material amendment procedure. More specifically, it is not considered that the amendments, either individually or cumulatively, will have any material impact on the overall design idiom, neighbour amenities, transport / highway safety or any other relevant intentions. This is primarily owing to the context of the overall approved scheme and the nature of the surrounding context, with the proposed changes being of a relatively minor scale and nature in this context.
- 6.10 It is also noted that the proposal has come about following the withdrawal of 180325 (see relevant history section above), with two of the three elements previously proposed now sought in this instance (increase in height by 0.5m and alterations associated with the omission of the mezzanine floor). One element previously proposed as part of 180325 is not sought in this instance (infilling of balconies with additional floorspace and associated works), as officers advised the applicant this would not be considered to be a non-material amendment. One new element (in addition to the two elements referenced above) is included in this submission, namely a change to the ground floor entrance door/window arrangement on the Oxford Road frontage. To be clear, all three changes proposed as part of this application are considered appropriate by officers and suitable to progress through the non-material amendment procedure.
- 6.11 An informative is recommended which states that this permission relates only to the changes highlighted on the plans. Furthermore it shall only be read in the context of the planning permission granted under reference number 150721, and is bound by all the conditions and obligations attached to that permission. A further informative is recommended which states: In reaching this decision, the local planning authority is satisfied that the changes proposed by this application do not constitute material changes to the approved planning permission.

i) Equality

- 6.12 In determining this application the Council is required to have regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sexual orientation. It is considered that there is no indication or evidence that the protected groups have or will have different needs, experiences, issues and priorities in relation to this particular application.

## 7. CONCLUSION

- 7.1 The proposals are considered to be acceptable as non-material amendments to the original permission and therefore are recommended to be agreed as such.

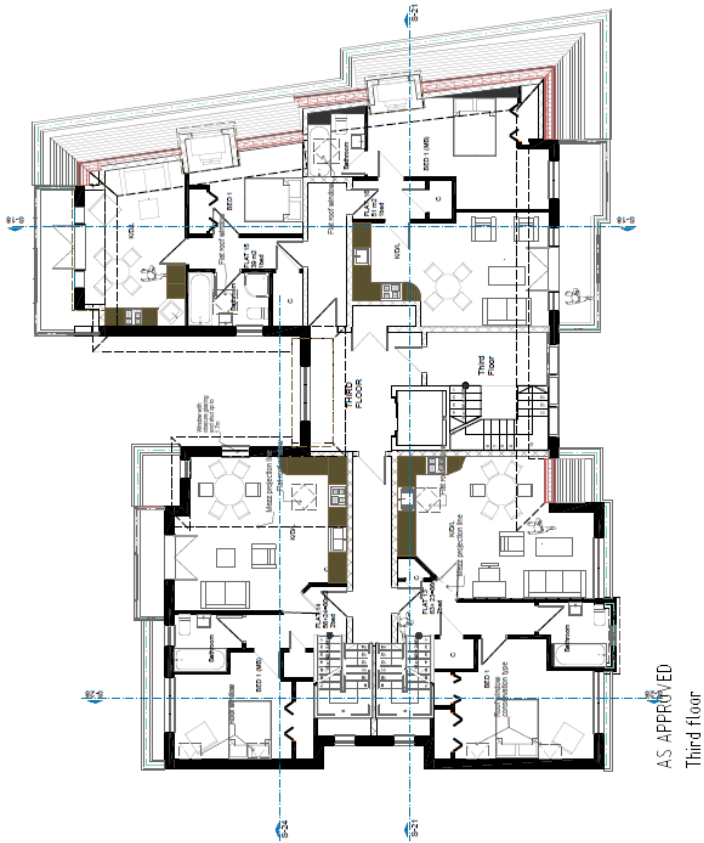
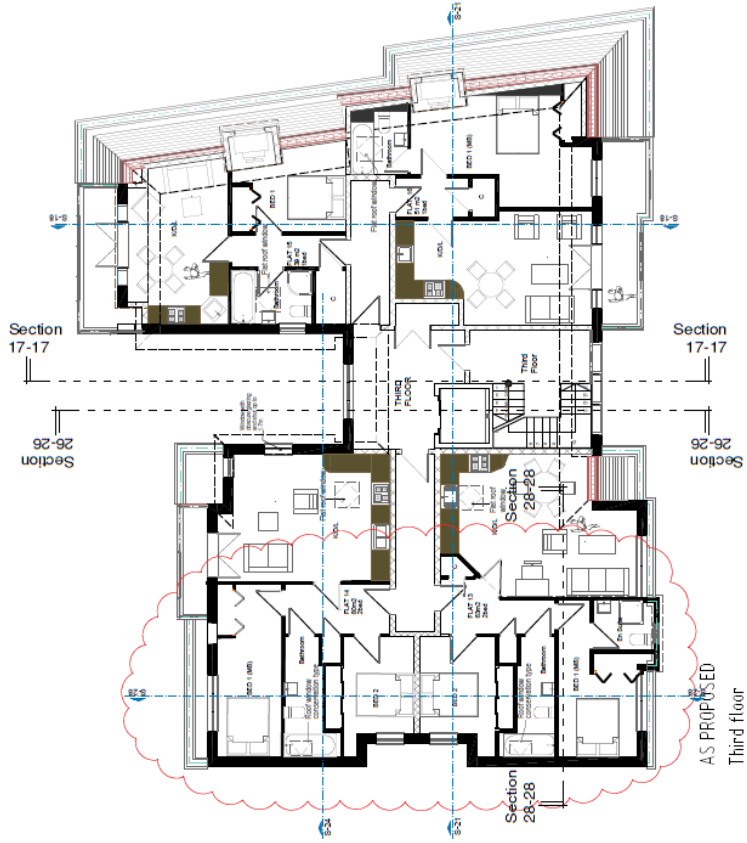
**Drawings:**

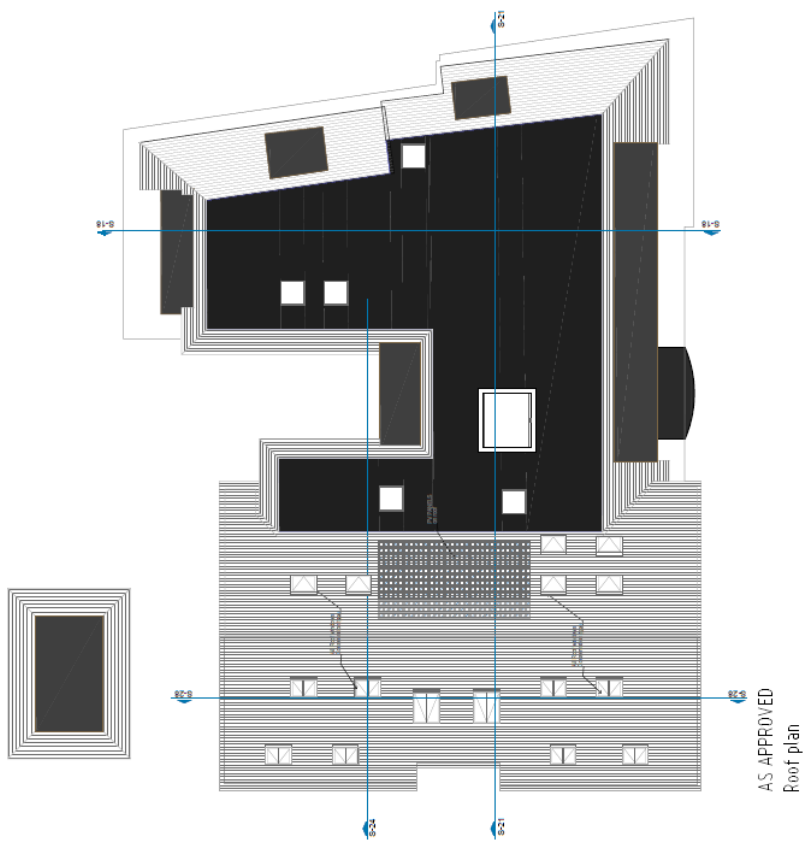
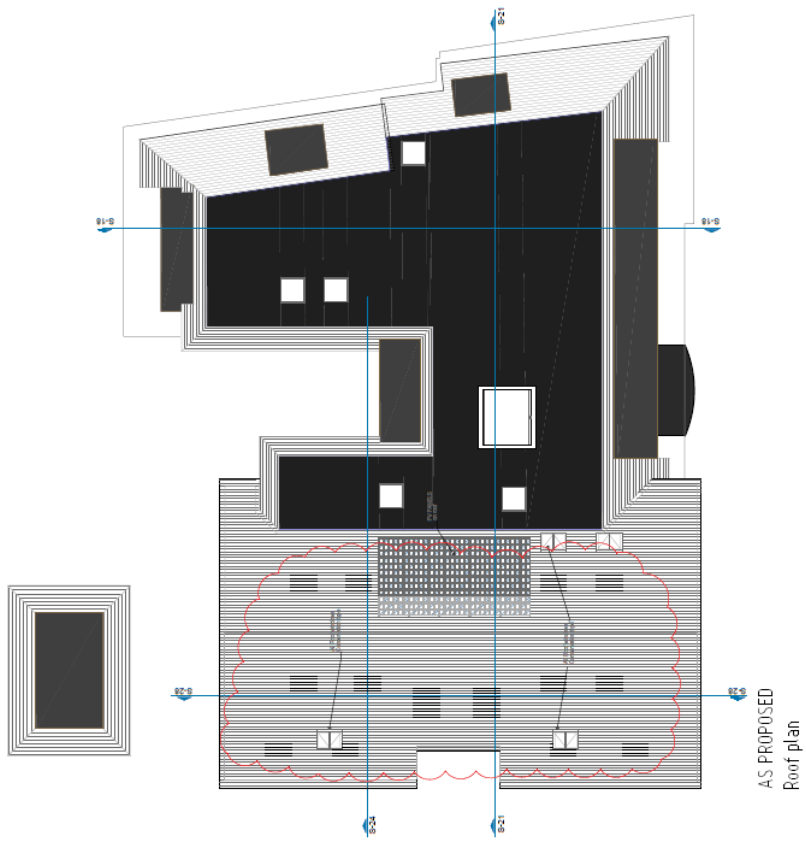
- 114:18:401 Rev B - Ground Floor, as received 02/05/18
- 114:18:402 Rev B - Third Floor, as received 02/05/18
- 114:18:403 Rev B - Front & Rear Elevations, as received 02/05/18
- 114:18:404 Rev B - Side Elevations, as received 02/05/18
- 114:18:405 Rev B - Inward Facing Rear Elevations, as received 02/05/18
- 114:18:406 Rev A - Street Scene, as received 02/05/18
- 114:18:408 - Mezzanine Plans & Sections, as received 02/05/18
- 114:18:410 Rev A - Block & Location Plans, as received 02/05/18
- 114:18:411 - Roof Plans, as received 02/05/18
- 114:18:412 - Proposed Building Sections, as received 02/05/18
- 114:18:413 Rev A - Block Plan, Drainage & Landscaping, as received 02/05/18
- Design and Access Statement by Fynecast Limited, as received 30/05/18

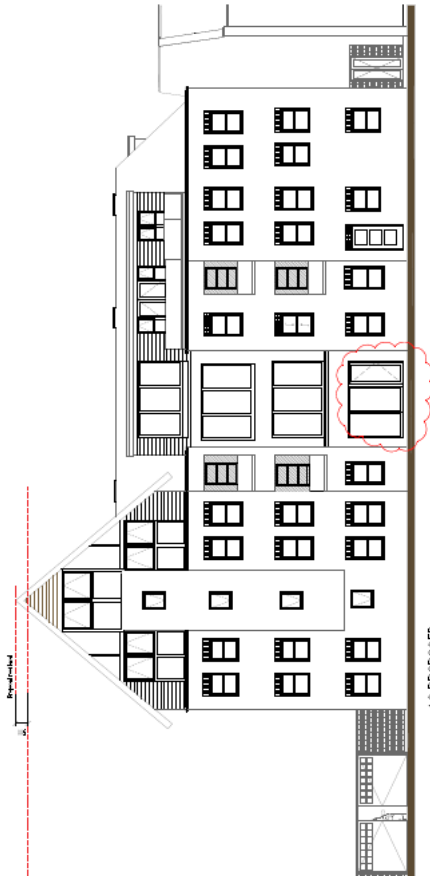
Case Officer: Jonathan Markwell











AS PROPOSED  
Front elevation



AS APPROVED  
Front elevation



AS PROPOSED  
Rear elevation



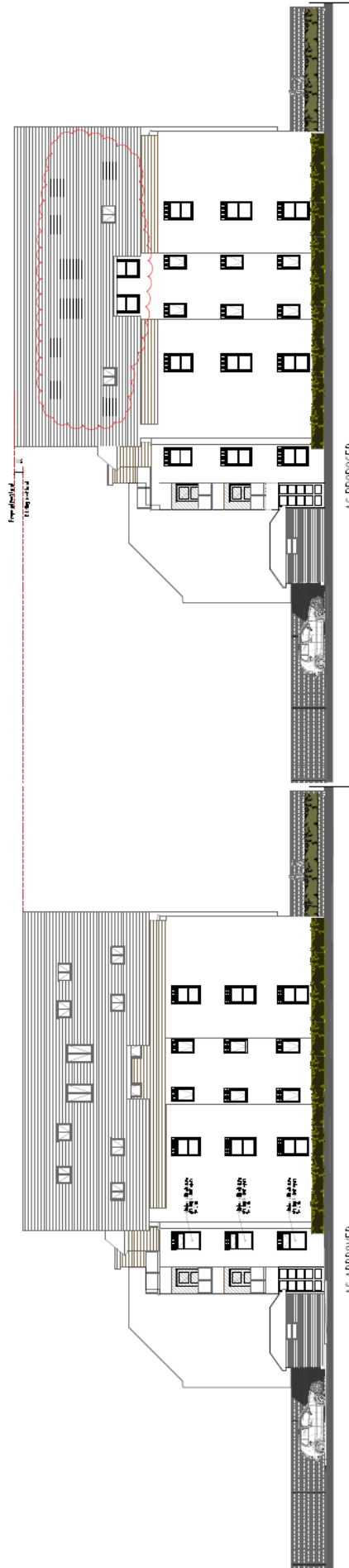
AS APPROVED  
Rear elevation



As approved

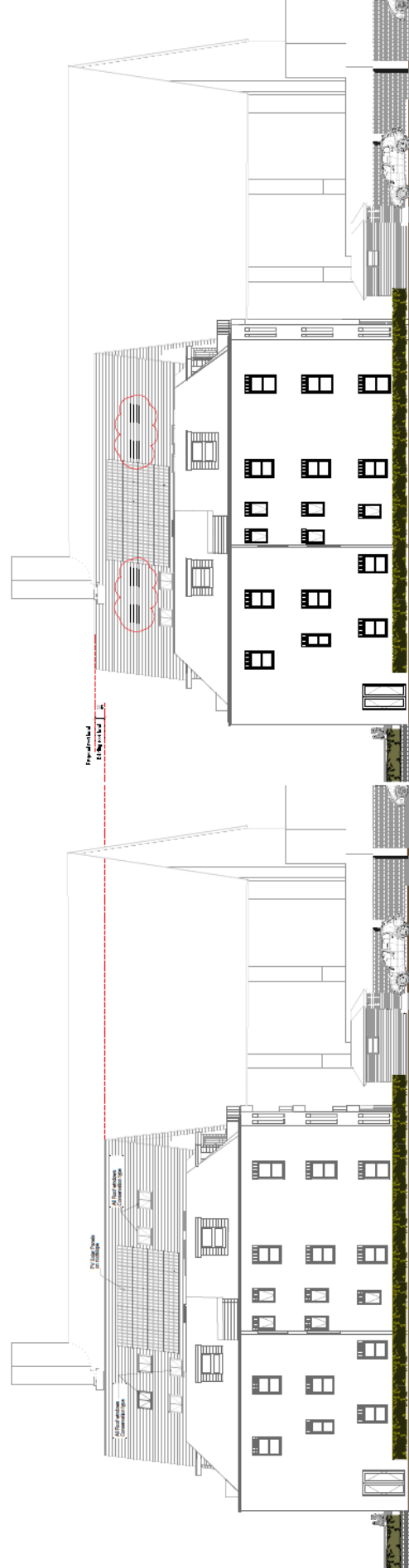


As proposed



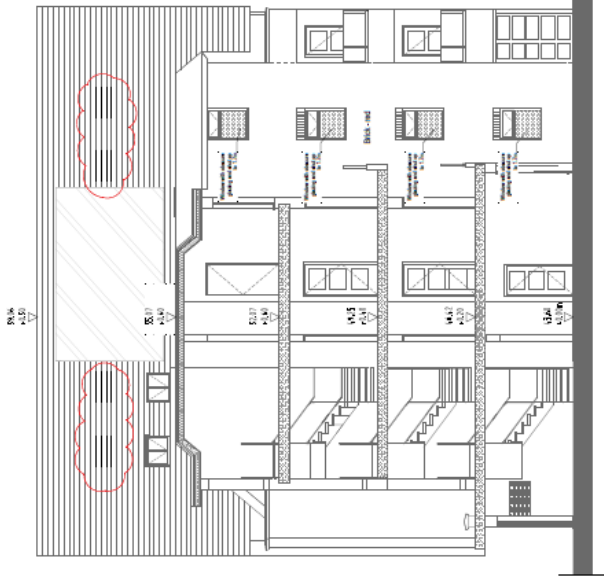
AS PROPOSED  
Side elevation - West

AS APPROVED  
Side elevation - West

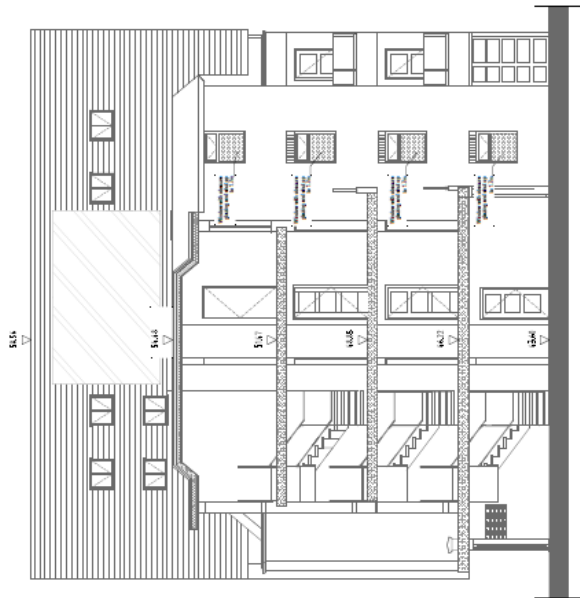
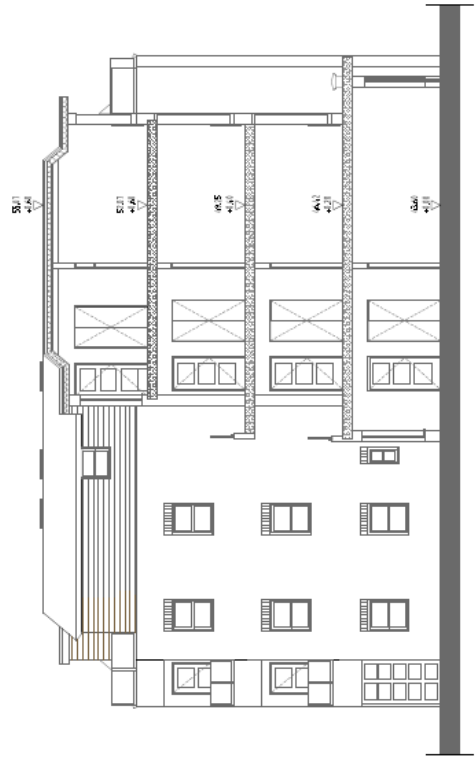


AS PROPOSED  
Side elevation - East

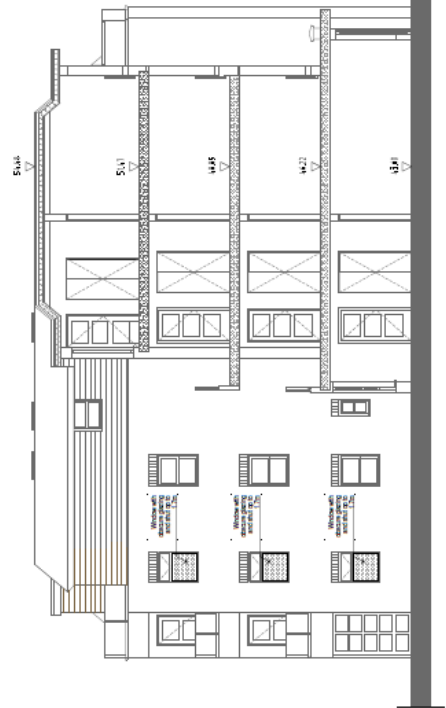
AS APPROVED  
Side elevation - East



As Proposed  
Section - Internal Side Elevation East 26-26



As Approved  
Section - Internal Side Elevation East 26-26

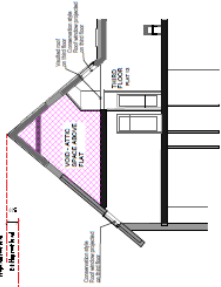




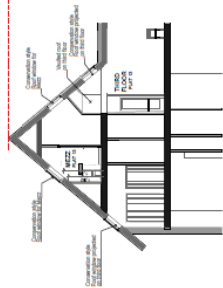
As approved  
Section 13-14



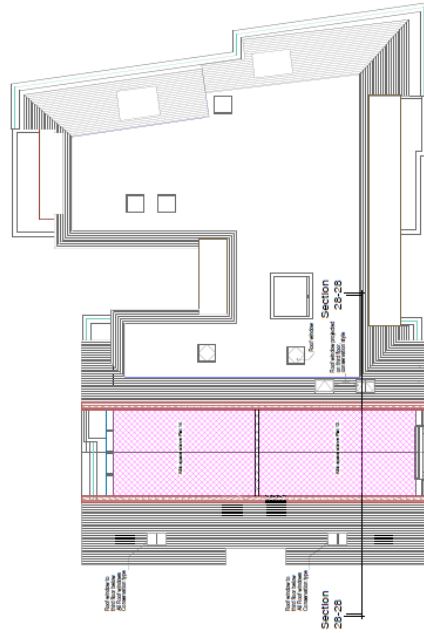
As approved  
Section 13-14



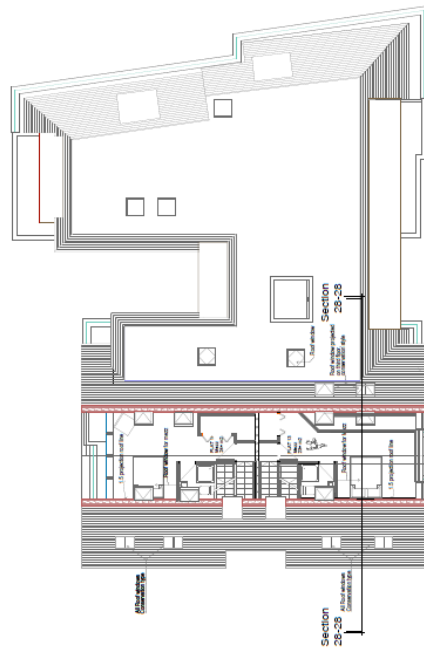
AS PROPOSED  
Section 28-28 Third floor - Mezzanine omitted



AS APPROVED  
Section 28-28 Third floor & Mezzanine



AS PROPOSED  
Mezzanine floor omitted



AS APPROVED  
Proposed Mezz Plan Flats 13 & 14



## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT AND NEIGHBOURHOOD SERVICES

READING BOROUGH COUNCIL

ITEM NO. 11

PLANNING APPLICATIONS COMMITTEE: 27th June 2018

Ward: Abbey

App No.: 172264/VARIAT

Address: County Arms PH 84 Watlington Street Reading

Proposal: Extension and conversion of existing building to create 1 x studio, 6 x 1-bed flats, 3 x 2-bed flats and associated works including parking, amenity space and landscaping without complying with conditions 2 (approved plans), 3 (materials), 4 (landscape), 6 (landscape maintenance), 7 (boundary treatment), 8 (Code for Sustainable Homes) and 11 (construction method statement) of planning permission ref. 141416/VARIAT to increase the number of units from 9 to 10 (as secured previously under planning permission ref. 11/00110), alterations to fenestration, relocation of cycle store to car park and alterations to car parking and landscaping layout

Applicant: Mr. J. Hayward Nevitt

Date received: 17th December 2017

Target decision date: Extension of time until 20<sup>th</sup> July 2018

### RECOMMENDATION

Delegate to the Head of Planning Development and Regulatory Services to GRANT the proposed variations/removal of conditions to planning permission 141416; subject to satisfactory completion of a section 106 legal agreement or to REFUSE planning permission should the legal agreement not be completed by 20<sup>th</sup> July 2018 (unless the Head of Planning, Development and Regulatory Services agrees to a later date for completion of the legal agreement).

The section 106 legal agreement is to be based upon the same terms as that attached to planning permission 141416 (Education Contribution of £6,173 and Leisure Contribution of £15,000).

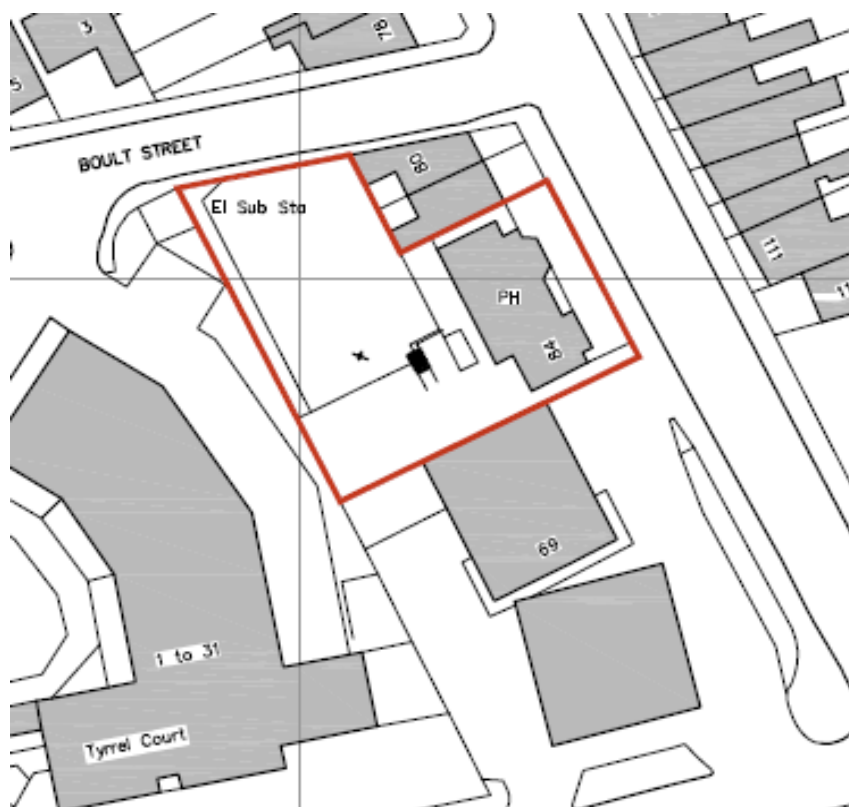
#### Conditions

Minor Amendments to conditions 2 (approved plans), 3 (materials), 4 (landscape scheme), 6 (landscape maintenance), 7 (boundary treatment), and 11 (construction method statement) and removal of condition no. 8 (code for sustainable homes)

All other conditions and informatives as per planning permission ref. 141416.

## 1. INTRODUCTION

- 1.1 The application site comprises the former County Arms Public House (No. 84) and land to the rear of Nos. 80/82 Watlington Street which is currently being converted to residential flats.



- 1.2 The site falls within the western edge of Eldon Square Conservation Area.

## 2. PROPOSAL

- 2.1 The application seeks a variation of the conditions of application ref. 141416 relating to the redevelopment and conversion of the former County Arms PH to residential flats.
- 2.2 The original application allowed at appeal in November 2011 (Ref. 111073) sought to convert and extend the building into 10 residential dwellings. A subsequent application for variation of condition was granted in February 2015 (Ref. 141416) and reduced the number of dwellings to 9.
- 2.3 A further variation of conditions application (Ref. 160944) was granted at planning applications committee in February 2017 which sought to revert the number of dwellings back to 10 along with a number of other minor alterations to the scheme development. These approved alterations included:

- Increasing the numbers from 9 flats to 10 (as secured under 111073 at appeal) by converting the basement cycle store into a 1 bedroom flat (42sqm);
- Cycle store relocated to rear adjacent to car park;
- Alterations to the elevations to include a glazed section of roof (front) and 3no. high level windows (side) to serve the basement flat
- Alterations to the unit sizes, type and layouts to provide a total of five 2-bedroom and six 1-bedroom flats
- Parking layout and landscaping altered to provide 10 no. spaces.

2.4 The current application seeks to again vary the conditions of permission 141416. The majority of the changes proposed are the same as those approved under the most recent variation of conditions application (ref. 160944), including reverting the number of units back from 9 to 10 with the applicant seeking approval for the alterations again to consolidate approved plan numbers and drawing references given the lengthy planning history to the development. However, there are also some minor additional changes proposed as part of the current application which include:

- Additional and re-positioning of roof lights to the rear facing roof planes of the building (4 additional)
- Addition of a second floor level terrace to serve flat no. 9. This is located to the south flank elevation adjacent to the petrol station
- Alterations to proposed landscaping
- Alteration to boundary treatment to Boulton Street
- Minor internal alterations to unit layouts
- Removal of Code for Sustainable Homes requirements
- Changes to approved Construction Method Statement

2.5 The application is required to be determined at Planning Applications Committee as it again seeks to revert the number of proposed residential units from 9 to 10 and as such is technically considered to be a major planning application.

### 3. PLANNING HISTORY

3.1 111073/FUL (11/00110/FUL) - Extension and conversion of existing building to create 1 x studio, 6 x 1-bed flats, 3 x 2-bed flats and associated works including parking, amenity space and landscaping - Allowed on appeal.

121147/APPPCON - Discharge of conditions 3, 4, 6, 7, 11 and 12 for planning permission 11/00110/FUL - Conditions Discharged.

141416/VARIAT - Application for removal or variation of a condition following grant of planning permission 11/00110/FUL (reduction in units from 10 to 9) - Granted.

141423/NMA - Alterations to materials, internal layouts, removal of a chimney and reduction in the no. of flats from 10 to 9 - Granted.

160944/VARIAT - Alterations to elevations and fenestration, relocation of cycle store to car park, increase in parking space from 8 to 10, amended landscaping layout and reverting no. of flats back from 9 to 10 by converting the basement cycle store - Granted.

#### 4. CONSULTATIONS

RBC Transport - No objection, the updated construction method statement is acceptable.

RBC Natural Environment Trees - No objection, the amended landscaping scheme is acceptable.

##### Neighbour Notification

Neighbour notification letters were sent to no.s 80 - 84 and 99 - 111 Watlington Street, Flats 1 - 31 Tyrrel Court, 69 London Road and no.s 1 - 7 Boulton Street. A site notice was also displayed at the application site. No letters of representation have been received.

#### 5. RELEVANT POLICY AND GUIDANCE

- 5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that proposals be determined in accordance with the development plan unless material considerations indicate otherwise. Material considerations include relevant policies in the National Planning Policy Framework (NPPF) published in March 2012 - among them the 'presumption in favour of sustainable development'. However the NPPF does not change the statutory status of the development plan as the starting point for decision making (NPPF paragraph 12).
- 5.2 In this regard, the NPPF states that due weight should be given to the adopted policies of the Local Development Framework (LDF) (Core Strategy and Sites and Detailed Policies Document) according to their degree of consistency with the NPPF. The closer the policies in the plan are to the policies in the NPPF, the greater the weight that may be given.
- 5.3 In Paragraph 7, the NPPF defines sustainable development and the social role of the planning system in supporting strong, vibrant and healthy communities by providing the supply of housing required to meet the needs of present and future generations. In Section 6: Delivering a Wide Choice of High Quality Homes local planning authorities are advised to boost significantly the supply of housing and that applications for such

should be considered in the context of the presumption in favour of sustainable development.

- 5.4 Accordingly, the National Planning Policy Framework and the following development plan policies and supplementary planning guidance are relevant:

**Reading Borough LDF Core Strategy 2008 (Altered 2015)**

CS1 Sustainable Construction and Design  
CS4 Accessibility and the intensity of development  
CS7 Design and the Public Realm  
CS9 Infrastructure, Services, Resources and Amenities  
CS15 Location, Accessibility, Density and Housing Mix  
CS20 Implementation of the Reading Transport Strategy  
CS24 Car/Cycle Parking  
CS33 Protection and Enhancement of the Historic Environment  
CS38 Trees, Hedges and Woodlands

**Sites and Detailed Policies Document 2012 (Altered 2015)**

SD1 Presumption In Favour Of Sustainable Development  
DM3 Infrastructure Planning  
DM4 Safeguarding Amenity  
DM8 Residential Conversions  
DM10 Private and Communal Outdoor Space  
DM12 Access, Traffic and Highway-Related Matters  
DM18 Tree Planting

**Supplementary Planning Guidance**

Revised Parking Standards and Design SPD 2011  
Residential Conversions (2013)  
Revised Section 106 Planning Obligations (2013)

## 6. APPRAISAL

- 6.1 An application submitted under section 73 allows the local planning authority to only consider the proposed amendments. All other matters remain as approved under planning permission ref. 141416. If planning permission to amend the plans is granted, this has the effect of granting a new permission for the whole development.
- 6.2 The proposed changes set out in paragraph 2.3 were approved under application ref. 160944 at Planning Applications Committee in February 2017. As such these changes are not assessed again as part of this report.
- 6.3 An assessment of the additional changes proposed by this application only is set out below:

Additional (four) and re-positioning of roof lights to the rear facing roof planes of the building (variation of condition no.s 2 and 3 - approved plans/materials)

- 6.4 Four modest additional roof lights are proposed to the rear elevation of the building (two have been omitted from the previously approved plans). The roof lights would be black conservation style roof lights with a low profile as per the materials approved previously under condition no. 3. The additional roof lights together with minor repositioning of those already approved are considered acceptable amendments to the scheme and to preserve the character of the host building and surrounding conservation area in accordance with Policies CS7 and CS33.

Addition of a second floor level terrace to the south flank elevation to serve flat no.9 (variation of condition no.2 - approved plans)

- 6.5 An additional modest terrace area is proposed to part of the east elevation roof plane which would serve as a private amenity space to flat no. 9. This change is incorporated by increasing a small area of flat roof to this part of the roof plane. This area would not be visible from the sensitive front elevation of the building or street-scene and is not considered to adversely impact upon the appearance of the building. The terrace area would face the adjacent petrol station and no undue overlooking or loss of privacy is considered to result. This amendment is considered to accord with Policies CS7 and CS33.

Amendments to approved hard soft landscaping layout (variation of conditions no.4 and 6 - landscape scheme and landscape management)

- 6.6 An amended landscaping scheme has been submitted as part of the application. This includes minor alterations to layout but also additional tree and shrub planting to the rear communal amenity space as well as to the frontage of the building. The Natural Environment Trees Officer has confirmed that the revised landscaping proposals are acceptable and accord with Policies CS7, CS38 and DM18.
- 6.7 The size of the communal grassed amenity space to the rear has been reduced to accommodate the proposed external cycle store. Given the central location of the site and access to nearby public recreation facilities there is no officer objection to the modest reduction in communal amenity space provision in the context of Policy DM10. Furthermore, there were increases to the size of the private patio/terraces provided to individual units under the previous variation of condition application (160944) and, as per paragraph 6.5 above, a new private terrace area is now proposed to flat no.9. On this basis officers consider that the mix of both communal and private amenity space provision within the development would be acceptable.

Alteration to boundary treatment to Boulton Street (variation of condition no. 7 - boundary treatment)

- 6.8 The current approved boundary treatment to Boulton Street consists of a brick wall and rail sections of between 1.5m and 2.2m in height (due to the sloping nature of this frontage) and 1.8m high rail entrance gates. The amended boundary treatment proposes a dwarf brick wall with black metal railings on top but at the same heights as that already approved. No entrance gates are now proposed. Officers consider that the lower brick wall with railings mounted on top would provide for a more open feel to the Boulton Street frontage than that of the approved boundary treatment. Brick work would be to match the host property. Officers consider that the amended boundary treatment would be an enhancement to the approved development and would assist in preserving the character of the surrounding area and conservation area in accordance with Policies CS7 and CS33.

Minor internal alterations to unit layouts (variation of condition no. 2 - approved plans)

- 6.9 There are a number of minor internal layout changes to the units proposed. These primarily involve small changes to internal wall and door placement. These amendments do not result in any material changes to unit sizes or any adverse impacts in terms of outlook and day lighting and would accord with Policy DM4.

Removal of code for sustainable homes requirements (removal of condition no. 8 - code for sustainable homes)

- 6.10 The Code for Sustainable Homes was withdrawn in 2015. The necessary internal standards of the proposed development would now be secured under building regulations requirements. Therefore, condition no.8 can be removed from the decision notice.

Amendments to the approved Construction Method Statement (variation of condition 11 - construction method statement)

- 6.11 Minor amendments to the approved construction method statement are proposed. Transport officers have confirmed that the minor alterations to the site compound layout are acceptable and would accord with Policies CS20 and DM12.

Other Issues

6.12 A section 106 was attached to the previous planning permission in relation to contributions towards education and leisure, payable prior to first occupation of the development. The consent to which these contributions relate has been implemented and construction is underway but it is necessary to enter into a new section 106 agreement in respect of this current application to carry the obligation to make these contributions forward. The applicant has indicated agreement to this. Any permission granted would be subject to satisfactory completion of a new legal agreement with the Council.

## 7. CONCLUSION

7.1 The proposed amendments are considered acceptable minor material amendments to planning permission 141416.

### Plans Considered:

#### Drawing no.s

02-20 P3 - Proposed Site Plan

03-20 P - Proposed Lower Ground Floor Plan

03-21 P - Proposed Ground Floor

03-22 P - Proposed First Floor

03-23 P2 - Proposed Second Floor

03-24 P - Proposed Roof Plan

90-20 P5 - Proposed Landscaping Plan Sheet 1 of 2

90-21 P5 - Proposed Landscaping Plan Sheet 2 of 2

90-22 P3 - Proposed Wall and Bin Store

90-23 P3 - Cycle Store

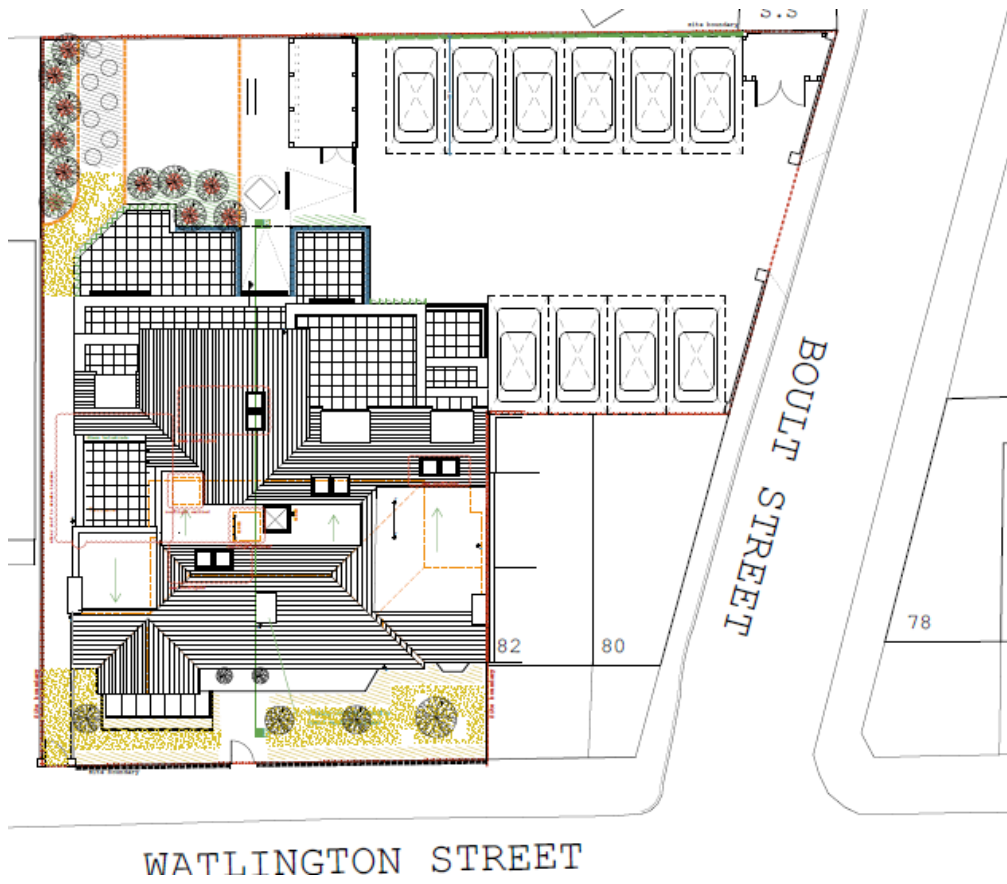
02-30 P - Construction Traffic Management Plan (CTMP)

05-20 P5 - Proposed Elevations

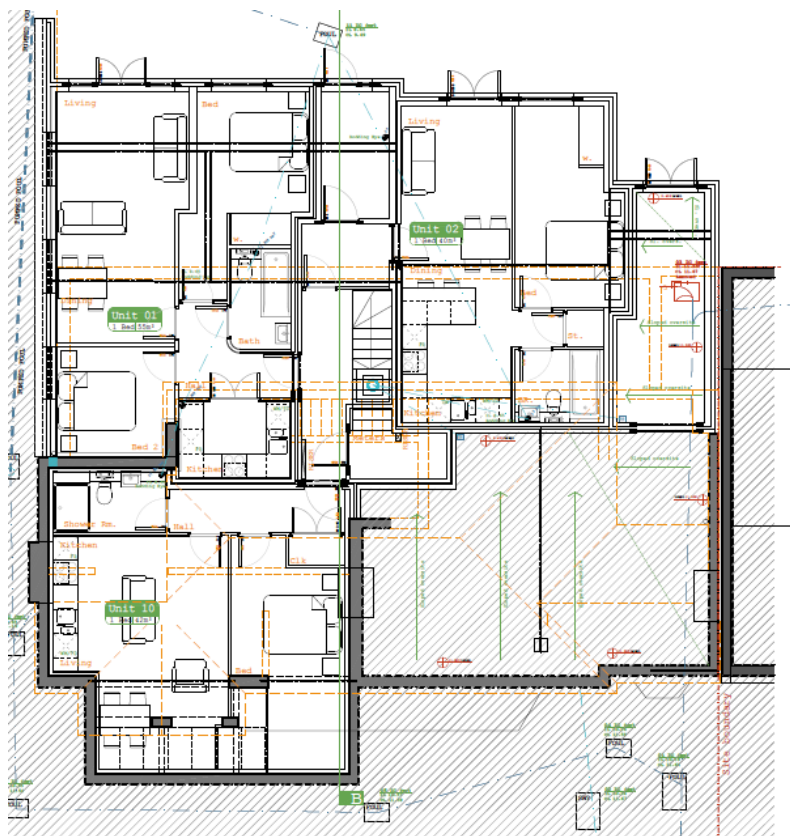
03-21 P - Proposed Section B-B

Case Officer: Matt Burns

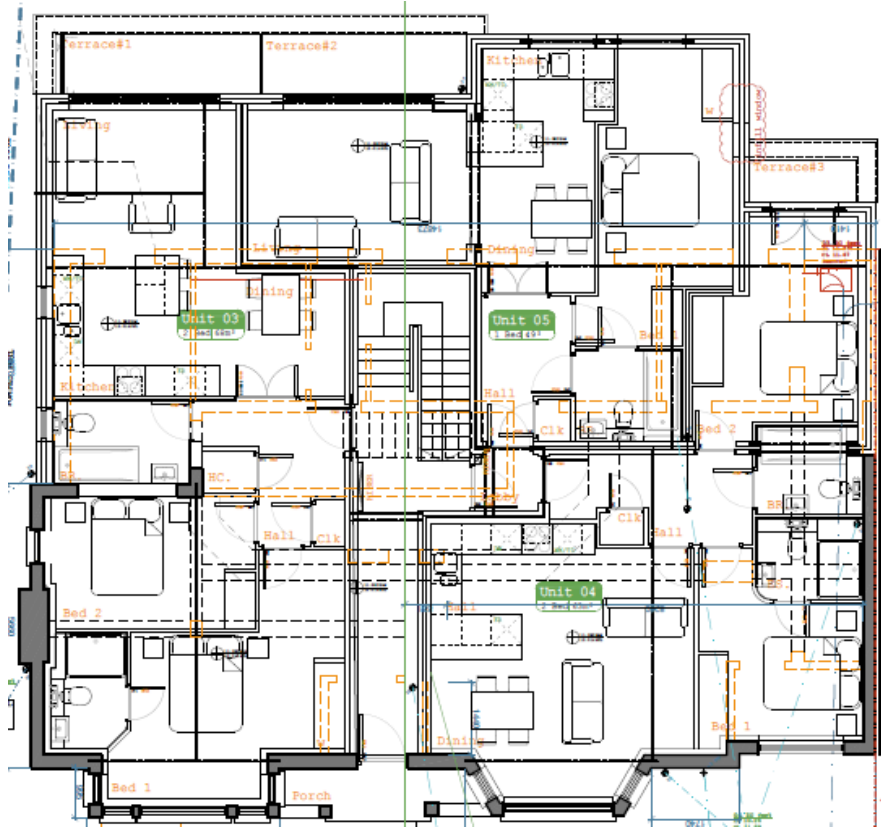




Proposed Site Plan



Proposed Lower Ground Floor Plan



Proposed Ground Floor Plan



Proposed First Floor Plan







Proposed Elevations



# NORCOT





## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT & NEIGHBOURHOOD SERVICES

READING BOROUGH COUNCIL

ITEM NO. 12

PLANNING APPLICATIONS COMMITTEE: 30 May 2018

Ward: Norcot

App No.: 161507/OUT

Address: 2-6 Water Road and 158 Dee Road

Proposal: Demolition of 4 existing dwelling houses 2,4,6 Water Road and 158 Dee Road and erection of 6 no.4 bedroom and 5 no. 3 bedroom dwellings and car parking.

Applicant: Mrs K Fielden

Date validated: 15 September 16

Major Application: 13 week target and 26 weeks target extended - now to 30 July 2018.

### RECOMMENDATION

GRANT Full Planning Permission, subject to conditions and informatives and subject to the satisfactory completion of a S.106 legal agreement or REFUSE permission should the legal agreement not be completed by 2018 unless a later date is agreed by the Head of Planning, Development & Regulatory Services,

The Section 106 Legal Agreement to secure the following:

At least 2 of the dwellings to be secured as affordable housing and £75,000 to be paid towards providing affordable housing elsewhere in the Borough. Houses to be provided before 6<sup>th</sup> private house sold and payment made before first occupation of any private house. Default financial payment towards off-site affordable housing should units on site not be acquired by RSL.

### CONDITIONS TO INCLUDE

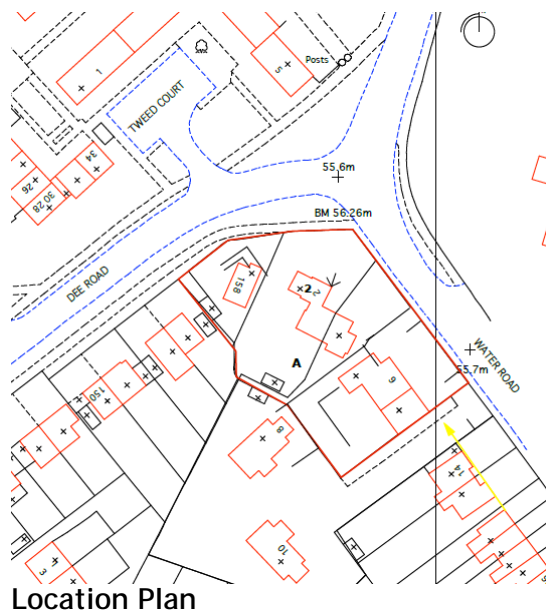
1. TL2 Outline - time limit - reserved matters
2. TL3 Outline - time limit - commencement
3. TL6 Outline - details of reserved matters (Landscaping).
4. AP1 The standard approved plans condition
5. AP2 The standard submission of plans as reserved matters
6. M2 The standard materials to be approved condition
7. L8 Landscape reserved matters condition
8. L9 The standard tree protection
9. L10 Landscape boundaries condition to include retention of boundary adjacent to no.8 Water Road.
10. PD2 No additional windows (in side elevations)
11. CO1 Construction/demolition - standard hours
12. CO2 Construction Method Statement
13. CO3 Establishing if site is contaminated
14. CO5 Remediation to be implemented if contamination found
15. CO6 To cover new contamination being identified
16. CO7 Noise and dust controlled during construction
17. CO8 No Bonfires allowed
18. SU1 Pre-commencement sustainable drainage details
19. SU2 Sustainable Drainage - completed in accordance with the submitted and approved details.
20. DC1 Vehicle parking provided and retained in accordance with approved plans
21. DC2 Vehicle access provided and retained in accordance with approved plans
22. DC6 Bin storage provided and retained in accordance with approved plans

## INFORMATIVES TO INCLUDE

1. The applicant is advised that the development lies adjacent to a potentially contaminated site (former brick kiln). Due to the nature of the historic land use there is the potential of contaminated land being present. The developer must be aware that any ground or enabling works may bring receptors into contact with the aforementioned contaminants. The developer may wish to satisfy themselves that the details of the construction proposals take the necessary account of the possibility of contaminants from that source.
2. IF1 Positive and Proactive Working - approval
3. IF2 Pre-commencement conditions
4. IF4 S106
5. IF5 Terms
6. IF7 Complaints about construction
7. I11 Community Infrastructure Levy (CIL)

## 1. INTRODUCTION

- 1.1 The application site comprises four existing dwellings. 2 and 4 Water Road are Victorian properties, and 6 Water Road is understood to date from the early 1900s. 158 Dee Road is a post war property.
- 1.2 The site is located adjacent to the junction of Dee Road, Water Road and Grovelands Road. The surrounding area comprises a mixture of dwelling houses and flats. The Dee Park Regeneration Area is located on the far side of Dee Road.



## 2. PROPOSAL

- 2.1 The outline proposal seeks permission to demolish the existing properties and erect 11 dwellings (6 no.4 bedroom and 5 no 3 bedroom dwellings). The original submission was for 12 no. 4 bed houses. All matters apart from landscaping are to be determined at this stage. Access to 8 of the dwellings will be taken from Water Road, at a distance of approximately 42 metres from the junction of Water Road, Dee Road and Grovelands Road. 3 of the dwellings will be accessed from Dee Road, at a distance of 37 metres from the junction.

Proposed Drawings & Documents:

100.PL1B Typical layouts 2 x 4 Bedroom house  
100.PL2A Side elevation 3 Bedroom house  
100.PL.3B Typical layouts 2 x 3 Bedroom house  
100.PL4B Main elevations 4x4 Bedroom House  
100.PL5A Layout plans, corner 4 Bedroom Semi detached  
100.EL6A Elevations 4 Bedroom corner house  
100.EL1E Site elevational sections sheet 1  
100.EL2F Site elevational sections sheet 2  
500.SP1 Site plan existing  
500.RP3 Site plan environmental  
3037.LD.1 Rev G Site layout landscape and drainage  
Visuals of proposed scheme 1- 5  
Design and Access statement  
ARBORICULTURAL SURVEY - SJA TREES  
ECOLOGY SITE REPORT Stage 1 and 2  
VIABILITY STUDY - CONFIDENTIAL

Amended CIL information form providing for an estimated payment of around £64,000 with the final figure subject to the usual reliefs or exemptions, such as for affordable housing, set out in the CIL Regulations.

### 3. PLANNING HISTORY

- 10-02105-PREAPP - Pre-application advice for proposed redevelopment of existing properties to provide 7 x 4-bedroom houses and 8 x 3 bedroom houses. Observations sent.
- 141022/FUL. Demolition of 4 houses and erection of 2 No. three bedroom and 12 No. four bedroom houses with associated landscaping, parking and access. Withdrawn.

### 4. CONSULTATIONS

#### 4.1 Statutory:

No statutory consultations were required given the nature of the application.

#### 4.2 Non-statutory:

- RBC Transport Development Control  
The site is located within Zone 3, Secondary Core Area, of the Council's adopted Parking Standards and Design SPD. Typically these areas are within 400m of a Reading Buses high frequency 'Premier Route', which provides high quality bus routes to and from Reading town centre and other local centre facilities.

The application site is located within close proximity to premier bus route 15 with frequent bus services every 30 minutes that run to and from the town centre. The site is, therefore, accessible to good public transport links, town centre services and employment areas.

In accordance with the adopted SPD, the development would be required to provide a parking provision of 2 spaces per 3/4 bedroom dwellings equating to 22 parking spaces in total. The proposed layout provides the required

number of parking spaces to the correct dimensions of 2.4m wide x 4.8m long with a 6m forecourt depth.

Access to the existing properties is taken in the form of several dropped crossings located on Water Road and Dee Road. This is to be revised so that one shared access point is provided from Dee Road and one shared access point is provided on Water Road.

Visibility splays and sight lines allow traffic on the minor road to see cyclists, vehicles and pedestrians on the main road. Visibility splays have been illustrated on the General Site Plan (Revision G) but the splays have not been illustrated correctly as is not measured along the nearside kerb line of the main arm. Therefore, I have undertaken my own assessment.

The required visibility can be achieved although it is evident that the visibility splay goes across third party land which provides access to no.8 Water Road. The applicant has confirmed that the boundary walls will be a maximum of 600 mm high which will ensure that adequate sight lines are maintained. No landscaping is proposed within the visibility splay which will ensure that the visibility from the access is maintained.

It has been confirmed that the 2 dwellings fronting onto Dee Road will utilise existing dropped crossing onto Dee Road. No street furniture or lamp columns will need to be relocated. The existing dropped kerb on the corner of Dee Road/Water Road will be stopped up and the verge reinstated to the satisfaction of the Local Highway Authority.

It is indicated that refuse collection will take place on-street as per the current arrangements. The location and size of the stores has been based on the Waste management guidelines. The stores are fitted with horizontal split sliding sectional shutters to avoid opening doors out onto the footway.

In accordance with the SPD, each dwelling should be provided with 2 cycle parking spaces which can be provided within a garden store for each property. However, I am happy to deal with this by way of condition.

- RBC Environmental Health:  
The applicant is advised that the development lies adjacent to the site of an historic brick works, which has the potential to have caused contaminated land and the proposed development is a sensitive land use. Conditions were recommended to ensure that future occupants are not put at undue risk from contamination.
- Even though this outline planning application reserves landscaping to be approved at a later stage advice from the Natural Environment Team was sought:  
The development site is located in a prominent location at the three-way junction of Water Road with Dee Road and Grovelands Road. This is visually a very open and green highway junction with wide verges and lawn areas on all sides and development set well back from the highway.

On the approach from Grovelands Road, trees form a prominent feature of the landscape both to the front of the dwellings and as a backdrop to the existing houses where the mature tree cover of Lousehill Copse is visible - part of the identified Wooded Ridgeline of West Reading and a designated

Major Landscape Feature. Planning policy CS37: Major Landscape Features and Strategic Open Space states that 'Planning permission will not be granted for any development that would detract from the character or appearance of areas designated as a Major Landscape Feature'.

The mature tree cover to the rear of the existing properties which is currently visible above and between the gaps between dwellings, enhances the treed suburban residential character of the local area. The continuous terraced block of development and increased ridgeline of the proposed new dwellings will block far reaching views of the mature tree cover to the rear of the site. This height and density of the proposed development will be detrimental to the verdant and open and comparably spacious street character which is enhanced by the views of mature trees growing in the wider landscape. This application proposes the removal of all trees within the site with very limited opportunity for substantial new planting. Although new planting is proposed, trees are unlikely to establish well in a hard landscaped area or where they do survive it is likely future occupants will want them to be removed where they cause damage to hard surfaces or become a nuisance to parked cars from leaf drop, mildew or bird droppings.

The front of the site would be almost completely laid with hard standing for parking and access with bin stores immediately adjacent to the highway. The loss of grass and soft landscaped garden areas at the front will increase the sense of overdevelopment in the site when viewed from the public highway to the detriment of the wider landscape. Without adequate provision for new planting and areas of soft landscaping to the front of the site this harm will be permanent. The site is within a 10% or less canopy cover area as defined in our Tree Strategy and as such landscaping, incorporating trees, should be an integral part of any proposal.

The most significant trees on site are two Leyland Cypress and two Silver Birch. Although I accept the Leyland Cypress are not the most desirable trees for many residential properties, the Silver birch are healthy trees of good form and I would not want to see them felled in order to improve the development potential of the site.

My colleague Sarah Hanson, expressed concern during consideration of an earlier application ref. 141022 about works within the RPAs of the two Birch on the frontage of 6 Water Road as she also felt these were two good, prominent trees.

A TPO was therefore served on Tuesday 4th October in order to protect these two Silver Birch - TPO No. 20/16. The TPO will ensure the trees cannot be felled in order to overcome any tree related objections to this application and can also be used to ensure any tree protection measures can be rigorously enforced if planning permission is granted to develop the site in the future with the trees retained.

*Officer note: The proposed development layout has been amended to address these and other concerns. Comments on the amended layout are:*

I can confirm that the plan addresses the points discussed with the arb consultant and therefore I have no objection to the proposal subject to recommended conditions and informatives. I would like to see the tree protective fencing extended as far as is feasible to protect future landscape areas, as recommended in BS 5837:2012.

#### 4.3 Public consultation:

47 properties were consulted by neighbour consultation letter. A site notice was displayed. 3 objections to the original application were received. They are identical letters raising the following:

- Objectors are under the impression that 2, 4 and 6 Water Road are listed buildings.
- Proposal will create additional traffic.
- Proposal could create dangerous traffic conditions.
- Proposal could generate noise.
- Proposal could result in loss of privacy to neighbours.
- Development will be out of keeping with its surroundings.

A further consultation was undertaken following the receipt of the amended plans. 5 objections were received, 3 from previous objectors using the same letter to raise the same concerns and two new ones from the bungalows behind the property raising concerns about the impact on their outlook and privacy.

#### 5. RELEVANT PLANNING POLICY AND GUIDANCE

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that proposals be determined in accordance with the development plan unless material considerations indicate otherwise. Material considerations include relevant policies in the National Planning Policy framework (NPPF) - among them the 'presumption in favour of sustainable development'.

The following local and national planning policy and guidance is relevant to this application:

##### 5.1 National Planning Policy Framework

- Part 4 - Promoting sustainable transport
- Part 6 - Delivering a wide choice of high quality homes
- Part 7 - Requiring good design
- Part 11 - Conserving and enhancing the natural environment

##### 5.2 Reading Borough Local Development Framework Core Strategy

- CS4 (Accessibility and the Intensity of Development)
- CS7 (Design and the Public Realm)
- CS15 (Location, Accessibility, Density and Housing Mix)
- CS16 (Affordable Housing)
- CS24 (Car/Cycle Parking)
- CS33 (Protection and Enhancement of the Historic Environment)
- CS36 (Biodiversity and Geology)
- CS38 (Trees, Hedges and Woodlands)

##### 5.3 Sites and Detailed Policies Document

- SD1 (Presumption in Favour of Sustainable Development)
- DM4 (Safeguarding Amenity)
- DM5 (Housing Mix)
- DM6 (Affordable Housing)
- DM10 (Private and Communal Outdoor Space)

DM11 (Development of Private Residential Gardens)  
DM12 (Access, Traffic and Highway Related Matters)  
DM18 (Tree Planting)

#### 5.4 Reading Borough Council Supplementary Planning Guidance

- Planning Obligations under Section 106 of the Town and Country Planning Act 1990 (Supplementary Planning Guidance).
- Parking Standards and Design (Supplementary Planning Document).
- Affordable Housing SPD

## 6. APPRAISAL

### Main Issues

The main issues are:

- i. Principle of redevelopment
- ii. Visual impact of the proposal
- iii. Residential amenity of neighbours and new occupiers
- iv. Trees
- v. Transport
- vi. Affordable Housing
- vii. Other matters

#### i. Principle of redevelopment

6.1 The application site lies within a suburban area of Reading and in a location relatively well served by amenities and on a bus route. None of the properties on site are listed and while they do have some merit in terms of their appearance the 4 dwellings on this c.3ha site make inefficient use (14 dwellings per hectare) of this sustainable site. The proposed scheme of 11 houses comprising 2 pairs of semi-detached houses and a terrace of 7 houses will give a density of 38 dwellings per hectare. This is still in the lower end of the range of target densities for accessible urban areas contained in Core Strategy CS15. However, Policy DM5 supports new housing provision of units larger than 3 bedrooms and all of the dwellings will meet this. The redevelopment of the site for additional residential development is therefore considered acceptable in principle and the rest of this report assesses the proposal against other material considerations.

#### ii. Visual impact of the proposal

6.2 Whilst this is an outline planning application, appearance is one of the reserved matters sought for approval at this stage. It is therefore considered appropriate for the LPA to satisfy itself that an acceptable design is likely to be achieved.

6.3 The site is currently occupied by 4 dwellings. 158 Dee Road is a 1970s property that is of no particular merit. 2 and 4 Water Road are a pair of Victorian properties with gables facing the junction. These properties are highly prominent, particularly in views along Grovelands Road. 6 Water Road is a large detached Edwardian property but less prominent than numbers 2 and 4.

6.4 The Council's Conservation consultant has confirmed that, whilst they have some merit, the three Water Road properties are not of sufficient quality to warrant inclusion on the Council's List of Locally Important Buildings and Structures. Nonetheless, they do make a positive contribution to the character of

the area and therefore any replacement should be of a good design which takes the opportunities available for improving the character and quality of the area.

6.5 The plans submitted for application 141022/FUL proposed three storey buildings in two terraces of four houses along Water Road, and two pairs of semi-detached houses along Dee Road. Whilst terraced and semi-detached properties are common in the area, officers considered that these dwellings would fail to integrate with the surrounding built form in terms of the built up coverage of each plot and building heights. The application proposed that the houses would be finished with a cream clay composite at ground floor level, with coloured render at first floor level and wooden boarding at second floor level. This palate of materials was considered to be an inappropriate contrast to the traditional brick of surrounding buildings. The design was considered to be confused, and of a low quality, and this was one of the reasons for refusing this application.

6.6 Plans originally submitted for the present application proposed a terrace of 8 x 3 storey dwellings on Water Road, a pair of 3 storey dwellings facing directly toward the junction and a pair of 3 storey dwellings on Dee Road. The second floor would be located within the roofspace, with rendered gables front and back. The roof would be in the form of a gambrel (a symmetrical two-sided roof with two slopes on each side, often known as a ‘Dutch Roof’). Officers were concerned that these plans failed to overcome the concerns identified during the previous application, and that the proposal would still fail to integrate with the surrounding area.

6.7 After a number of iterations amended plans have been received reducing the number of proposed dwellings to 11 and introducing a mix of sizes which is considered acceptable.



Proposed



Existing



6.8 The design of the existing houses on Dee Road uses relatively shallow roofs and the proposed houses on Dee Road will be taller than those on Dee Road. However, the new houses will be at a lower level than the existing houses which will allow this difference in height to not be particularly pronounced.

#### iii. Residential amenity of neighbours and new occupiers

6.9 The proposed houses will be set further back from Water Road than the existing properties. Their rear elevations will be positioned approximately 7.2 metres beyond the existing rear wall of 6 Water Road. However, the side boundary of 14 Water Road is approximately 10.2 metres from the new houses, on the far side of the access to 8 and 10 Water Road. Therefore the proposal will not have a detrimental impact on occupants of number 14 as a result of loss of light or being overbearing.

6.10 There is a significant planted screen on the boundary between No.8 and the application site and there are no side windows in the north east elevation of 8 Water Road. The potential for loss of privacy caused by overlooking from rear facing windows in the new dwellings is therefore reduced with these new windows being at least 10 metres from the boundary with No. 8.

6.11 The proposed Dee Road dwellings will be sited closer to 156 Dee Road than the current property at 158. However, the only windows in the flank wall of 156 Dee Road facing the application site are obscure glazed so the rooms they serve are unlikely to be habitable. The proposed houses would not extend significantly beyond the rear of no 156. The proposal is therefore not considered to have a detrimental impact on the occupants of this property in terms of loss of light or being overbearing.

6.12 The amenity space for the proposed dwellings range in size from 44 square metres to over 90 square metres so not all meet the basic size standards expected. However, this is balanced against the provision of landscaping and off road parking at the front of the site, which is why the plots have been pushed back into the site, and officers consider the gardens to be large enough to allow a reasonable level of amenity for future residents. It is also relevant that Prospect Park lies a short distance away at the top of Water Road.

6.13 The internal layout of the proposed dwellings, and the room sizes, are of an acceptable size. The dwellings will provide a good quality of accommodation.

6.14 Officers conclude that the proposal is not in conflict with the aims of Policy DM4 in terms of safeguarding the amenities of existing and future residents.

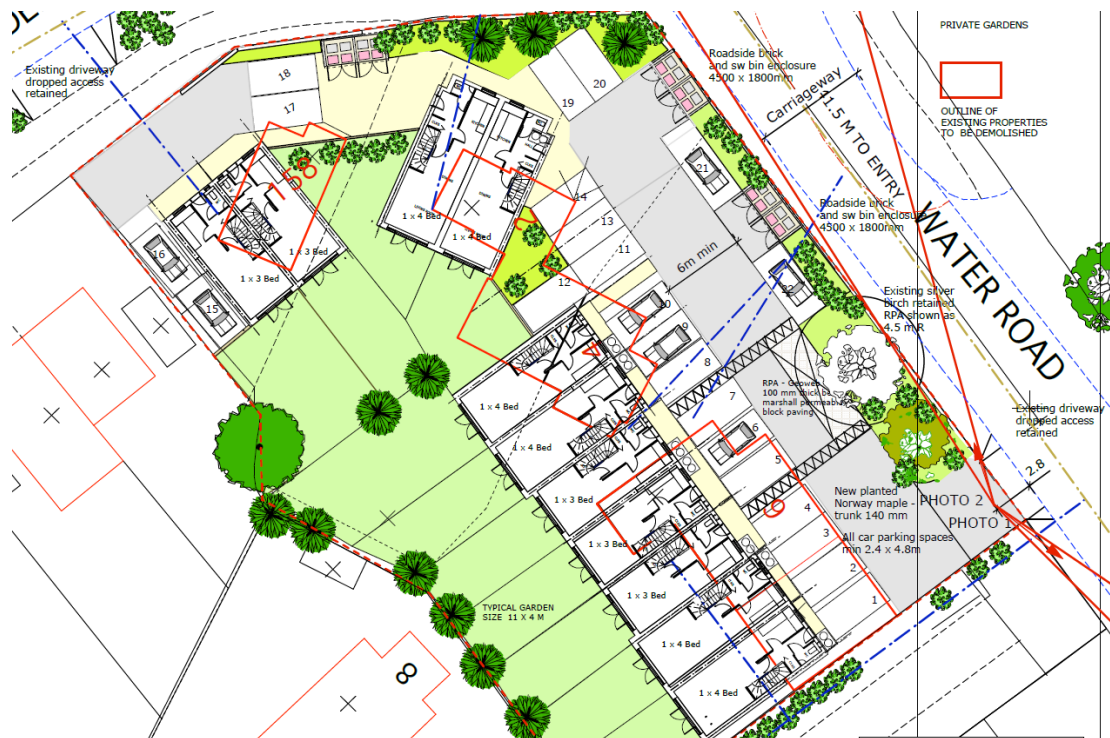
#### iv. Trees

6.15 The development site is located in a prominent location at the three-way junction of Water Road with Dee Road and Grovelands Road. This is visually a very open and green highway junction with wide verges and lawn areas on all sides and development set well back from the highway.

6.16 The Consultant Arboriculturalist drew attention to the mature tree cover to the rear of the existing properties and the potential for character to be harmed by a tall continuous terraced block of development. The amended scheme has reduced the length of the terrace from 34 metres to 29 metres and dropped the height of the dwellings by one metre. In accordance with the serving of the TPO on the two Silver Birches on the frontage the layout now retains both of these trees and more planting is proposed. It can now be confirmed that the proposal adequately addresses the landscaping protection and enhancement policies.

## v. Transport

6.17 The Transport Strategy officer (see comments above) has confirmed that the access arrangements for the new houses are acceptable and comply with our policies and guidance. Conditions are recommended to deal with matters of detail.



## v. Affordable Housing

6.18 Officers have negotiated with the applicant over how the development could remain viable and still make an acceptable affordable housing offer. Notwithstanding that the original offer was nil the negotiated outcome is that 2 of the houses (2 x 3 bed units on Dee Road) will be offered as affordable housing to a Registered Social Landlord and £75,000 will be paid to go towards providing affordable housing elsewhere in the Borough. Officers can confirm that this formula is policy compliant and will be secured with a S106 Agreement. This agreement will also include a default clause to secure finance in lieu of the 2 dwellings should an RSL (or the Council) not show interest in acquiring the units.

## vi. Other Matters

### 6.10 Equalities impact assessment

In determining this application the Committee is required to have regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, gender, sexual orientation. There is no indication or evidence (including from consultation on the application) that the protected groups have or will have different needs, experiences, issues and priorities in relation to the particular planning application.

In terms of the key equalities protected characteristics it is considered there would be no significant adverse impacts as a result of the development.

## 7. CONCLUSION

The proposed residential redevelopment is considered to comply with the relevant Development Plan Policies as assessed above. It is therefore recommended that

approval be granted, subject to the completion of a S106 Agreement and the recommended planning conditions.

Case Officer: Julie Williams



WATER ROAD ELEVATION GROUP 1 - 3 BEDROOM



GARDEN ELEVATION GROUP 1 - 3 BEDROOM



SECTION THROUGH 3 BED UNIT ON ELEVATION OF 4 BED UNIT

General Construction Specifications:

Frame: Fly Road timber frame with integral insulation to meet panel. Vapor check inside and out.

Floors: Clear spanning timber joist sections and ply fixed to form loadbearing floor.

External Walls: Ground and first floor facing bricks, cables to masonry fixed on timber frame. Cornices: half self-colored or brick to match main elev. Group 3 corner pan to have render panel returns to front.

Roofs: Cement slate tile on membrane and battens over insulated ply spanning panels and dormers.

Walls: Black UPVC gutters and 100 mm dia downpipes.

Windows: Triple glazed casement in timber clad metal core frames. Slated.

French windows: Triple glazed metal core, timber clad and slated.

- B: Roof design altered to dual pitch and lowered. Roof lights removed to front elev. 24.3.17
- A: Dormers removed from 2 storey houses to front elevations & rooflights added in place. 24.2.17

PLANNING APPLICATION

REVISIONS

1	Initial	24.2.17
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PDF ARCHITECTURE  
15, 16 & 17 DEE ROAD, READING, RG1 1AA

2,4,6 WATER ROAD READING REDEVELOPMENT

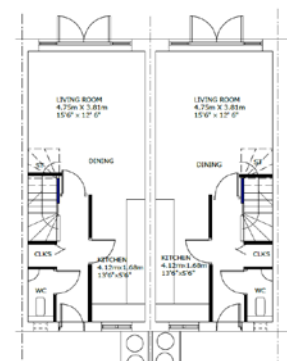
No plans submitted and not to be submitted without the approval of the Council. Planning permission is required for any work to be carried out.

SIDE, GARDEN AND WATER ROAD ELEVATIONS PROPOSED GROUP B

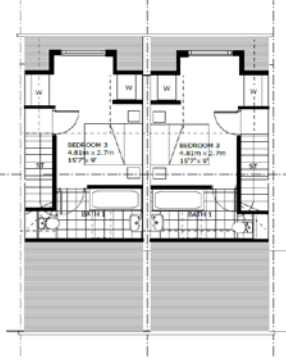
SCALE: 1:100 SITE: WATER: GARDEN:

Project No: 1583 Draw No: 1583.1 Rev: A

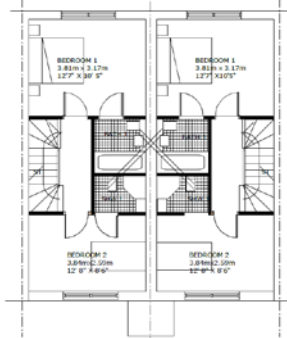
0 1 2 3 4 5



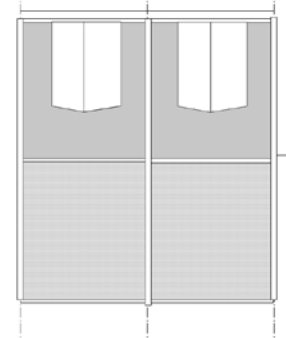
GROUND FLOOR PLAN LAYOUT - 3 BED GROUP 1



SECOND FLOOR PLAN LAYOUT - 3 BED GROUP 1



FIRST FLOOR PLAN LAYOUT 3 BED GROUP 1



ROOF PLAN GROUP B - 3 BED GROUP 1

General Construction Specifications:

Frame: Fly Road timber frame with integral insulation to meet panel. Vapor check inside and out.

Floors: Clear spanning timber joist sections and ply fixed to form loadbearing floor.

External Walls: Ground and first floor facing bricks, cables to masonry fixed on timber frame. Cornices: half self-colored or brick to match main elev. Group 3 corner pan to have render panel returns to front.

Roofs: Cement slate tile on membrane and battens over insulated ply spanning panels and dormers.

Walls: Black UPVC gutters and 100 mm dia downpipes.

Windows: Triple glazed casement in timber clad metal core frames. Slated.

French windows: Triple glazed metal core, timber clad and slated.

- B: Roof design altered to dual pitch and lowered. Roof lights removed to front elev. 24.3.17
- A: Dormers removed from 2 storey houses to front elevations & rooflights added in place. 24.2.17

PLANNING APPLICATION

REVISIONS

1	Initial	24.2.17
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PDF ARCHITECTURE  
15, 16 & 17 DEE ROAD, READING, RG1 1AA

2,4,6 WATER ROAD READING REDEVELOPMENT

No plans submitted and not to be submitted without the approval of the Council. Planning permission is required for any work to be carried out.

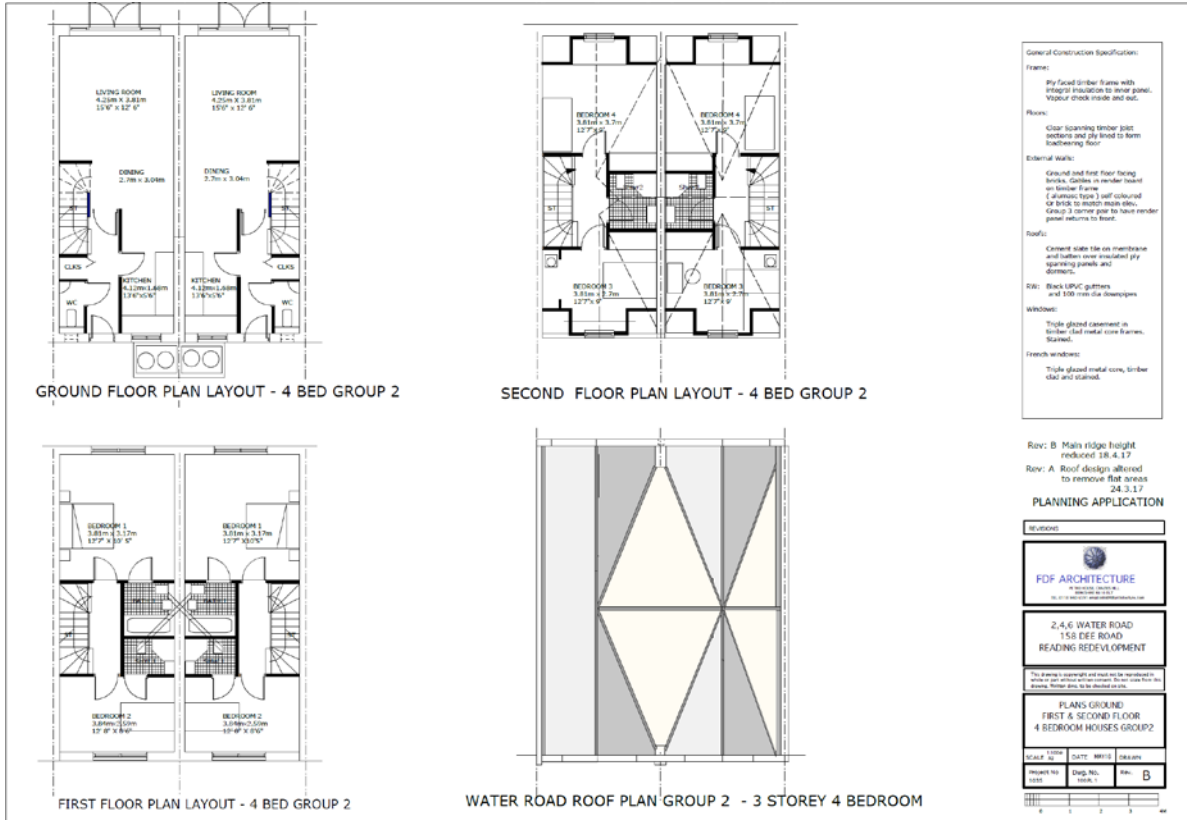
PLANS GROUND FIRST & SECOND FLOOR 3 BEDROOM HOUSES GROUP 1/4

SCALE: 1:100 SITE: WATER: GARDEN:

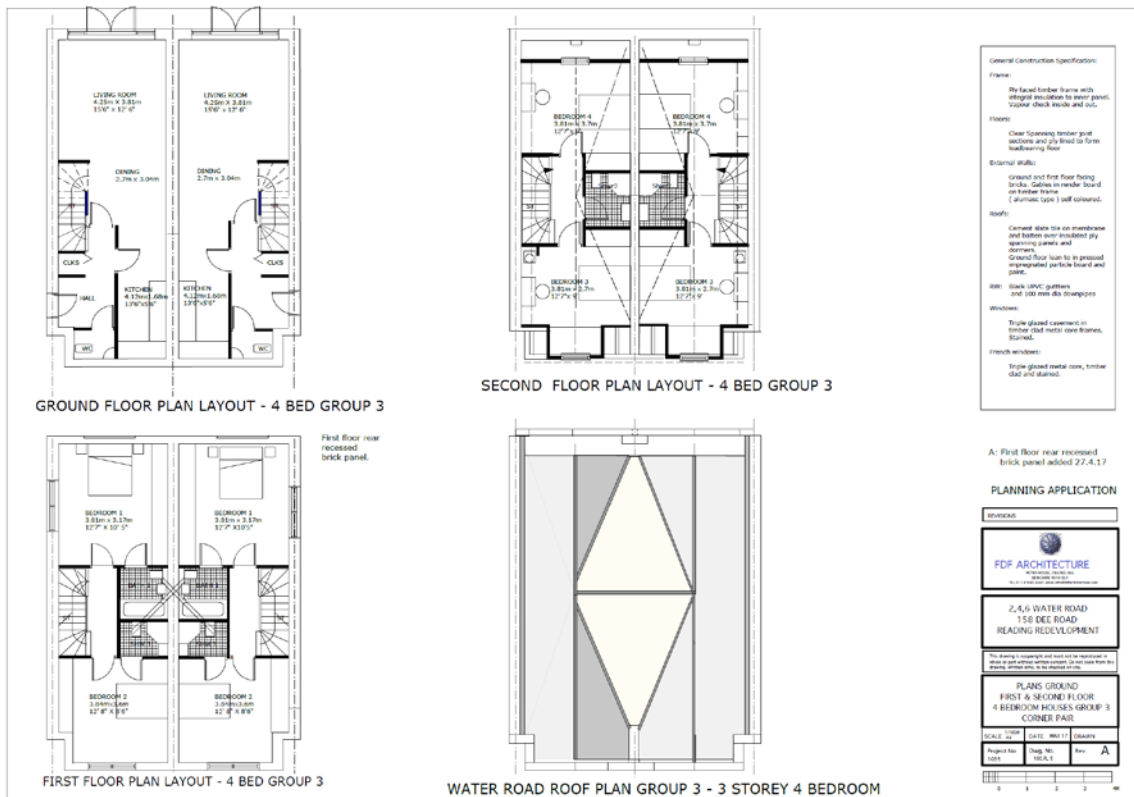
Project No: 1583 Draw No: 1583.1 Rev: B

0 1 2 3 4 5

Three Bedroom units in mid terrace and proposed facing Dee Road



End blocks of terrace



Pair of 4 Bedroom corner units facing onto roundabout





<b>Z, A &amp; B WATER ROAD READING REDEVELOPMENT</b>	
<b>MAIN ELEVATIONS PROPOSED SHEET 1</b>	
Project No: 2021/001	Date: 15/01/2024
Page No: 1	Rev: 0



<b>Z, A &amp; B WATER ROAD, 156 DEE ROAD READING REDEVELOPMENT</b>	
<b>MAIN ELEVATIONS PROPOSED SHEET 2</b>	
Project No: 2021/001	Date: 15/01/2024
Page No: 2	Rev: 0

Elevations



VIEW 1A  
FROM NORTH



VIEW 2  
FROM SOUTH



VIEW 3A  
FROM EAST

2,4,6 WATER ROAD 158 DEE ROAD READING RG30  
AERIAL VIEWS OF PROPOSALS





**PARK**



## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT & NEIGHBOURHOOD SERVICES  
READING BOROUGH COUNCIL  
PLANNING APPLICATIONS COMMITTEE: 27 June 2018

ITEM NO. 13

Ward: Park

App No.: 180786/FTL

Address: Fire Station, Wokingham Road, Reading, RG6 1JU.

Proposal: Telecommunications application for replacement of 1 no. existing flagpole antenna to top of fire training tower with 1 no. new tri-sector antenna. Installation of 1 no. equipment cabinet within the existing cabin, plus associated ancillary development.

Applicant: H3G UK Ltd.

Date validated: 17 May 2018

8 week target decision date: 12 July 2018

### RECOMMENDATION

Grant Full Planning Permission

### CONDITIONS TO INCLUDE

1. Full - time limit - three years
2. Standard approved plans condition

### INFORMATIVES TO INCLUDE

1. Standard positive and proactive informative.

## 1. INTRODUCTION

- 1.1 The site fronts Wokingham Road at the corner with Holmes Road and comprises the existing fire training tower to the north of the main fire station building within the main fire station compound.
- 1.2 A number of telecommunications antennae exist on the building including one disguised as a flagpole, which is to be replaced.
- 1.3 The site is located close to the boundary with Wokingham Borough.



Site Location Plan



### Site Photograph

## 2. PROPOSAL

- 2.1 Full Planning Permission is sought for the replacement of the existing 4.5m tall flagpole antenna with a 5.5 metre tall mast to the top of the existing tower. The overall height above ground level would be 18.7 metres.
- 2.3 The supporting statement submitted with the application explains that there is a specific requirement for a radio base station upgrade at this location to provide the 4G service; the existing mast is limited to 3G.
- 2.4 A declaration has been submitted by the applicant confirming compliance with the International Commission on Non-ionizing Radiation (ICNIRP) guidelines.

## 3. PLANNING HISTORY

- 3.1 990789 Erection of a 3.5 metre stub mast on the drill tower with 3 x cross polar antennas and 4 dish antennas attached, together with an equipment cabin at ground level. Approved
- 3.2 020753 Single storey pitch roof brick building together with 3 dual band dual polar pole mounted antenna on existing training tower and 2.38m high GRP screen on existing tower finished to match existing structure. Approved

## 4. CONSULTATIONS

### 4.1 Statutory:

- Wokingham Borough Council - Confirm no objection.

### 4.2 Non-statutory:

- RBC Environmental Protection (EP)  
An ICNIRP certificate has been submitted. EP therefore have no objections.
- RBC Natural Environment (NE)

All the works appear to be on or within existing structures and within the site, i.e. behind the front boundary wall. As such, it should not affect the Plane tree on the grass area fronting Wokingham Road hence NE have no objections.

#### 4.3 Public consultation:

- Site notices were displayed on Wokingham Road close to the boundary of the site and also at the corner of Wokingham Road and Holmes Road.
- Letters were sent to addresses surrounding the site and to local schools and other educational establishments.
- No representations were received.

### 5. RELEVANT PLANNING POLICY AND GUIDANCE

5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that proposals be determined in accordance with the development plan unless material considerations indicate otherwise. Material considerations include relevant policies in the National Planning Policy framework (NPPF) - among them the 'presumption in favour of sustainable development'.

5.2 Full Planning Permission has been applied for as the development exceeds permitted development rights under Class A, Part 16 of the Town and Country Planning (General Permitted Development) (England) Order 2015.

5.3 The following local and national planning policy and guidance is relevant to this application:

#### 5.4 National Planning Policy Framework

Part 5 - Supporting high quality communications infrastructure

Part 7 - Requiring good design

#### 5.5 Reading Borough Local Development Framework Core Strategy CS7 (Design and the Public Realm)

#### 5.6 Sites and Detailed Policies Document

SD1 (Presumption in Favour of Sustainable Development)

DM21 (Telecommunications Development)

### 6. APPRAISAL

6.1 Policy DM21 states that proposals for telecommunications development will be permitted provided that:

- They do not have an adverse impact on the visual amenity of the surrounding area;
- The apparatus will be sited and designed so as to minimise its visual impact by the use of innovative design solutions such as lamp column 'swap-outs' or concealment/camouflage options; and
- Alternative sites and site-sharing options have been fully investigated and it has been demonstrated that no preferable alternative sites are potentially available which would result in a development that would be less visually intrusive.

*Impact on Visual Amenity*

- 6.2 The proposal involves the removal of an existing 3.5 metre high ‘flagpole’ design and its replacement with a wider diameter 5.5 metre high monopole. The proposal includes the provision of a new ancillary equipment cabinet within an existing cabin at ground level.
- 6.3 The mast would be clearly visible from the adjacent roads and nearby dwellings. The increase in height and diameter compared to the existing mast would increase its prominence. However it is considered that its siting on a functional structure in amongst other antenna would reduce its potential obtrusiveness and the proposal would not be visually harmful within this context.

*Alternative Sites*

- 6.4 The re-use of existing sites, such as that currently proposed, is in accordance with paragraph 43 of the NPPF and is within the spirit of Sites and Detailed Policies Document Policy DM21 which encourages lamppost swap-outs, i.e. the replacement of one structure with another to minimise the visual impact. On this basis, and taking into account the lack of visual harm identified above, it is considered that an alternative site is not required for the proposed development.

*Equalities impact assessment*

- 6.5 In determining this application the Committee is required to have regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, gender, sexual orientation. There is no indication or evidence (including from consultation on the application) that the protected groups have or will have different needs, experiences, issues and priorities in relation to the particular planning application. In terms of the key equalities protected characteristics it is considered there would be no significant adverse impacts as a result of the development.

7. CONCLUSION

- 7.1 The proposal is considered to comply with the relevant Development Plan Policies, and national policy guidance, as assessed above. It is therefore recommended that approval be granted, subject to suitable conditions.

Drawings:

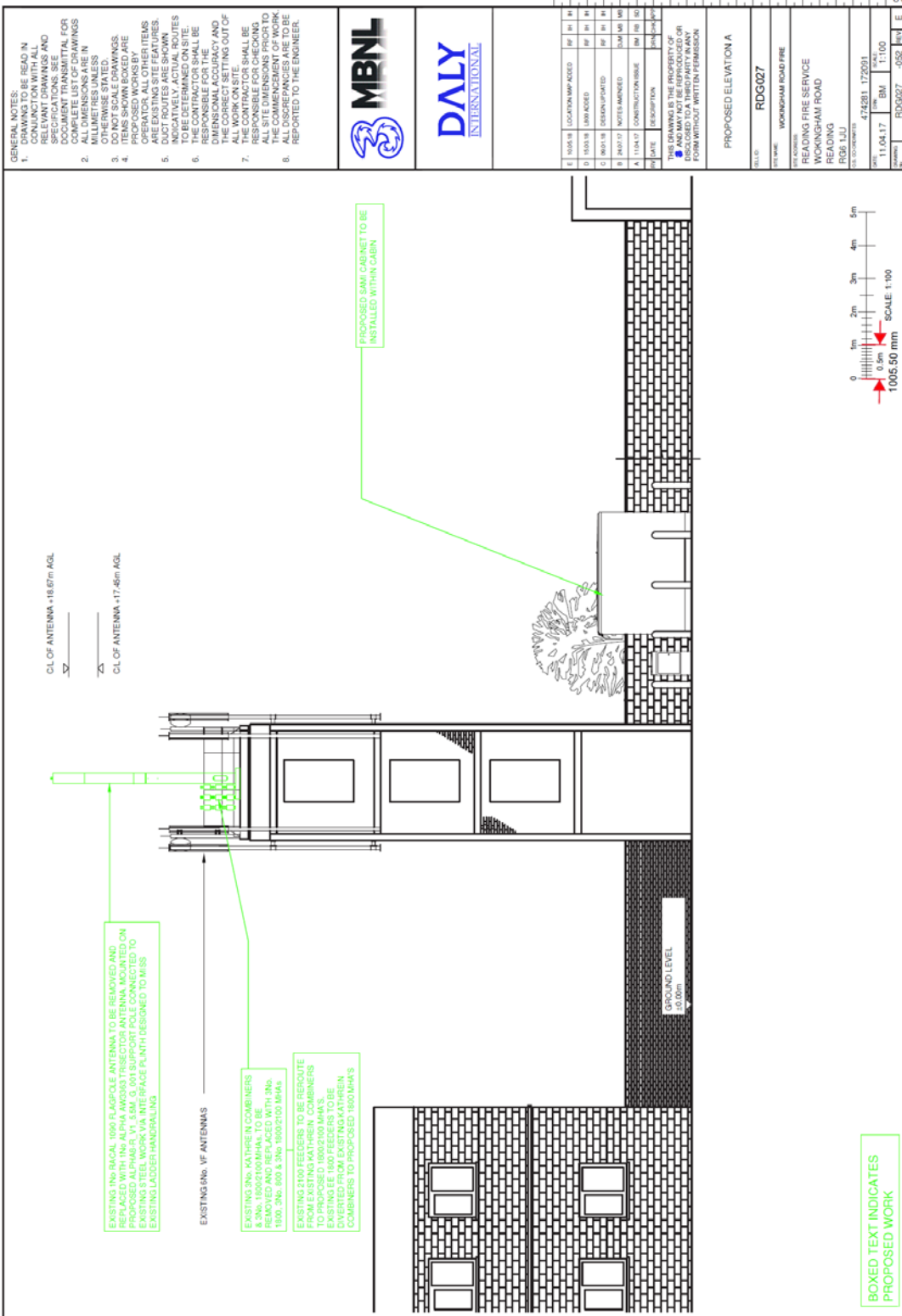
RDG027 -050 Rev.E dated 10 May 2018

RDG027 -051 Rev.E dated 10 May 2018

RDG027 -052 Rev.E dated 10 May 2018

Case Officer: Steve Vigar





Proposed Elevation

**GENERAL NOTES:**

- DRAWINGS TO BE READ IN CONJUNCTION WITH ALL RELEVANT DRAWINGS AND SPECIFICATIONS. A COMPLETE LIST OF DRAWINGS IS PROVIDED ON SHEET 01.
- ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE STATED.
- DO NOT SCALE DRAWINGS. ITEMS SHOWN BOXED ARE TO BE INSTALLED BY THE CONTRACTOR. ALL OTHER ITEMS ARE EXISTING SITE FEATURES. DUCT ROUTES ARE SHOWN FOR INFORMATION ONLY. DIMENSIONS TO BE DETERMINED ON SITE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CORRECT SETTING OUT OF ALL WORK ON SITE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL SITE DIMENSIONS PRIOR TO THE COMMENCEMENT OF WORK. ALL DISCREPANCIES ARE TO BE REPORTED TO THE ENGINEER.

**3 MBNL**

**DALY INTERNATIONAL**

REV	DATE	DESCRIPTION	BY	CHK
E	15/05/18	LOCATION MAP ADDED	RF	SH
D	15/05/18	LINK ADDED	RF	SH
C	06/01/18	DESIGN UPDATED	RF	SH
B	24/07/17	NOTES IMPROVED	DM	MS
A	11/04/17	CONSTRUCTION ISSUE	DM	MS

THIS DRAWING IS THE PROPERTY OF MBNL AND MAY NOT BE REPRODUCED OR TRANSMITTED IN ANY FORM WITHOUT WRITTEN PERMISSION.

**PROPOSED ELEVATION A**

**RDG027**

**WORKINGHAM ROAD FIRE**

**READING FIRE SERVICE**

**WORKINGHAM ROAD**

**READING**

**R6E LJJ**

DATE: 11.04.17  
SCALE: 1:100  
DRAWN BY: RDG027  
CHECKED BY: E



**PEPPARD**



## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT AND NEIGHBOURHOOD SERVICES  
READING BOROUGH COUNCIL  
PLANNING APPLICATIONS COMMITTEE: 27<sup>th</sup> June 2018  
ITEM NO. 14

Ward: Peppard  
App No.: 180720/REG3  
Address: 11 Knights Way, Emmer Green, Reading, RG4 8RJ  
Proposal: Proposed single storey rear extension  
Applicant: Mr Paul Barnfarther - Reading Borough Council Property Services  
Date validated: 30<sup>th</sup> April 2018  
Other Application: 8 week target decision date: 25<sup>th</sup> June 2018  
Agreed extension of time: 4<sup>th</sup>. July 2018

**RECOMMENDATION:**  
**GRANT**

Conditions:

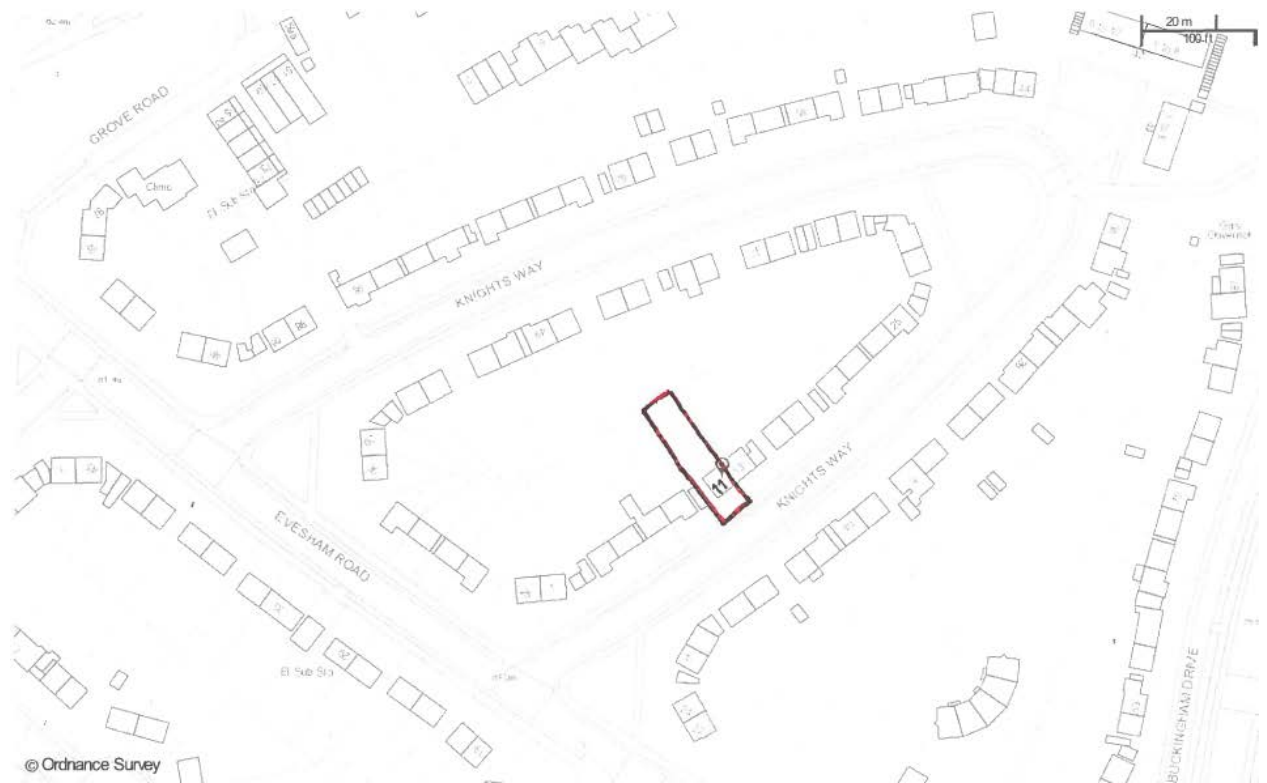
1. Tl1 Time limit for implementation
2. M1 Use of materials
3. Ap1 Approved plans

Informatives:

1. Terms and conditions
2. Building Control
3. Complaints about construction
4. Positive and proactive

### 1. INTRODUCTION

- 1.1 The application relates to a two storey semi-detached dwelling on the north west side of Knights Way. The main architectural feature of the application property is the prominent two storey bay in the front elevation of the building. The application site has a rear garden approximately 25m long and 9m wide. The surrounding area is predominantly residential consisting of similar style semi-detached and terrace properties.
- 1.2 This minor application is reported to Planning Applications Committee as a Regulation 3 Planning Approval as Reading Borough Council is the applicant.



## 2. PROPOSAL

- 2.1 The proposal seeks planning permission for a single storey rear extension. The proposal would project 3m beyond the existing rear elevation of the lounge and 4.7m beyond the existing rear elevation of the kitchen. The proposed extension would have a lean-to mono-pitch roof form with a projecting gable. The proposal would have a maximum height of 3.4m and an eaves height of 2.4m. Four roof lights are proposed in the lean-to roof, two windows are proposed in the rear elevation and one window is proposed in the south west side elevation. The external materials proposed match the existing house.

## 3. PLANNING HISTORY

- 3.1 None.

## 4. CONSULTATIONS

- 4.1 Public consultation  
 No.s 9, 13, 43 and 45 Knights Way have been notified of the application and site notice has also been displayed at the site from the 4<sup>th</sup> May 2018.  
 No responses have been received.

## 5. RELEVANT PLANNING POLICY AND GUIDANCE

- 5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that proposals be determined in accordance with the development plan unless material considerations indicate otherwise. The Development Plan for Reading relevant to the application site comprises the Reading Local

Development Framework 'Core Strategy' 2008 (Altered 2015) and 'Sites and Detailed Policies Document' 2012 (Altered 2015).

- 5.2 The 'National Planning Policy framework' ('NPPF') 2012 states clearly that its content is to be a material consideration in the determination of applications. The 'NPPF' states that due weight should be given to the adopted policies of the Local Development Framework (LDF) (Core Strategy and Sites and Detailed Policies Document) according to their degree of consistency with the 'NPPF' (the closer the policies in the plan to the policies in the 'NPPF', the greater the weight that may be given). Accordingly, the 'NPPF' and the following development plan policies and supplementary planning guidance are relevant:

National Planning Guidance

National Planning Policy Framework: Chapter 7. Requiring Good Design

Reading Borough Local Development Framework Core Strategy (2008) Policies

CS7 (Design and the Public Realm)

Sites and Detailed Policies Document (2012) Policies:

DM4: Safeguarding Amenity

DM9: House Extensions and Ancillary Accommodation

Supplementary Planning Guidance - A Design Guide to House Extensions (2003)

## 6. APPRAISAL

Design, impact on the host dwelling, character of the area and street scene

- 6.1 As the proposed extension is to the rear of the property it would not be visible from the streetscene of Knights Way. Being set back 4.1m from the boundary with no.9 Knights Way the proposal would retain the existing spaciousness to the boundary and would not appear unduly cramped. The proposal would be set 0.2m off the boundary with no.13 Knights Way, and given the semi-detached nature of the application property this is considered to follow the character and pattern of the application site and the surrounding area.
- 6.2 The extension would project 4.7m from the existing rear elevation of the kitchen. Whilst this is more than the 4m recommended in the Council's 'A Design Guide to House Extensions' SPG, given the length of the plot, the scale of the host dwelling and considering the single storey nature of the proposal; the longer extension is considered appropriate in this instance. Made with materials to match existing, the proposed extension is considered to integrate satisfactorily with the character of the host dwelling and visual amenity of the surrounding area.
- 6.3 The host dwelling would retain sufficient amenity space to the rear elevation, which is considered adequate for a dwelling of this size and relative to the character of the amenity spaces of surrounding dwellings.

- 6.4 The proposal is therefore considered to be in accordance with Policy CS7 of the Core Strategy (2008, 2015) and Policies DM9 and DM10 of the Sites and Detailed Policies Document (2012, 2015).

Impact on neighbouring amenity

- 6.5 The properties potentially affected by the proposal are no.9 and no.13 Knights Way.
- 6.6 Regarding no.9, the proposal would retain the existing 4.1m gap to the common boundary. Further, the existing detached garage at no.9 is between the proposal and the main house at no.9. Combining the large separation distance between the proposal and the habitable rooms at no.9, the single storey nature of the proposal - with an eaves height of 2.4m - and the existing boundary treatment; the result is that the proposal is not considered to have any adverse effect in terms of loss of light or overbearing. Whilst a side facing window is proposed, it would be at ground floor level and given the separation distance to the habitable rooms at no.9, it is considered that the proposal would not result in a material loss of privacy or an unacceptable overlooking impact.
- 6.7 On the side closest to no.13, the proposed extension would project 3m from the existing rear elevation, have an eaves height of 2.4m and be sited 0.2m from the boundary. Given the existing rear conservatory at no.13 which is located close to the boundary, the relatively modest depth of the extension and that the proposal would be single storey; it is considered that the proposed extension would not cause a significant detrimental impact to the living environment in terms of visual dominance and overbearing effects or access to sunlight and overshadowing. As the proposed extension would be single storey with no windows in the side elevation facing no.13, it is not considered that the proposal would result in a significant adverse impact in relation to privacy or overlooking.
- 6.8 The proposal is therefore considered to be in accordance with Policies DM4 and DM9 of the Sites and Detailed Policies Document (2012, 2015).

## 7. CONCLUSION

The proposed development is considered acceptable in planning terms and for the reasons given above.

Plans:

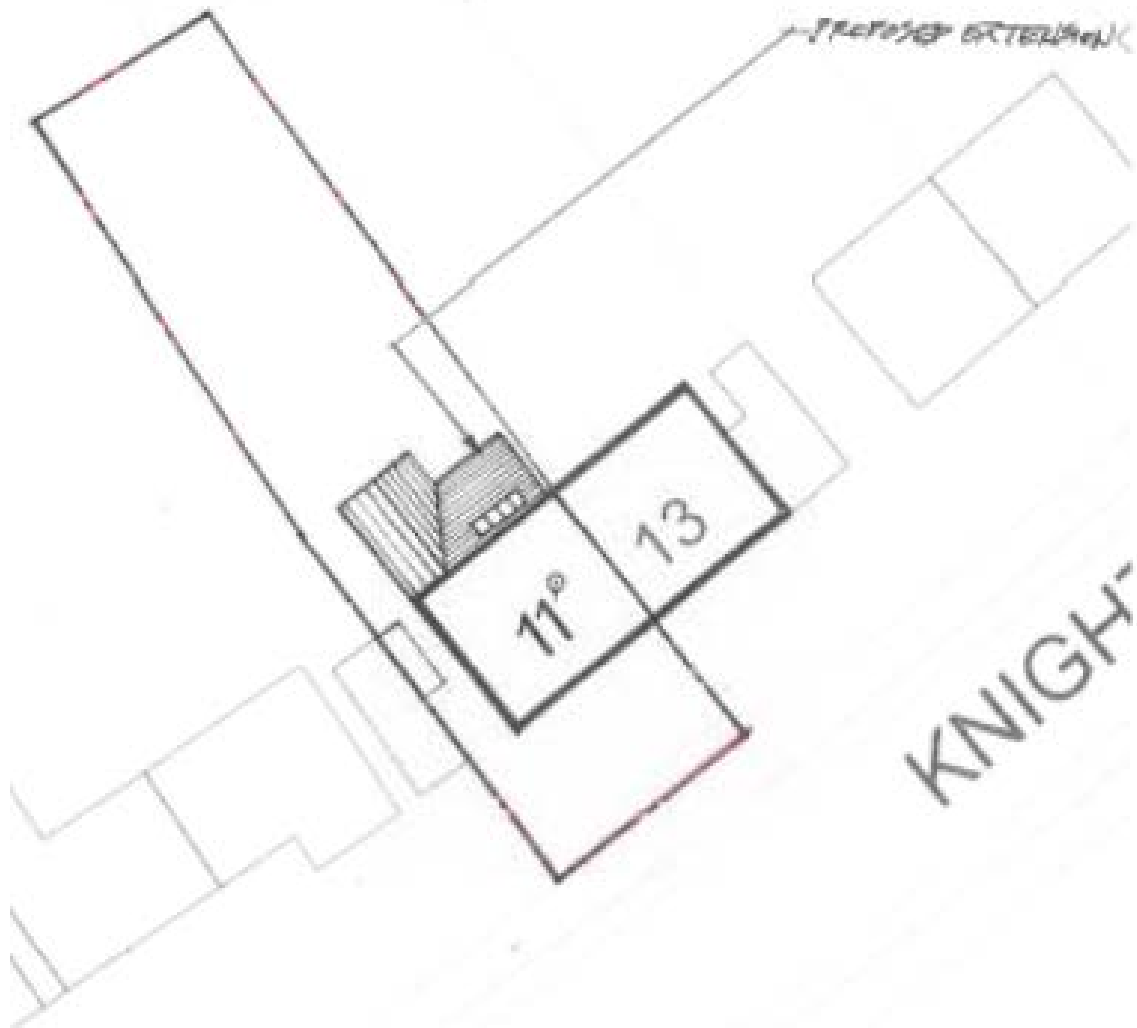
Drawing No.:

- KNW - ELEVS1
- KNW - ELEVS2
- KNW - ELEVS3
- KNW - P2
- KNW - P3
- KNW - SP1
- KNW - BP1
- KNW - LP1

As received: 30/04/2018

Case Officer: Tom French

11 KNIGHTS WAY, EMMER GREEN, READING RG4 8RJ



DRAWING No KJW-SP1

PROPOSED SITE PLAN      APRIL 2018

1:250

RBC PROPERTY SERVICES

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Scale: 1:250

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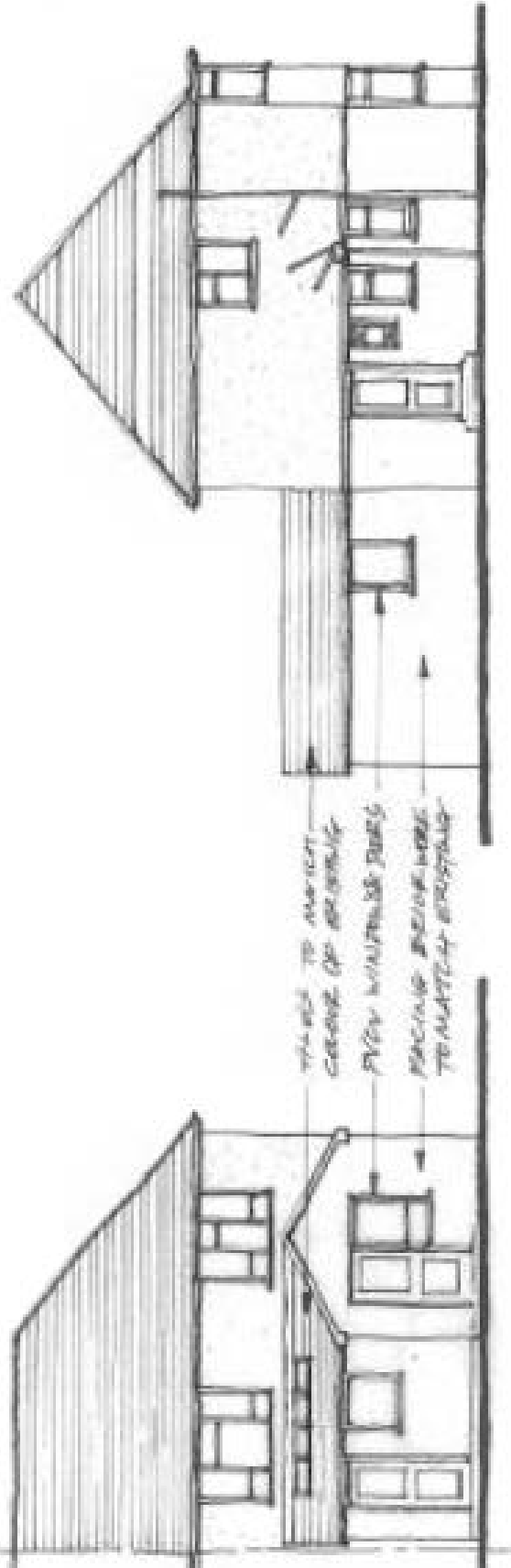


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11 KNIGHTS WAY, EMMER GREEN, READING RG4 0RJ - ELEVATIONS



REAR ELEVATION AS PROPOSED  
(SOUTH WEST)

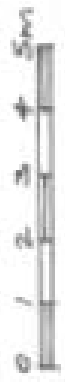
SIDE ELEVATION AS PROPOSED  
(SOUTH WEST)

DRG No KNN - ELEV 2

APRIL 2018

SCALE 1:100

 Reading RBC PROPERTY SERVICES





# REDLANDS



## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT AND NEIGHBOURHOOD SERVICES  
READING BOROUGH COUNCIL  
PLANNING APPLICATIONS COMMITTEE: 27 June 2018

ITEM NO. 15

Ward: Redlands

App No.: 180144/FUL

Address: 25 Redlands Road, Reading

Proposal: Demolition of a single-storey rear projection, followed by the construction of a single-storey rear extension, internal modifications and refurbishment to facilitate change of use from a single dwelling house with detached garage (C3a) to 5 no. self-contained flats (C3a) with associated car parking, bin and cycle storage.

Applicant: Mr Paul Kilshaw

Minor Application 8 week target decision date: 21 March 2018. Extended to 27 July 2018

### RECOMMENDATION

Delegate to the Head of Planning, Development & Regulatory Services to GRANT Full Planning Permission, subject to conditions and informatives and subject to the satisfactory completion of a S.106 legal agreement, or REFUSE permission should the legal agreement not be completed by 27 July 2018 unless a later date is agreed by the Head of Planning, Development & Regulatory Services.

The Section 106 Legal Agreement to secure the following:

£13,000 - towards the provision by the Council of Affordable Housing in the Borough. Payable prior to first occupation and index-linked from the date of permission.

### CONDITIONS TO INCLUDE:

1. TL1 - Full - time limit - three years.
2. Approved Drawings.
3. Materials
4. Provision of bin stores in accordance with approved drawings, prior to occupation.
5. DC1 - Vehicle parking spaces to be provided in accordance with approved plans
6. The covered bicycle storage spaces shown on the approved drawings shall be provided and equipped with secure Sheffield cycle stands prior to occupation of the dwellings to which they relate.
7. Pre-commencement submission and approval of hard and soft landscaping details.
8. Hard and soft landscaping to be implemented in accordance with the approved plans and documents.
9. Maintenance of planted materials for 5 years with replacement if required.
10. Pre-commencement submission of Arboricultural Method and Tree Protection Plan.
11. Prior to occupation a management agreement to be submitted to and approved by the Local Planning Authority, which covers the details of use of the car parking
12. Prior to occupation a management agreement to be submitted to and approved by the Local Planning Authority, which covers the details of the maintenance of the landscaping.
13. The layout, number and size of units to be retained as shown on the approved plans.
14. The residential flats hereby approved shall not be occupied until the Council has been notified in writing of the full postal address of the units.
15. Prior to any agreement being entered into for a new occupation of, or transfer of any interest in, the residential flats hereby approved the prospective occupier/transferee

shall be informed of the prohibition on entitlement to a car parking permit for any existing residential parking permit schemes and future schemes on adjacent and surrounding streets. All material utilised for advertising or marketing the residential flats for letting or sale shall make it clear to prospective tenants and occupiers that there is no automatic right to a parking permit.

16. Hours of working - construction and demolition phase.
17. No bonfires on site during demolition or construction.

**INFORMATIVES TO INCLUDE:**

1. Terms and conditions.
2. Building regulations.
3. Pre-Commencement conditions
4. Encroachment
5. Sound Insulation to meet Building Regulations requirements
6. Damage to the highway
7. No parking permits for occupiers
8. Works affecting the highway
9. Environmental protection information regarding the control of nuisance during construction and demolition.
10. Housing Act requirements
11. Insulation requirements to achieve those set out in Building Regs Part E
12. CIL
13. Positive and proactive.

**1. INTRODUCTION**

1.1 Consideration of this application was due to take place at the last Planning Applications Committee on 30<sup>th</sup> May 2018 but was deferred before being discussed to allow Councillors time to carry out an accompanied site visit to the property for further information relevant to their reaching a decision. The visit took place on 21 June 2018.

1.2 The officer recommendation is set out above (with target timescale extended) and the full report from 30<sup>th</sup> May 2018 is appended.

BY THE DIRECTOR OF ENVIRONMENT AND NEIGHBOURHOOD SERVICES  
 READING BOROUGH COUNCIL  
 PLANNING APPLICATIONS COMMITTEE: 30 May 2018

ITEM NO.14

Ward: Redlands

App No.: 180144/FUL

Address: 25 Redlands Road, Reading

Proposal: Demolition of a single-storey rear projection, followed by the construction of a single-storey rear extension, internal modifications and refurbishment to facilitate change of use from a single dwelling house with detached garage (C3a) to 5no. self-contained flats (C3a) with associated car parking, bin and cycle storage.

Applicant: Mr Paul Kilshaw

Minor Application 8 week target decision date: 21 March 2018. Extended to 8 June 2018

#### RECOMMENDATION

Delegate to the Head of Planning, Development & Regulatory Services to GRANT Full Planning Permission, subject to conditions and informatives and subject to the satisfactory completion of a S.106 legal agreement, or REFUSE permission should the legal agreement not be completed by 8<sup>th</sup> June 2018 unless a later date is agreed by the Head of Planning, Development & Regulatory Services.

The Section 106 Legal Agreement to secure the following:

£13,000 - towards the provision by the Council of Affordable Housing in the Borough. Payable prior to first occupation and index-linked from the date of permission.

#### CONDITIONS TO INCLUDE:

18. TL1 - Full - time limit - three years.
19. Approved Drawings.
20. Materials
21. Provision of bin stores in accordance with approved drawings, prior to occupation.
22. DC1 - Vehicle parking spaces to be provided in accordance with approved plans
23. The covered bicycle storage spaces shown on the approved drawings shall be provided and equipped with secure Sheffield cycle stands prior to occupation of the dwellings to which they relate.
24. Pre-commencement submission and approval of hard and soft landscaping details.
25. Hard and soft landscaping to be implemented in accordance with the approved plans and documents.
26. Maintenance of planted materials for 5 years with replacement if required.
27. Pre-commencement submission of Arboricultural Method and Tree Protection Plan.
28. Prior to occupation a management agreement to be submitted to and approved by the Local Planning Authority, which covers the details of use of the car parking
29. Prior to occupation a management agreement to be submitted to and approved by the Local Planning Authority, which covers the details of the maintenance of the landscaping.
30. The layout, number and size of units to be retained as shown on the approved plans.
31. The residential flats hereby approved shall not be occupied until the Council has been notified in writing of the full postal address of the units.
32. Prior to any agreement being entered into for a new occupation of, or transfer of any

interest in, the residential flats hereby approved the prospective occupier/transferee shall be informed of the prohibition on entitlement to a car parking permit for any existing residential parking permit schemes and future schemes on adjacent and surrounding streets. All material utilised for advertising or marketing the residential flats for letting or sale shall make it clear to prospective tenants and occupiers that there is no automatic right to a parking permit.

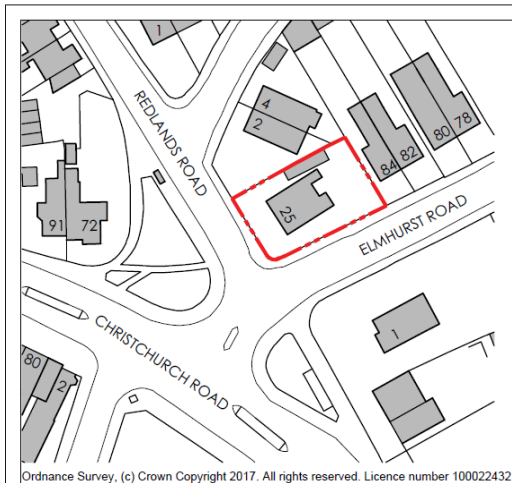
33. Hours of working - construction and demolition phase.
34. No bonfires on site during demolition or construction.

**INFORMATIVES TO INCLUDE:**

14. Terms and conditions.
15. Building regulations.
16. Pre-Commencement conditions
17. Encroachment
18. Sound Insulation to meet Building Regulations requirements
19. Damage to the highway
20. No parking permits for occupiers
21. Works affecting the highway
22. Environmental protection information regarding the control of nuisance during construction and demolition.
23. Housing Act requirements
24. Insulation requirements to achieve those set out in Building Regs Part E
25. CIL
26. Positive and proactive.

## 2. INTRODUCTION

- 2.1 The existing property is a large detached early 20<sup>th</sup> Century house with on-site parking at the front, accessed off Redlands Road with garden wrapped round the east side of the house alongside Elmhurst Road and extending to the rear. The site lies in the Redlands Conservation Area but the house is not Listed.



Site location plan



View of 25 Redlands Road

- 1.2 The property was last used as and is laid out as a family home. The plan above shows the relationship of the property to adjacent houses and streets and the closeness to the traffic light controlled junction. Redlands Road and Christchurch Roads are bus routes. On the far side of Elmhurst Road is the University of Reading campus.



### 3. PROPOSAL

2.1 Permission is sought to convert the house to 1 x 2 bed and 3 x 1 bed flats and to convert the garage to a 1 bed studio. The filling in of an undercroft area and a single storey extension to the rear is also proposed (amended plans show the size of the single storey extension proposed reduced and the existing front elevation of the garage retained). This minor category planning application was called in to be determined by Planning Applications Committee by Councillors Gavin and Josh Williams in response to concerns raised by neighbours.

2.2 The floorspace would be as follows:

*Ground floor -*

- Flat 1 - 1 bed - 30 sqm
- Flat 2 - 2 bed - 62.5 sqm
- Garage - 1 bed studio - 27.5 sqm

*First Floor -*

- Flat 3 - 1 bed - 39 sqm
- Flat 4 - 1 bed - 43.5 sqm

2.3 3 no. car parking spaces are proposed using the existing vehicular access.

2.4 An area of communal amenity space as well private space for the garage studio and 2 bed flat. Additional planting proposed.

2.5 Cycle store and bin area proposed.

**Plans.**

Drawing 17009-PL-1 Location / Proposed Block Plans

Drawing 17009-PL-2 Existing Site Plan

Drawing 17009-PL-3 Existing Floor Plans

Drawing 17009-PL-4 Existing Elevations

Drawing 17009-PL-5 Rev B Proposed Site Plan

Drawing 17009-PL-6 Rev A Proposed Floor Plans

Drawing 17009-PL-7 Rev A Proposed SW & SE Elevations

Drawing 17009-PL-8 Rev A Proposed NW, NE & Garage Elevations

**Statements:**

Heritage Statement

Design & Access (DAS) statement (updated on 16/5/18).

2.6 The DAS explains; *“The existing dwelling on the site has a floor area of 189.5 sq.m (G.I.A.) with an additional 31.1sq.m. (G.I.A.) provided by the detached garage. The total floor area of the proposed residential development is 230.2 sq.m (G.I.A.). The additional 9.6 sqm within the proposal will be provided by the single-storey rear extension and enclosed loggia, however there is only a 5.3 sqm enlargement to the building footprint. The proposed development provides 154.0 sq.m of shared amenity space, 37.0 sq.m of private terrace area to Unit 2 and 21.8sq.m of private terrace area to Unit 5. Secure and covered storage for 4no. bicycles and appropriate bin storage area are also proposed with access at the front of the property, as suggested on pre-application advice report.”*

#### 4. PLANNING HISTORY

- 4.1 None apart from pre-app enquiries last year exploring the options of converting the property to a large HMO or self-contained flats.

#### 5. CONSULTATIONS

Statutory:

None required

Non-statutory:

RBC Natural Environment (Trees/Landscape):

As advised at pre-app, the proposal is acceptable in principle subject to landscaping/mitigation for tree loss being acceptable. I note that 3 new trees are indicated, one being to the rear of the garage as a direct replacement for one to be felled and another two in the shared garden directly adjacent to the north-east elevation.

The proposed tree to the rear of the garage will be in a terraced area, as opposed to a grass area, so the tree pit design will need careful consideration. Given the proximity of the other two trees in the shared lawn area to the building, the species choice will be limited to small species, hence any public amenity gained from these in the future will be negligible.

It was suggested at pre-app that consideration be given to tree planting in a more prominent location, which in this case would be on the Redlands Road frontage. However, the proposed parking and access (although the access point is not clear) would preclude this.

I assume the existing boundary treatment on the Redlands Road frontage will be remaining? It is not clear from the proposed plans.

In relation to retained trees, the cycle store is proposed between two trees and most likely within their RPAs. Consideration will therefore need to be given to the construction of this to avoid root disturbance. The applicant should also confirm whether any pruning is required, e.g. crown lifting, to provide clearance from the cycle store - without this being included in the planning application, a separate Section 211 (for tree works in a conservation area) will need to be submitted. Retained trees will need to be protected during construction so a tree protection plan will be required, to accompany a brief Arb Method Statement to deal with the cycle store, railings and any other ground works within RPAs.

It would be preferable to get a response to the cycle store queries prior to a decision. However if you are minded to approve the application on current information conditions will be required.

(Officer note: The applicant has provided the following response which has been confirmed as an acceptable approach: *The cycle store structure will only be lightweight as it appears to be a covered arbour type structure. Whether there are post holes dug, or pads used to support proprietary feet, the impact will be minimal providing they are installed sympathetically. No-dig surfacing is commonplace and there are a variety of methods available. Alternatively a prefabricated shed structure may be used, these can simply be located upon paving slabs to minimise excavation.*

*Whichever option chosen the method statement will reflect this and include procedures for looking after roots accordingly.*

*The pruning is a valid point and cannot be taken for granted, and once the structure has been finalised, the pruning can be specified and incorporated within the submission to meet the condition below).*

**Ecology:**

The application site comprises a detached dwelling where it is proposed to convert the property into 5 flats, involving the demolition and replacement of a single-storey rear extension. Considering the extent of the works and structures to be affected, it is unlikely that bats or other protected species will be adversely affected by the proposals. As such, there are no objections to this application on ecological grounds.

**RBC Transport Strategy:**

This application proposal is for construction of a larger single-storey rear extension, internal modifications and refurbishment to facilitate change of use from a single dwelling house with detached garage (C3a) to 5no. self-contained flats.

The site is located in Zone 2, Primary Core Area, of the Revised Parking Standards and Design SPD. This zone directly surrounds the Central Core Area and extends to walking distances of 2 kilometres from the centre of Reading. A frequent service of public transport is available along Christchurch Road which provides a good frequency of services to and from the town centre. The site is within cycling distance of Reading town centre, and walking distance of local services.

In accordance with the adopted Parking Standards and Design SPD, the development would be required to provide a parking provision of 1 space per 1-2 bedroom flat equating to 5 parking spaces.

The plans illustrate that the development would utilise the existing access from Redlands Road and parking for 3 vehicles can be accommodated on-site which is below the Council's adopted parking standards.

The development site is located in an area designated as a Residents Parking Permit Area; Zone 15R and the property is not included within the scheme as it has on-site parking. The applicant has stated that;

“It is requested that the additional 2no. car spaces be provided by entitlement to 2no. on-street car parking permits allocated to the remaining two flats not served by onsite car parking.”

Under the Borough's current parking standards, this proposal would generate additional pressure for parking in the area which is not acceptable. Therefore, there should be an assumption that any future occupants of the proposed flats will not be issued with resident parking or visitor permits which would be covered by condition and an informative applied. This will ensure that the development does not harm the existing amenities of the neighbouring residential properties by adding to the already high level of on street car parking in the area.

In accordance with the Borough's Parking Standards and Design SPD, a minimum provision of 0.5 cycle storage spaces should be provided per unit. Cycle storage

has been proposed and at the front of the property within a covered store and equipped with Sheffield type stands which is acceptable.

Bin storage should comply with Manual for Streets and British Standard 5906: 2005 for Waste Management in Buildings to avoid the stationing of service vehicles on the carriageway for excessive periods. The bin store is conveniently located at the front of the site which will provide easy access for refuse collection.

There are no transport objections subject to recommended conditions being used.

#### Environmental Health

No objections subject to the garage door windows to the studio flat 5 (in the converted garage) being capable of being opened to provide ventilation there are no objections to the proposal. The developer should be advised that they would need to meet Building Regs. for thermal insulation and means of escape for all of the new flats.

#### CAAC

*"This property is located within the Redlands Conservation Area (CA) but the documentation provided with the application does not include a heritage statement or deal adequately with the impact of this change on the CA. We object to this application for the following reasons:*

##### *1. HERITAGE*

*1.1 A heritage statement should be provided dealing with heritage matters in detail.*

*1.2 Maps of the area indicate that house was built at the end of the nineteenth century and the garage was added at a later date probably added in the 1920s.*

*1.3 The CA appraisal mentions that one of the negative features of the area is 'loss of original brick walls and/or railings e.g. replacement of railings with brick walls and/or timber fencing'. This is what has happened in relation to this property. The opportunity should be taken in any refurbishment of the property to replace fencing with railings and/or hedges.*

##### *2. EXTERNAL FEATURES*

*2.1 The design and access statement (para 8) suggests that windows facing the garage will be filled in. This will affect the appearance of the property visible from the street and is not appropriate in the CA.*

*2.2 The proposed conversion of the garage is problematic in relation to the impact it will have on the character and appearance of the conservation area.*

*2.3 All materials and external features replaced should not only 'match existing' but be specified by condition to be appropriate to the age and setting of the property. This may mean the upgrading of some existing features in order to enhance the character of the conservation area.*

##### *3. PARKING AND TRAFFIC*

*3.1 The property is situated on a three-way corner plot with Elmhurst Road a few yards away from a busy junction, which makes the property difficult and potentially dangerous for vehicles parking on the drive to go in and out.*

*3.2 There are traffic lights immediately in front of the property.*

*3.3 Although it is not in use, there is a bus stop on Elmhurst Road at the side of the property.*

*3.4 The proposal is to park three cars on the site and for two parking permits to be allocated. In practice there is likely to be more than car per flat. The fact that*

residents have guests also puts more parking needs on the local streets. How visitor parking is to be accommodated needs to be addressed.

3.5 Because of the location of the property at this dangerous road intersection it is suggested that consideration is given to moving the vehicular access to the property to Elmhurst Road.

#### 4. AMENITY FOR OCCUPIERS

4.1 This substantial home is situated on what is today a very noisy street corner. A creative solution should be found to protect all occupiers from external noise from passing traffic and pedestrians on this busy route to and from the University.

4.2 The planned five unit conversion and extension of the house and garage is an overdevelopment on this cramped site. The total area proposed is 257.3 sqm and although the dimensions of the flats and rooms within are not shown in the plans this equates to 21.4 sqm per person for 12 people. The area of existing house is 189.5 sqm, which if occupied by a family of 6 would have been 31.6 sqm per Person.

4.3 The occupants of the proposed converted garage would bear the brunt of noise from vehicles coming and going and parking in front of their bedroom windows. Neither does this unit have any screening from noise in the form of a fence or hedges.

4.4 The provision for bins on the front drive adds to the crowding and cramped space for parking and manoeuvring of vehicles (see below).

4.5 A landscaping plan should be required by condition to enhance the grounds of the property and protect it from traffic noise and pollution.

#### 5. IMPACT ON THE NEIGHBOURHOOD

5.1 Because of its proximity to the University, the property is within the area covered by an article 4 direction which requires planning permission for all HMO conversions.

5.2 The conversion proposed has six bedrooms which could result in up to 12 adults living in a large family house. Whilst this is a flat conversion and not an HMO the principle of over intensification of use and the detrimental impact that this could have on the mix of properties in the neighbourhood is the same.

#### 6. CONCLUSION

6.1 Whilst understanding that this property might be problematic as a single family dwelling because of the current location, the solution proposed is not the right one. It fails to preserve or enhance the conservation area in which it is situated.

6.2 The attempt to squeeze 5 units into the available space cannot be supported because of the detrimental impact it will have on the neighbourhood in terms of parking and density of occupation. The area is covered by an Article 4 in relation to HMOs because of its proximity to the University.

6.3 The conversion of the garage, which requires the blocking up of windows on that side of the house and by virtue of its location at the vehicular and pedestrian entrance to the plot, is a step too far.”

#### Neighbour Notification:

Nos. 2 & 4 Marlborough Avenue; 84 Elmhurst Road; 1 Shinfield Road & 72 Redlands Road were consulted and a site notice displayed.

There have been 16 objections to the original proposal and 13 further objections following consultation on the amended scheme. The main areas of concern are:

- Impact on the character and appearance of the conservation area through the change of use and the use of the garage for residential accommodation.
- Parking problems in the area - this scheme will make matters worse.
- Impact on the traffic using the busy junction and the hazard of turning into and out of the site.
- Loss of family dwelling to flats.

## 5. RELEVANT POLICY AND GUIDANCE

- 5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that proposals be determined in accordance with the development plan unless material considerations indicate otherwise. Material considerations include relevant policies in the National Planning Policy Framework (NPPF) published in March 2012 - among them the 'presumption in favour of sustainable development'. However the NPPF (and the draft NPPF 2018) does not change the statutory status of the development plan as the starting point for decision making.
- 5.2 Section 72 (1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires the local planning authority in the exercise of its functions to pay special attention to the desirability of preserving or enhancing the character or appearance of a conservation area.
- 5.3 Accordingly, the National Planning Policy Framework and the following development plan policies and supplementary planning guidance are relevant:

### Reading Borough LDF Core Strategy 2008 (Altered 2015)

- CS1 Sustainable Construction and Design
- CS2 Waste Minimisation
- CS4 Accessibility and the intensity of development
- CS5 Inclusive Access
- CS7 Design and the Public Realm
- CS18 Residential Conversions
- CS20 Implementation of the Reading Transport Strategy
- CS24 Car/Cycle Parking
- CS33 Protection and Enhancement of the Historic Environment
- CS34 Pollution and Water Resources
- CS36 Biodiversity and Geology
- CS38 Trees, Hedges and Woodland

### Sites and Detailed Policies Document 2012 (Altered 2015)

- SD1 Presumption In Favour Of Sustainable Development
- DM1 Adaptation to Climate Change
- DM4 Safeguarding Amenity
- DM8 Residential Conversions
- DM10 Private and Communal Outdoor Space
- DM12 Access, Traffic and Highway-Related Matters
- DM18 Tree Planting
- DM19 Air Quality

### Supplementary Planning Guidance

- Revised Parking Standards and Design SPD (2011)
- Revised SPD Planning Obligations under Section 106 (2015)
- Residential Conversions (2013)

## 6. APPRAISAL

### 6.1 The main issues to be considered are:

- a) Principle of conversion
- b) Impact of physical changes on conservation area
- c) Parking and transport issues
- d) Impact on amenities of adjoining occupiers and future occupiers
- e) Impact on trees and landscaping
- f) Future management of the site
- g) Affordable housing and CIL

### a) Principle of conversion

6.2 Policies CS18 & DM8 seek to manage the conversion of houses to flats or HMO use in order to protect the existing housing stock as well as the amenity and character of the surrounding area, particularly in terms of intensification of activity. For future residents they also, with the adopted SPD, seek to ensure that there is adequate privacy, external amenity space, on-site car/cycle parking and bin storage areas.

6.3 The starting point is to check that the original house meets the basic policy size threshold to be considered for conversion. The SPD states that “The property to be converted to a flat or large HMO should have four or more bedrooms or measure more than 120 square metres gross. When calculating the floor area of the property the measurement should be based on the external dimensions as at 1st July 1948 or when built (whichever is the later)”. The existing house at 25 Redlands Road meets the minimum size criteria. The other criteria relate to residential amenity of new occupiers and neighbours, impacts on parking and traffic, impacts on landscaping and future management of amenities. As the property is in a conservation area the merits of the site and whether the proposed alterations would harm that character and appearance of the conservation area also needs to be considered.

### b) Impact on character of the conservation area

6.4 The house is an attractive feature on the junction and contributes positively to the character and appearance of the Redlands Conservation Area. Recent legal cases have established that within the terms of the Planning (Listed Buildings and Conservation Areas) Act 1990 the general power to grant planning permission is expressly subject to Section 72(1), which provides that the local authority has a statutory duty: ‘with respect of any building or other land in a conservation area... special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area’. In the case of developments in conservation areas ‘preserving’ is taken to mean ‘doing no harm’.

6.5 The proposed conversion would require only modest changes to the external appearance of the house by replacing an existing single storey rear extension with a new, slightly larger one, enclosing an open sided area at the rear and making alterations to the garage to make it acceptable for residential use but amended to keep the existing traditional style garage doors to retain its existing appearance when seen from the street.

6.6 Many objectors are particularly aggrieved by the principle of converting the garage to habitable use in this conservation area. However, dwellings in conservation areas benefit from having mostly the same permitted development rights as dwellings in other parts of the Borough including being able to convert

outbuildings or garages to habitable use. In this case the applicant wants to make the garage an independent unit and, bearing the above extract from the Act on development in a conservation areas in mind, officers are satisfied that the proposed conversion of the garage or the main house would not detract from the appearance of the existing building or harm the character or appearance of the conservation area. The proposed conversion is therefore in accordance with policies CS7 and CS33.

c) Parking and transport issues

6.7 The comments from transport officers are provided above. In essence the usual concerns that inadequate on-site parking facilities will lead to on street parking are addressed by the parking permit scheme in force in the area. Overspill parking will be strongly regulated against and the recommended conditions will ensure that occupiers of the flats are made aware of this when considering purchasing a flat.

6.8 Objectors have raised concerns about lack of parking and the problems that might occur when traffic queuing at the traffic lights block to access. However, in full use this family home could easily have been served by 3 cars so it is not reasonable to claim that the proposed development would make access to the site significantly worse than it is now. Transport officers have confirmed that there is no change to the existing access arrangements and its proximity to the junction remains the same. There is already a large area of hardstanding which could accommodate 3 vehicles, manoeuvring in and out of the access. To ensure that vehicles can enter and leave the site in forward gear, a small enlargement to the driveway is proposed. In view of this, the parking layout is deemed acceptable.

6.9 The applicant had originally stated that 'It is requested that the additional 2no. car spaces be provided by entitlement to 2no. on-street car parking permits allocated to the remaining two flats not served by onsite car parking.'

6.10 Transport colleagues have clarified that there should be an assumption that any future occupants of the proposed flats will not be issued with resident parking or visitor permits. The applicant has responded in the amended DAS to confirm 'It is proposed for 3no. car parking spaces to be provided on site at the front of the property accommodated by a small enlargement to the driveway. Given the sustainable location of the site with good access to amenities, employment opportunities, public transport and secure and sheltered cycle storage, we suggest that the shortfall of 2no. car spaces from the council's standards should present no major issues for potential residents of these 2no. one-bedroom flats who would be informed that there would be no entitlement for car parking'.

6.11 Officers are therefore satisfied that the proposed level of car and cycle parking is acceptable and that the proposed conversion is unlikely to have a significant impact on the functioning of the adjacent traffic junction in accordance with policies CS24 and DM12. Conditions are recommended and a construction method statement will be required to demonstrate how traffic associated with the construction stage will be managed to minimise nuisance caused to users of the road and residents close by.

d) Impact on amenities of adjoining occupiers and future occupiers

6.12 Policy DM4 (Safeguarding Amenity) states that development should not cause a significant detrimental impact to the living environment of existing or new residential properties in terms of privacy and overlooking, access to sunlight and



daylight, visual dominance and overbearing, noise and disturbance, artificial lighting, crime and safety etc. The single storey rear extension has been amended to reduce the size and officers are satisfied that the amenities of neighbours will not be harmed by these works. The internal room sizes are adequate and the indicated stacking of rooms above rooms also is acceptable and there is good access to natural light for all occupiers.

- 6.13 At least one of the units (flat 2) is suitable for family occupation with two bedrooms. It is located on the ground floor with access to an area of private outdoor space.
- 6.14 It is accepted that the property will be occupied by more people than previously but as this is a large 5 bedroom house it is possible that at least 6 people could have easily lived here. There is no evidence to suggest that occupiers of 5 self-contained flats would be any noisier than a large family would be.
- 6.15 The neighbours at 2 Marlborough Avenue have raised a concern about the converted garage on their shared boundary and whether the structure is capable of being converted. They also have concerns about noise and disturbance arising from the residential use of the garage. Officers can advise that the structural soundness of the garage is a matter for the developer to be satisfied about. Regarding concerns about noise and disturbance these need to be seen in the context that as there are no planning restrictions on the residential use of the garage or activities in the garden were the property to remain as a single family home it is unlikely that the proposed conversion would lead to more disturbance for neighbours.
- 6.16 The conclusion reached is that the proposed conversion is unlikely to harm the residential amenity of neighbours and overall policy DM4 is complied with.



- e) Affordable Housing & CIL
- 6.17 An acceptable level of contribution has been offered which meets the criteria set out in the Affordable Housing SPD in accordance with Policy DM6 (Affordable

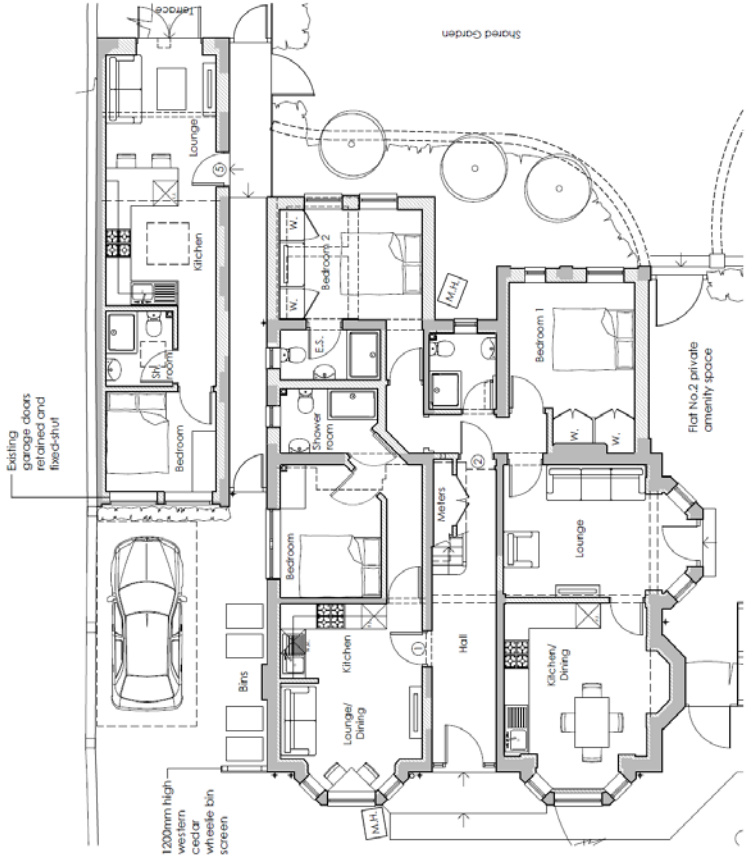
Housing) of the Reading Borough Council Sites and Detailed Policies Document 2012 (Altered 2015). A CIL payment is also required for the small amount of additional floorspace proposed to enable the residential conversion to proceed.

## 7. CONCLUSION

- 7.1 Notwithstanding the objections received from neighbours the application has been assessed for compliance with adopted planning policies and guidance and has been found to be acceptable in all respects. The proposed conversion and minor physical alterations will not harm the appearance of the building nor the contribution that it makes to the character and appearance of the conservation area. The parking provision is accepted as workable given the parking restrictions in place and the impact on the functioning of the junction unlikely to be worse than were the house in full occupation by a family.
- 7.2 The recommendation is to grant planning permission subject to a S106 agreement being completed and conditions applied.

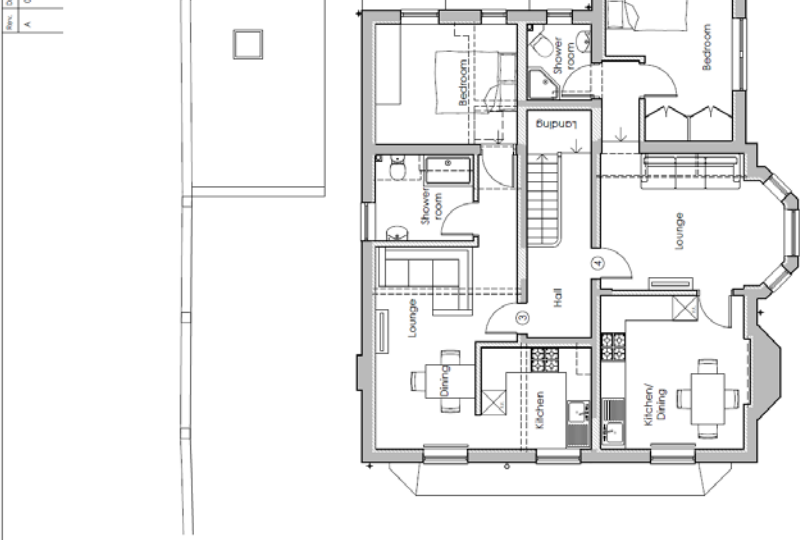
Case Officer: Julie Williams

Rev.	Date	Notes	Author	Checked
A	04/03/18	Revised to address BEC comments	AS	CTW



PROPOSED GROUND FLOOR PLAN

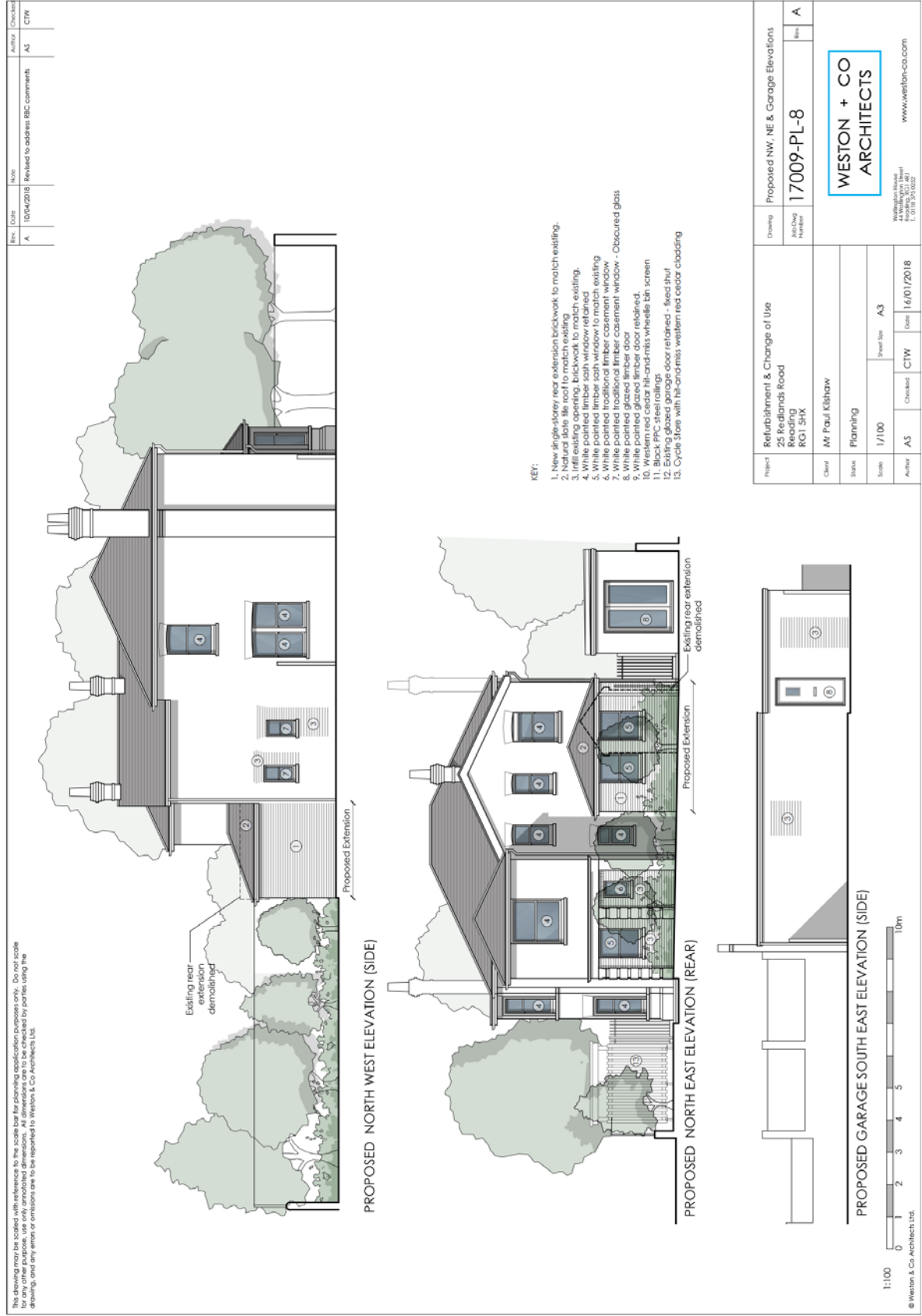
KEY  
 - - - - - Proposed walls / partitions for demolition  
 = = = = = Existing walls / partitions retained  
 = Proposed new walls / partitions



PROPOSED FIRST FLOOR PLAN

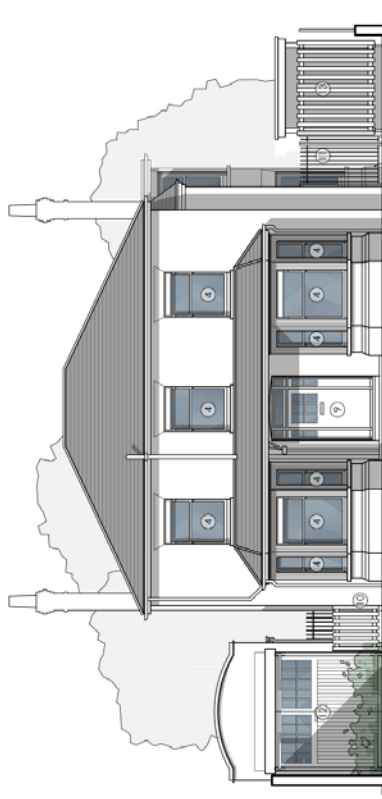
Project	Returbishment & Change of Use 25 Redlands Road Reading RG1 5HX	Drawing Jobbing Number	Proposed Floor Plans 17009-PL-6	Rev.	A
Client	Mr Paul Kilshaw	Status	Planning	Scale	1/100
Author	AS	Checked	CTW	Date	16/01/2018
		Sheet Size	A3		
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Proposed floor plans

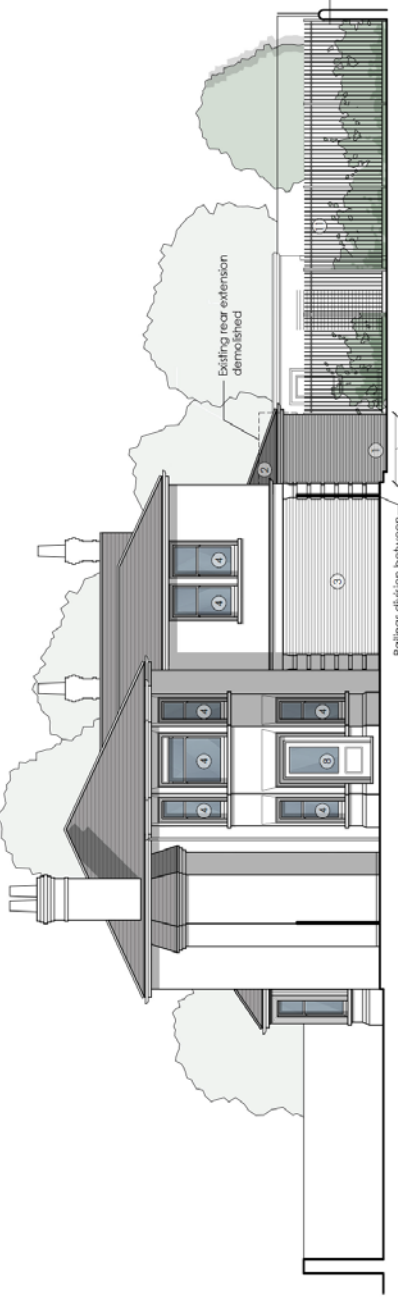


Proposed side and rear elevations

Rev: A	Date: 10/01/2018	Note: Revised to address RBC comments	Author: AS	Checked: CTW
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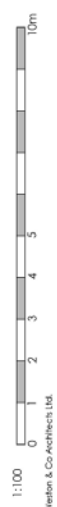
PROPOSED SOUTH WEST ELEVATION(FRONT)  
 Note: building timbers underlined



PROPOSED SOUTH EAST ELEVATION (SIDE)

KEY:

1. New single-storey rear extension brickwork to match existing.
2. Half red slate tile roof to match existing.
3. White painted garage door to match existing.
4. White painted timber casement window to match existing.
5. White painted timber casement window to match existing.
6. White painted traditional timber casement window - Obscured glass.
7. White painted traditional timber casement window - Obscured glass.
8. White painted glazed timber door.
9. White painted glazed timber door.
10. White painted glazed timber door.
11. Black PPRC steel railings.
12. Existing glazed garage door retained - fixed shut.
13. Cycle store with fill-and-miss western red cedar cladding.



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Project	Refurbishment & Change of Use 25 Reclanads Road Reading RG1 3HX	Drawing	Proposed SW & SE Elevations
Client	Mr Paul Kilshaw	Job/Draw Number	17009-PL-7
Status	Planning	Rev	A
Scale	1/100	Sheet Size	A3
Author	AS	Checked	CTW
Date	16/01/2018	www.weston-co.com	



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Proposed front and rear elevations



# SOUTHCOTE





## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT & NEIGHBOURHOOD SERVICES  
READING BOROUGH COUNCIL  
PLANNING APPLICATIONS COMMITTEE: 27 June 2018

ITEM NO. 16

Ward: Southcote  
App No.: 180704/FUL  
Address: 1 Kenilworth Avenue, Reading, RG30 3DL  
Proposal: Erection of 1no. four bedroom detached dwelling  
Applicant: Ms Lorna Tee.  
Date validated: 1 May 2018  
8 week target decision date: 26 June 2018

### RECOMMENDATION

Refuse Full Planning Permission for the following reasons:

1. The proposed dwelling, by reason of its detached character, scale, and unsympathetic siting and orientation in relation to neighbouring dwellings, would be distinctly at odds with the established design character of the street with a discordant effect within its setting. This effect would be particularly pronounced due to the prominence of the building within the streetscene and its intrusion into a spacious visual gap between existing buildings.

For these reasons, the proposed dwelling would appear incongruous and out of keeping with its setting, with consequent harm to the character and appearance of the area. On this basis the proposal is contrary to Policy CS7 (Design and the Public Realm) of the Reading Borough LDF Core Strategy 2008 and Policy DM11 (Development of Private Residential Gardens) of the Reading Borough LDF Sites and Detailed Policies Document 2012.

2. In the absence of a completed legal agreement to secure an acceptable amount of Affordable Housing, the proposal fails to contribute adequately to the housing needs of Reading Borough and the need to provide sustainable and inclusive mixed and balanced communities. As such the proposal is contrary to Policy CS16 of the Reading Borough LDF Core Strategy 2008 (altered 2015) and Affordable Housing Supplementary Planning Document 2013.”

### INFORMATIVES TO INCLUDE

1. Standard positive and proactive informative.
2. Refused drawings

## 1. INTRODUCTION

1.1 The site comprises the whole curtilage of 1 Kenilworth Avenue, a semi-detached house fronting Kenilworth Avenue at the junction with Southcote Lane. The pair of houses (1 Kenilworth Ave. and 15 Southcote Lane) is angled away from other houses in the street to address the street corner, in common with the pair of houses to the other side of the junction to the south west.



Site Location Plan



Site Photograph

## 2. PROPOSAL

- 2.1 Full Planning Permission is sought for a new two storey detached dwelling adjacent to the existing house. An additional vehicle access from Kenilworth Avenue and parking area in the existing front garden is proposed.

### *Submitted drawings*

18.06-100, dated 6 March 2018

18.06-101, dated 6 March 2018

18.06-102, dated 6 March 2018  
18.06-103 Rev.A, dated 8 March 2018  
18.06-104 Rev.A, dated 8 March 2018  
18.06-105, dated 6 March 2018  
18.06-106, dated 6 March 2018  
18.06-107, dated 6 March 2018  
18.06-108, dated 6 March 2018

*Supporting Documents*

Planning, Design and Access Statement  
CIL Additional Information Form

3. PLANNING HISTORY

3.1 None

4. CONSULTATIONS

4.1 Statutory:

Thames Water (TW)

An agreement with TW is required to build within three metres of a public sewer which TW maintain.

TW will be checking our records to see whether the site owner has submitted an application form. If there is no record of an agreement in place, TW will send the applicant or agent an application form for an approved build over agreement.

4.2 Non-statutory:

- RBC Natural Environment (NE)

The site has 3 conifers on the frontage and a small Cherry tree on the grass verge in front of the site - only the latter is shown on plans - see attached photo.

The proposal appears to result in the loss of all three conifers, with associated loss of amenity value to the street, and includes a new vehicular access very close to the Cherry. With regards to the Cherry, it's RPA is likely to be limited given its small size but as the canopy slightly overhangs the driveway already and will only get wider, this could be an issue in the future in terms of ensuring driveway access under the canopy - moving the access away from the Cherry should be considered. Also, whilst this tree is shown to be retained, will this be acceptable in terms of visibility splays?

Street tree planting along Kenilworth Avenue is part of the character of the street, presumably undertaken by Southcote Estate/Kenilworth Avenue Trust. NE assume they have to agree the vehicular access (before a planning decision). An additional street tree on the frontage would be beneficial but it is not possible for us to secure this. However, within the site, plans indicatively show hedging - replacement tree planting should be incorporated on the frontage.

Tree matters should be resolved prior to a decision.

- RBC Transport (Highways Authority)

The development site is located on a private road and is located in Zone 2, Primary Core Area, of the Revised Parking Standards and Design SPD. This zone directly surrounds the Central Core Area and extends to walking distances of 2 kilometres from the centre of Reading.

In accordance with the adopted Parking Standards and Design SPD, the development would be required to provide parking provision of 2 spaces for the proposed dwelling as well as retaining the existing provision for No 1 Kenilworth Avenue. The submitted proposed plan, Drawing No 18.06-103 A, illustrates that the garage associated with No 1 will be lost due to the development of the proposed dwelling; however each dwelling will be provided with 2 off road parking spaces; dimensions for each parking space should be a minimum of 2.4m x 4.8m Proposed provision is in accordance with the Council's current standards and is therefore deemed acceptable.

The new dwelling will be served by a new access from Kenilworth Avenue whilst the existing property would continue to use the original access point. Visibility splays of 2.4m x 43m should be illustrated on revised plans for the proposed access, as well as the location of lamp columns that are situated adjacent to the site.

It would appear that there are a number of trees on/adjacent to the site; the Council's Natural Environment Officer should be contacted to assess any risk of root damage etc.

In accordance with the adopted Parking SPD, the new development is required to provide a minimum of 2 cycle parking spaces for a 4 bedroom C3 dwelling which should be in a conveniently located, lockable, covered store. Plans submitted illustrate storage in the rear garage which is deemed acceptable.

Bin storage area is indicated in Drawing No 18.06-103 A. This should be located no further than 15m from the access point of the site to avoid the stationing of service vehicles on the carriageway for excessive periods. It is assumed that curb side refuse and recycling collection will be the same as other properties in the Avenue.

#### 4.3 Public consultation:

Letters were sent to addresses surrounding the site. 68 objections have been received, summarised as follows:

- The proposed development is not in keeping with the existing houses. This is a detached house, whereas all the other properties are semi detached.
- The design and scale is out of keeping with other dwellings in the street.
- The proposed dwelling is sited up to 2 metres to the front of the building line in Kenilworth Avenue harmful to streetscene and amenity of 3 Kenilworth Avenue.
- Doubling of car parking spaces close to busy Southcote Lane junction, raises safety concerns.
- Access is shown over land belonging to Southcote Park Estate Trust. There is no right of access.

- Overlooking from landing, bathroom and bedroom 3 windows to 3 Kenilworth Avenue and 15A Southcote Lane.
- Loss of light to 3 Kenilworth Avenue - bedroom 4, bathroom, landing and kitchen.
- Proposed garden is too narrow compared to neighbouring properties.
- Incursion onto land owned by 3 Kenilworth Avenue.
- Proposed street elevation drawing doesn't show existing dwelling clearly.
- Loss of visual amenity due to loss of street tree. Tree not owned by applicant would need to be removed to allow access and visibility splays.
- The matter should be referred to the statutory water undertaker for comments due to proximity to sewer.
- Southcote Park Estate Trustees object on the grounds that permission has not been sought or granted for access over Southcote Park Estate Land; Permission has not been sought or granted for the damage proposed to the Estate (e.g. removal of a tree), or compensation agreed; The legal implications regarding the covenants relating to the properties on the Estate have not been clarified or the costs agreed.
- There are stag beetles in the area.

## 5. RELEVANT PLANNING POLICY AND GUIDANCE

- 5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that proposals be determined in accordance with the development plan unless material considerations indicate otherwise. Material considerations include relevant policies in the National Planning Policy framework (NPPF) - among them the 'presumption in favour of sustainable development'.
- 5.2 The following local and national planning policy and guidance is relevant to this application:

### National Policy

National Planning Policy Framework  
National Planning Practice Guidance

### Reading Borough Local Development Framework Core Strategy (2008)

CS1 (Sustainable Design and Construction)  
CS2 (Waste Minimisation)  
CS4 (Accessibility and the Intensity of Development)  
CS5 (Inclusive Access)  
CS6 (Settlement Boundary)  
CS7 (Design and the Public Realm)  
CS9 (Infrastructure, Services, Resources and Amenities)  
CS15 (Location, Accessibility, Density and Housing Mix)  
CS20 (Implementation of The Reading Transport Strategy)  
CS24 (Car/Cycle Parking)  
CS34 (Pollution and Water Resources)  
CS36 (Biodiversity and Geology)  
CS38 (Trees, Hedges and Woodlands)

### Sites and Detailed Policies Document (2012)

SD1 (Presumption in Favour of Sustainable Development)  
DM1 (Adaptation to Climate Change)  
DM3 (Infrastructure Planning)  
DM4 (Safeguarding Amenity)

DM6 (Affordable Housing)  
DM10 (Private and Communal Outdoor Space)  
DM11 (Development of Private Residential Gardens)  
DM12 (Access, Traffic and Highway Related Matters)  
DM18 (Tree Planting)

SPG/SPD

- Section 106 Planning Obligations SPD 2013
- Affordable Housing SPD 2013
- Sustainable Design and Construction SPD 2013
- Revised Parking Standards and Design SPD 2011

6. APPRAISAL

Main Issues:

Design & Appearance  
Residential Amenity  
Affordable Housing  
Trees  
Drainage  
Land Ownership Matters

Design & Appearance

- 6.1 Kenilworth Avenue is a Private Road that is characterised by a distinctly uniform character comprising semi-detached houses of the same age and style within a regular planned layout with a clearly defined building line to the frontage, which is maintained in an arc following the bend in the road near to the junction with Southcote Lane. Where extensions and alterations to the houses have taken place these remain ancillary to the original and are not considered to have altered the fundamental character of the street.
- 6.2 It is considered that the proposed detached dwelling would conflict with this established character and fail to respond positively to its local context or reinforce local distinctiveness. The detached layout is inherently at odds with the character of the street as described above.
- 6.3 Furthermore, the orientation of number 1 Kenilworth Avenue/15 Southcote Lane appears as a deliberate arrangement designed to act as an end stop to the street and to address the corner at the entrance to the estate. This is mirrored by number 2 Kenilworth/17 Southcote Lane to the opposite side of the junction. The gap between numbers 1 and 3 Kenilworth Avenue provides a degree of spaciousness at the entrance to the estate and serves to accommodate the change in orientation without the layout appearing awkward or contrived. The proposed attempt to introduce a dwelling into this gap would remove this spaciousness and would result in a building which fails to relate positively to either the orientation of number 1, or the positioning of number 3. The new building would appear awkwardly juxtaposed with the flank of number 1 and would jar with the distinct and otherwise uniform curved building line defined by the façades of houses in Kenilworth Avenue.
- 6.4 It is considered that the proposals are contrary to Policies CS7 and DM11 on this basis.

### Residential Amenity

- 6.5 The proposed dwelling would be sited close to the flank wall of 3 Kenilworth Avenue. The side windows of the proposed dwelling are shown as being obscure glazed. It is considered that this would prevent harmful overlooking. The rear windows of the proposed house would be orientated looking down the garden and would not result in direct or harmful overlooking of the rear garden of number 3. The house would be separated from 15 Southcote Lane by the garden of the existing house at 1 Kenilworth Avenue. It is considered that this arrangement would not result in harmful overlooking to this neighbour.
- 6.6 Some loss of daylight would occur to the side (north west) facing windows of 3 Kenilworth Avenue. Those serving the bathroom and landing do not serve habitable rooms and therefore the effect on light is less harmful. Bedroom 4 would continue to receive sufficient daylight due to its south westerly orientation. The kitchen is also served by north east facing windows in the rear elevation. On this basis it is considered that the effect on daylight is not sufficiently harmful to warrant refusal. This does not affect any statutory Rights to Light that may exist outside of the Planning process.
- 6.7 The house is considered to be sufficiently distant from 15 Southcote Lane to avoid harmful loss of daylight. There would be some impact on the outlook from the rear of the existing house at 1 Kenilworth, however any harm caused is not considered sufficient in this instance to warrant being a reason for refusal.
- 6.8 The two houses would share the current plot and both would benefit from useable rear gardens. It is not considered that these are so small or poorly proportioned as to warrant being a reason for refusal given that garden sizes are not uniform in the street. Policy DM10 applies.

### Affordable Housing

- 6.9 For a development of the size proposed, Policy DM6 requires a contribution to enable the equivalent of 10% of the Gross Development Value of the development to be provided as Affordable Housing. This would be a financial contribution secured under s.106 to secure provision off-site elsewhere in the Borough of Reading. The Council's Affordable Housing SPD (adopted 2013) sets out the procedure for calculating the required contribution. The Appellant does not agree that the Council is justified in seeking a contribution and has therefore not provided valuation details to confirm the Gross Development Value of the proposal.
- 6.10 In considering the weight to be given to Policy DM6, relative to any other material considerations, it is relevant that a significant need for Affordable Housing exists within the Borough, as demonstrated by the up-to-date assessment contained within the 'Berkshire (including South Bucks) Strategic Housing Market Assessment' (February 2016). Additionally, a significant proportion of housing is provided on smaller sites within Reading due to the developed, urban nature. There is therefore a need for these smaller sites to contribute towards delivering Affordable Housing.
- 6.11 For these reasons it is considered that Policy DM6 remains relevant to this application and is not outweighed by other material considerations, including the changes in national policy guidance. Although the applicant has indicated a willingness to provide an appropriate contribution, this would need to be secured by S106 legal agreement and this has not been

progressed due to the other concerns with the application. In the circumstances the proposal therefore fails to contribute adequately to the housing needs of Reading Borough and the need to provide sustainable and inclusive mixed and balanced communities. As such the proposal is contrary to Policy DM6 of the Sites and Detailed Policies Document 2012, Affordable Housing Supplementary Planning Document 2013, and Paragraph 50 of the NPPF. The application is recommended for refusal on this basis.

#### Trees

- 6.12 The proposals would involve the loss of trees which currently contribute positively to the visual amenity of the street. It is considered that sufficient space would exist within the forecourt areas of the houses to provide suitable replacements in mitigation.
- 6.13 The Cherry within the highway verge is not under the control of the Applicant. It would require the permission of the owner to be removed. It would appear that space exists for a replacement elsewhere within the verge if necessary.

#### Drainage

- 6.14 The proposed building passes close to a main sewer owned by Thames Water. Their permission is required to build in this location. This is a matter to be resolved between the interested parties.

#### Land Ownership Matters

- 6.15 The site is located within the Southcote Park Estate. The separate permission of the Trustees will be required to access the land, and other restrictions may exist. This is a private matter to be resolved between the interested parties and would not affect Planning Permission being granted, although it may ultimately prevent implementation of the development.

## 7. CONCLUSION

- 7.1 Although the proposal would not be sufficiently harmful to neighbouring amenity to warrant refusal, it is considered that the proposal would result in unacceptable harm to the character of the area. A suitable contribution towards Affordable Housing has also not been secured.

Case Officer: Steve Vigar

Drawings (selection only) - Full details at:  
[http://planning.reading.gov.uk/fastweb\\_PL/welcome.asp](http://planning.reading.gov.uk/fastweb_PL/welcome.asp)





Proposed Site Layout



Proposed Street Elevation

# THAMES



## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT CULTURE & SPORT  
READING BOROUGH COUNCIL  
PLANNING APPLICATIONS COMMITTEE: 27 June 2018

ITEM NO. 17

Ward: Thames

Application No.: 180556

Address: Caversham Primary School, Hemdean Road, Caversham, RG4 7RA

Proposal: Demolition of existing single storey extension to school building and replacement with new single storey extension.

Applicant: Education and Skills Funding Agency

Date received: 4 April 2018

Application: 8 week target decision date: 30/05/2018

### RECOMMENDATION

Grant planning permission subject to conditions:

Conditions to include:

1. Time Limit
2. Approved Plans.
3. In accordance with submitted hard and Soft landscaping details.
4. In accordance with submitted arboricultural method statement
5. In accordance with submitted construction method statement
6. In accordance with submitted ecology survey report
7. Noise and dust control measures to be submitted and agreed prior to commencement of development
8. Reinstatement of Victoria Road boundary treatment

Informatives:

1. Positive and Proactive
2. Terms and Conditions
3. Permission and Licenses to use Victoria Road
4. Building Regulations

## 1. INTRODUCTION

- 1.1 The site is located approximately 600 metres to the north of Caversham District Centre and is occupied by a Council-owned primary school providing for two forms of entry. Vehicular access is at the south east corner of the site from Hemdean Road which is a bus route between central Caversham and Emmer Green. The main school buildings are located towards the south eastern corner of the site and comprise a main block arranged around a central entrance with a number of detached single storey modular buildings to the rear and to the west of the main building. A small area of grassed play space exists to the north west corner of the site between the existing modular buildings and Victoria Road, with

a larger hard paved play ground to the north of the main school building. A small playing field exists to the south of the modular buildings at the south west corner of the site.

- 1.2 The school site is bounded by Victoria Road to the north; this is a Council-owned but un-adopted road leading to Caversham Cemetery. Council-owned allotments abut the site to the west and the rear boundaries of houses in Queen Street and Hemdean Road adjoin to the south and east.



Site Location Plan

## 2. PROPOSALS

- 2.1 Permission is sought for demolition of an existing single storey dining hall extension to the south west part of the schools building and replacement with new single storey dining hall extension in the same location.

- 2.2 At 133m2 the proposed extension would be slightly smaller than the existing extension building to be replaced (141m2).
- 2.3 The extension would be flat roof (3.5m in height) with a white render exterior with a band of dark Eternit panelling with full height windows.
- 2.4 Supporting information submitted with the application:

EFA-PPA-03-00-DR-A-20001 Site Location Plan  
 EFA-PPA-03-00-DR-A-20010 Site Plan Existing  
 EFA-PPA-03-00-DR-A-20100 Site Plan Proposed  
 EFA-PPA-03-00-DR-A-20150 Phasing Plans  
 EFA-PPA-03-ZZ-DR-A-20200 Existing Floor and Roof Plans  
 EFA-PPA-03-ZZ-DR-A-20210 Proposed First Floor and Roof Plans  
 EFA-PPA-03-XX-DR-A-20300 Proposed Site Sections AA & BB  
 EFA-PPA-03-XX-DR-A-20301 Proposed Site SECTIONS CC & DD  
 EFA-PPA-03-XX-DR-A-20400 Existing South & West Elevations  
 EFA-PPA-03-XX-DR-A-20401 Existing North & East Elevations  
 EFA-PPA-03-XX-DR-A-20410 Proposed South & West Elevations  
 EFA-PPA-03-XX-DR-A-20411 Proposed North & East Elevations  
 EFA-PPA-03-XX-VS-A-20500 Existing View from South  
 EFA-PPA-03-XX-VS-A-20501 Existing View from West  
 EFA-PPA-03-XX-VS-A-20510 Proposed View from South  
 EFA-PPA-03-XX-VS-A-20511 Proposed View from South  
 EFA-FHA-03-00-DR-L101 Landscape General Arrangement Plan  
 EFA-FHA-03-00-DR-D201 Typical Paving and Edging Details  
 EFA-FHA-03-00-DR-D410 Typical Soft Landscape Details

Design and Access Statement  
 Planning Statement  
 Arboricultural Impact Assessment and Arboricultural Method Statement  
 Preliminary Ecological Appraisal  
 Building Service Design Report  
 Construction Method Statement  
 Flood Risk Assessment

### 3. RELEVANT PLANNING HISTORY

- 3.1 96/00792/REG3 - Removal of 4 existing classrooms and replacement with 4 modular classrooms with covered link. Reg 3 - approved.
- 3.2 99/00030/REG3 - Installation of single storey modular classroom unit including corridor link to existing building - approved
- 3.3 03/00463/REG3 - Installation of a temporary classroom - approved.
- 3.4 09/00432/REG3 - Demolition of a small staff toilet extension and the re-construction of a larger toilet extension to accommodate new toilets for use by foundation stage children, and replacement of staff toilets - Granted

- 3.5 09/01159/APPCON - Discharge of condition 6 of planning consent 09/00432/REG3 - Granted
- 3.6 13/00229/REG3 - Provision of one demountable modular double classroom and associated external works - Granted
- 3.7 13/00701/APPCON - Discharge of conditions 3 and 4 of planning permission 130090/REG3 - Granted.

#### 4. CONSULTATIONS

##### *Public Consultation*

- 4.1 Neighbour notification letters were sent to no.s 154-178 Hemdean Road and 1-27 Queen Street and a site notice was displayed at the application site. No letters of representation have been received.

##### **RBC Transport Development Control**

- 4.2 No objection, subject to a condition requiring works to be undertaken in accordance with the submitted construction method statement

##### **RBC Ecology Consultant**

- 4.3 No objection, subject to a condition requiring works to be undertaken in accordance with the measures set out in the submitted ecological report

##### **RBC Environmental Protection**

- 4.4 No objection, subject to a condition to require submission and approval by the Local Planning Authority prior to the commencement of works on site of a scheme to control noise and dust during construction.

##### **RBC Natural Environment Trees**

No objection, subject to a condition to requiring works to be undertaken in accordance with the submitted arboricultural method statement

#### 5. RELEVANT PLANNING POLICY AND GUIDANCE

##### *Legal and Planning Policy Context*

- 5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that proposals be determined in accordance with the development plan unless material considerations indicate otherwise. Material considerations include relevant policies in the National Planning Policy framework (NPPF) - among them the 'presumption in favour of sustainable development'.

- 5.2 The following local and national planning policy and guidance is relevant to this application:



National:

National Planning Policy Framework

Reading Borough Core Strategy (January 2008):

- CS1 (Sustainability)
- CS4 (Accessibility and the Intensity of Development)
- CS5 (Inclusive Access)
- CS7 (Design and the Public Realm)
- CS22 (Transport Assessments)
- CS24 (Car / Cycle Parking)
- CS31 (Additional and Existing Community Facilities)
- CS34 (Pollution and Water Resources)
- CS36 (Biodiversity and Geology)
- CS38 (Trees, Hedges and Woodlands)

Reading Borough Sites and Detailed Policies Document (2012):

- DM4 (Safeguarding Amenity)
- DM12 (Access, Traffic and Highway-Related Matters)

Supplementary Planning Guidance

Parking Standards and Design (2011)

6. APPRAISAL

i) Principle

6.1 Policy CS31 states that “*Proposals for new, extended or improved community facilities will be acceptable, particularly where this will involve co-location of facilities on a single site.*” The proposal is located within an existing school site and seeks to provide a replacement dining hall. The footprint of the extension would be smaller than that of the existing to be replaced and the proposal does not facilitate an increase in pupil or staff numbers. The provision of improved facilities is considered to be in accordance with policy CS31 of the Reading Borough Core Strategy.

ii) Visual amenity and the public realm

6.2 The replacement single storey extension would be sited in the same location as existing but would be smaller in footprint. The modern replacement extension is considered an enhancement to the school site above that of the existing utilitarian dining hall building which is in a poor state of repair. The site is set back from the road and is not in a prominent location when viewed from public areas outside the school grounds. The proposal is considered to be in accordance with Policy CS7.

iii) Impact on Neighbouring Amenity

6.3 As a replacement extension in the same location and of similar size to that existing there would be no additional adverse impact upon surrounding properties in terms of privacy, loss of light or overbearing. The proposal does not seek to increase pupil or staff numbers such that the extension is not considered

to result in any increase noise or disturbance to surrounding occupiers. The proposal is considered to accord with Policies DM4 and CS34.

- 6.4 Environmental Protection Officers have recommended a scheme to control noise and dust during construction is provided. This can be secured by way of a suitably worded pre-commencement condition.

iv) Transport

- 6.5 The proposed would not result in any increase of pupils or teaching staff. Therefore transport officers have confirmed that the proposal would not generate any additional demand for parking.

- 6.6 A construction method statement has been provided as part of the application which transport officers have confirmed is acceptable. The construction methodology proposed would involve formation of a temporary access point from Victoria Road to the north by temporarily removing the existing metal rail fence and bollards. Victoria Road is not a public highway but is privately owned by RBC. An informative will be added to any decision notice to advise the applicant to secure the appropriate licenses and permissions from RBC Asset Management prior to starting works.

- 6.7 A suitably worded condition is also proposed to require the reinstatement of the Victoria Road boundary prior to first occupation of the extension.

- 6.8 There are no transport objections to the proposal which is considered to accord with policies CS24 and DM12.

v) Trees

- 6.9 The proposed replacement building is located close to one medium sized tree. An arboricultural method statement has been submitted as part of the application and the RBC Natural Environment Trees Officer is satisfied that this proposed suitable protection measures for this tree during the course of the building works.

- 6.10 A second tree is proposed to be cut back by 3m in order to facilitate the temporary access route from Victoria Road. This tree is a semi mature specimen, not subject of a TPO and the Tree Officer has raised no objection to this work being undertaken.

- 6.11 The proposal is considered acceptable in terms of impact on trees subject to a condition to require works to be undertaken in accordance with the submitted arboricultural method statement. The proposal is considered to accord with Policy CS38.

vi) Ecology

- 6.12 The ecology survey report submitted with the application concludes that the extension to be demolished is unlikely to host roosting bats, but there is a small risk that bats may shelter between the asbestos sheeting. As such, it is

recommended in the report that the removal of the roofing material is carried out under the supervision of a suitability qualified ecologist.

6.13 The amenity grassland surrounding the extension is of limited value to reptiles, badgers and other protected species. To minimise the risk of adversely affecting these species, the report states that an ecologist will supervise the site clearance. To provide opportunities for wildlife post-development, the report also indicates that two bird and bat boxes will be installed on trees along the western site boundary.

6.14 The Council's Ecology Consultant has reviewed the proposals and considered that the precautionary working measures and biodiversity enhancements detailed in the submitted ecology survey report are sufficient to ensure that protected wildlife is not adversely affected by the development. The proposal is considered to accord with Policy CS36 subject to a condition to require the works to be undertaken with the measures set out in the submitted survey report.

iii) Sustainability

6.15 A design report has been submitted as part of the application which demonstrates that the building uses modern construction techniques and technologies in terms of energy efficiency. This is considered to accord with Policy CS1.

ix) Other Matters

*Equality Impact*

6.16 In determining this application the Committee is required to have regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief and sexual orientation. There is no indication or evidence that the protected groups have or will have different needs, experiences, issues and priorities in relation to this particular planning application.

6.17 The proposal would not be liable for the Community Infrastructure Levy.

7. CONCLUSION

7.1 There is no objection to the principle of a replacement dining hall extension and the proposal would not be harmful in visual terms or result in significant harm to the amenity of neighbouring properties. The application has demonstrated that it would not be detrimental in terms of transport, trees or ecological impacts and is considered to accord with local and national policies.

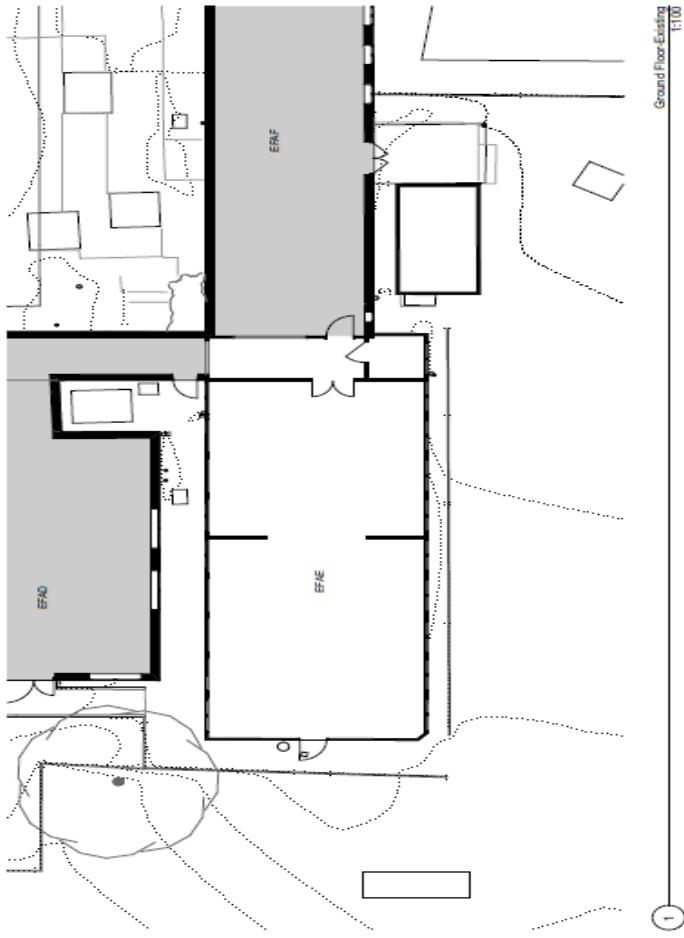
Case Officer: Matt Burns



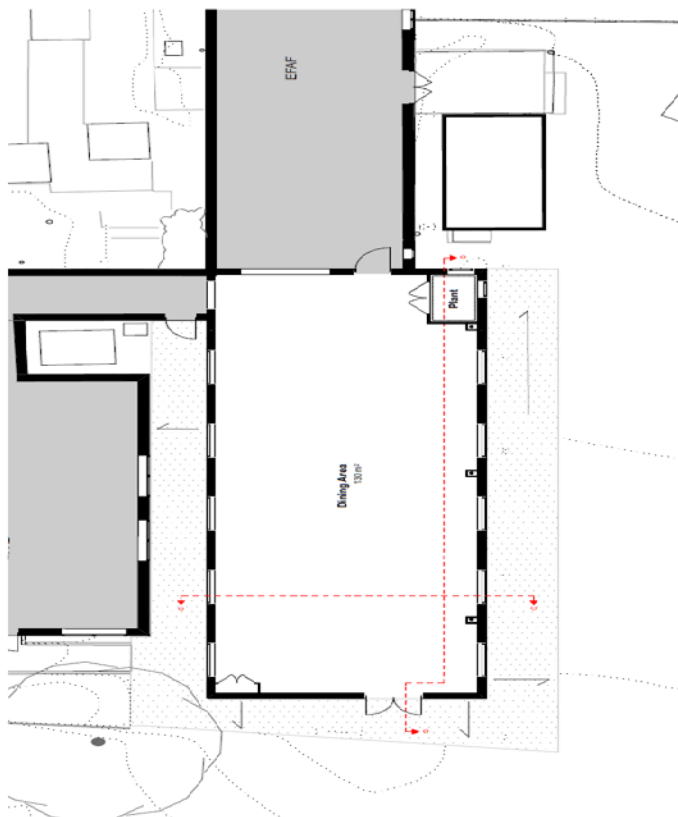
Existing Site Plan



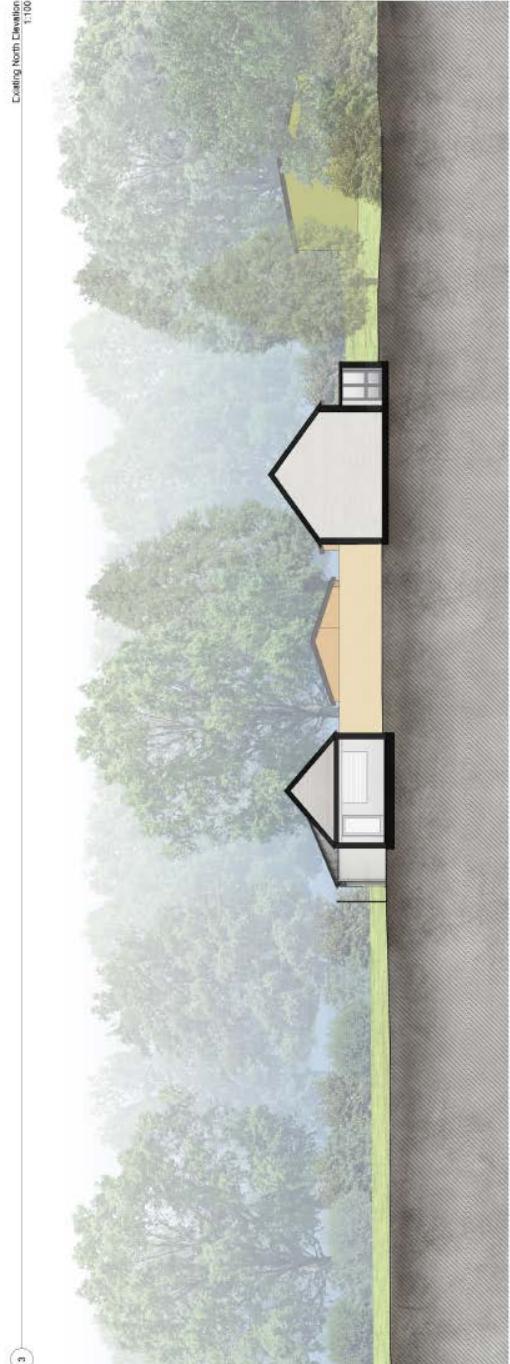
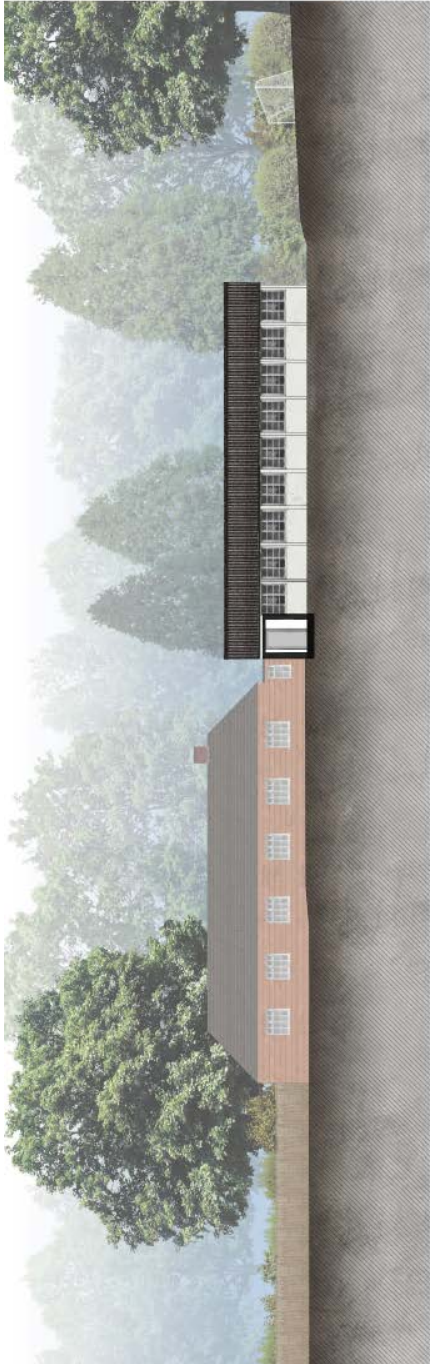
Proposed Site Plan



Existing Floor Plan



Proposed Floor Plan



Existing North and East Elevations





Existing South Elevation  
1:100



Existing West Elevation  
1:100

Existing South and West Elevations



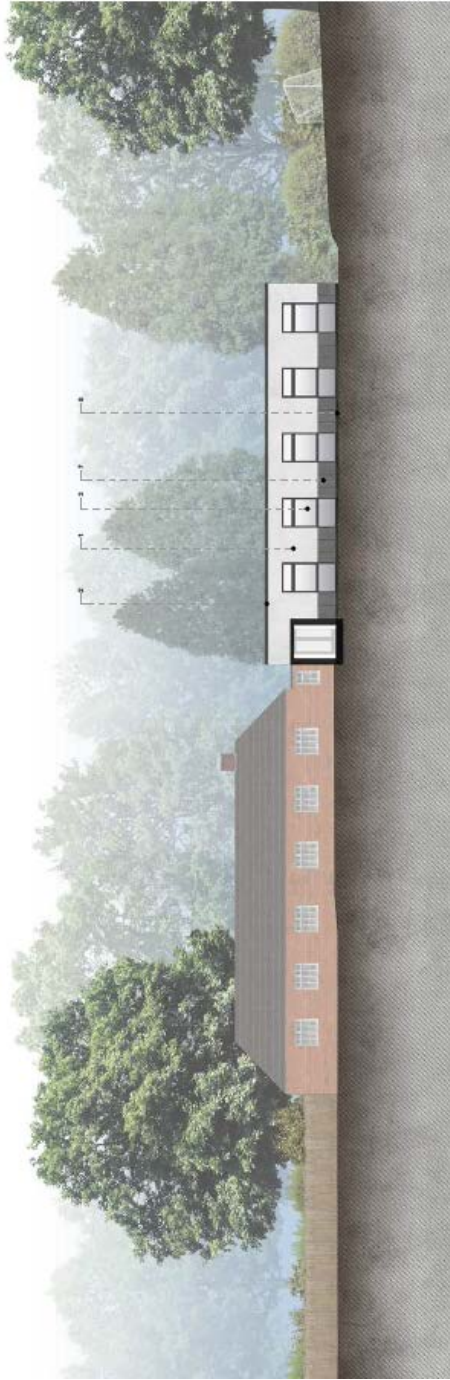
South Elevation



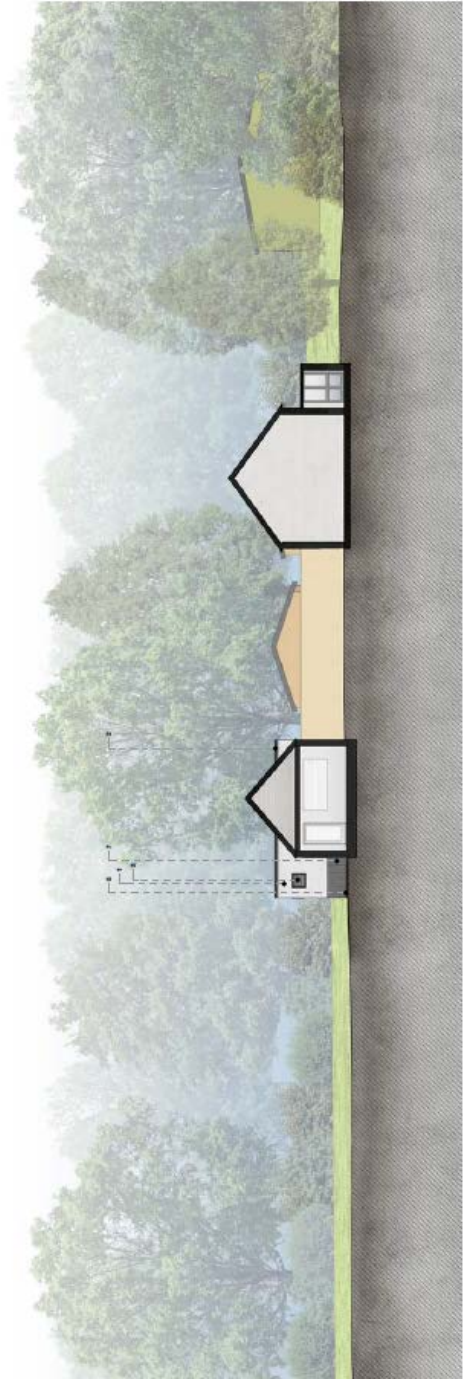
West Elevation

Proposed South and West Elevations





North Elevation



East Elevation

Proposed North and East Elevations



# TILEHURST



## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT AND NEIGHBOURHOOD SERVICES

READING BOROUGH COUNCIL

ITEM NO. 18

PLANNING APPLICATIONS COMMITTEE: 27<sup>th</sup> June 2018

Ward: Tilehurst

App No.: 180171/REG3

Address: Moorlands Primary School, Church End Lane, Reading

Proposal: School expansion from a two form of entry (420 pupils) to a three form of entry (630 pupils) to include two, two-storey double modular units (with new cladding), one single storey modular building (with new cladding) and two single storey extensions, demolition of single temporary classroom, retention of 2 double modular units, external landscaping works and increase in car parking numbers including off- site parking on adjacent Recreation Ground.

Applicant: Reading Borough Council

Date validated: 28<sup>th</sup> January 2018

Other Application: 8 week target decision date: 29<sup>th</sup> June 2018.

26 week date: 29<sup>th</sup> July 2018.

### RECOMMENDATION

Delegate to Head of Planning, Development and Regulatory Services to (i) GRANT permission subject to completion of a S106 legal agreement but (ii) to REFUSE permission should the legal agreement not be completed by 29th June 2018 (unless the Head of Planning, Development and Regulatory Services agrees to a later date for completion of the legal agreement). The legal agreement to secure the following:

To secure financial contributions of £5,000 to enhance the parking restrictions along Church End Lane and £40,000 to improve the pedestrian crossing facilities.

Conditions to include:

1. Time limit
2. Materials
3. Approved Plans
4. Programme of archaeological work
5. Vegetation Clearance
6. Biodiversity Enhancements
7. Reporting of unexpected contamination
8. CMS
9. Hours of working
10. Bonfires
11. External lighting
12. Noise Assessment
13. Sustainability statement
14. Sustainable Drainage - details to be submitted
15. Sustainable Drainage - in accordance with approved details
16. Hard and soft landscaping - details to be submitted
17. Landscaping implementation
18. Standard Landscaping Maintenance
19. Arboricultural Method Statement
20. Car park management

Informatives to include:

1. Pre-commencement conditions
2. S106 Agreement
3. Positive and proactive
4. Terms and conditions of permission
5. Building Regulations
6. Construction and demolition
7. Recommendations in the Ground Investigation Report

## 1. INTRODUCTION

- 1.1 The school is located on Church End Lane. The buildings on site are a mixture of single and two storeys with flat and pitched roofs. The school has two existing modular buildings. The surrounding area is predominantly residential. To the south west is Meadway Recreation Ground, beyond which is a church and to the north west is Blundell Copse, identified as a strategic open space and biodiversity opportunity area in the Development Plan.



## 2. PROPOSAL

- 2.1 The application is for two, two-storey double modular units to the north west of the existing school, one single storey modular building to the west of the existing school and two single storey extensions to the front elevation of the existing school building. There has been a slight amendment to the single storey modular building from that originally submitted as it has been increased in height by 283mm with the addition of two windows in each classroom. These amendments are required to meet natural ventilation requirements. The double modular units will be relocated from Alfred Sutton and Ridgeway Primary Schools and will allow teaching to continue in

the existing classrooms throughout the construction period without the need for temporary classroom accommodation. The modular units will have a light render finish with an element of vertical timber cladding. A single temporary classroom will be demolished and two existing double modular units are to be retained.

- 2.2 The total new build has a floor area of approximately 1,204m<sup>2</sup> and provides 10 classrooms, group working spaces, extended staff accommodation and ancillary accommodation for the increased pupil and staff numbers. The proposal also involves additional landscaping and tree planting and extensions to external play spaces and the reconfiguration and extension of the existing car park to provide 22 parking spaces and entrance forecourt.
- 2.3 The proposal also includes the provision of additional car parking adjacent to the existing public car park on the recreation ground. The additional car park was initially proposed to the rear of the existing car park but not to constrain options for the future of the recreation ground the proposed car park was relocated onto an adjacent basketball court. The basketball court would be relocated to the north of the existing courts.
- 2.4 Reading has a rising demand for Primary School places and Moorlands Primary School has been identified for expansion. The extension will enable the school to expand from a 2 form of entry (420 pupils) to a 3 form of entry (630 pupils). The school has already taken on additional bulge classes in 2012 and 2013 and there are currently 461 pupils. The school currently has 59.4 full time equivalent staff which is anticipated to increase to 73.2 full time equivalent staff.

### 3. PLANNING HISTORY

08/00418/FUL (Civica Ref: 080524) - Extension to car park, drop-off point for taxis and delivery vehicles, and create new front access and gates. Permitted 15/07/2008.

08/00462/FUL (Civica Ref: 080451) - Alterations and extensions to the administration wing. Permitted 15/07/2008.

12/00074/REG3 (Civica Ref: 121040) - Installation of roof mounted solar photovoltaic (PV) panels. Permitted 09/02/2012.

12/00906/REG3 (Civica Ref: 121623) - Provision of 1 temporary classroom unit and associated external works. Permitted 12/07/2012.

12/01578/FUL (Civica Ref: 120836) - Development of 1x 11-a-side junior football pitch, 1x 9v9 pitch, 1x 7v7 pitch, 2 team changing rooms, officials changing room, store building, access to hard surface and associated car parking. Permitted 08/01/2013.

151082/REG3 - Single storey temporary classroom. Permitted 22/09/2015.

160303/APPCON - Discharge of conditions 3, 4, 5, 6 and 7 of planning permission 151082/REG3. Discharged 12/04/2018.

### 4. CONSULTATIONS

- 4.1 Statutory:

Sport England - are satisfied that the Multi Use Games Area (MUGA), which was to be lost due to the car parking is now being replaced adjacent to the remaining MUGA. Sport England considers this to meet their planning policy exception E4. Sport England do not wish to raise an objection to this application.

#### 4.2 Non-statutory:

Development Control Transport - see Appendix A below.

Natural Environment Trees raised no objection subject to conditions.

Berkshire Archaeology raised no objection given the scale of the proposals and the foundation design. Berkshire Archaeology have recommended that the impact of the development on archaeological deposits could be mitigated by archaeological monitoring of all ground work and therefore raised no objection subject to a condition.

Thames Water advised they do not require an agreement due to the type of work being carried out.

Reading Borough Council Leisure - Full comments relating to the additional car parking were provided - the following is an agreed summary:

An assessment of the area by the Council's Leisure Department indicates that the land has limited recreational value. Previous proposals to locate facilities likely to attract evening use attracted negative comment from local residents. The space between the road and the enclosed courts (Multi-use games area and Tennis Court) was identified as a buffer and a location for an extension of car parking should demand increase (from intensification of sporting activity). It is anticipated that the former Meadway School Redgra area which has been reinstated as level grass will be used for formal sport and greater use of the hard surfaced sport area will be made. Current limited car parking restricts this intensification of use. The informal basketball court has been relocated into an area with limited value being isolated between existing courts and school.

A small loss of the recreation ground for school car parking will not make a material difference to the functionality and value of the open space. A replacement sports court is being provided and the availability of space for parking will increase the capacity of the recreation ground to support formal sport. However, the proposal does not identify a pedestrian access from the car park into the recreation ground. Appropriate access should be developed between Reading Borough Council Leisure Officers and the school during the detail design stage along with a future management strategy. This will enable the overflow car park to be available to clubs and other organisations who may in the future hire, or use, facilities at Meadway Recreation Ground outside of school hours free of charge.

Environmental Health - raised no objection subject to the suggested conditions above.

A Ground Investigation Report (terrafirma (south) report no. 5846/GI, June 2017) has been submitted and confirms the soil chemical testing results were all below the relevant guideline values for a Public Open Space -



Residential Development Scenario. As such, there are considered to be no contaminants of concern and the site as a whole can be considered uncontaminated. However a condition is required in case contamination is encountered.

In addition, the report also contains numerous recommendations for the development including engineering recommendations and foundation/floor slab recommendations. As such, the contractors should be made aware of this report and able to familiarise themselves with it. An informative will be included in the decision.

The Design and Access Statement provides some information about the proposed external lighting at the site but no additional information has been provided. A condition will be required for details of external lighting to be submitted.

Reading Borough Council Ecology advised the risk of the works adversely affecting protected species is minimal, subject to appropriate precautionary measures. It is recommended in Section 9 of the ecology report submitted with the application that any vegetation clearance should be undertaken outside of the bird nesting season. Other opportunities to incorporate biodiversity in and around the developments are also recommended. There are no Ecology objections to the proposal subject to conditions.

#### 4.3 Public consultation:

Properties at 10-24 (e) Calder Close, 38-44 (e) and 41-85 (o) Church End Lane and Neath Gardens (all) were consulted. A site notice was posted to the front and side of the site on 15<sup>th</sup> February 2018 with a 21 day consultation date of 8<sup>th</sup> March 2018. Two letters of objection have been received with regards to:

1. Inadequate parking provision.
2. Overlooking.
3. Noise pollution during construction.

Amended plans letters were sent to all residents advising of the relocation of the proposed car park with a 14 day consultation date of 3<sup>rd</sup> April 2018. At the time of writing one letter of observation has been received with regards to:

1. No assessment has been made of the current on-road parking nor the impact of the school extension.
2. The proposed relocation of the recycling bins is likely to create a deleterious visual impact and they should remain as far away from the road as possible.

Amended plans letters were sent to all residents advising of the relocation of the basketball court, relocation of recycling bins and the submission of a Travel Survey with a 14 day consultation date of 23<sup>rd</sup> May 2018. One letter of objection has been received with regards to:

1. The staff car parking remains totally inadequate and it is unclear how the parking for the nursery provision staff will be accommodated in the proposals.

## 5. RELEVANT PLANNING POLICY AND GUIDANCE

- 5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that proposals be determined in accordance with the development plan unless material considerations indicate otherwise. Material considerations include relevant policies in the National Planning Policy framework (NPPF) - among them the 'presumption in favour of sustainable development'.
- 5.2 The following local and national planning policy and guidance is relevant to this application:

### National Planning Policy Framework (2012)

Paragraph 72

### Reading Borough Core Strategy (January 2008):

- CS1 (Sustainable Construction and Design)
- CS4 (Accessibility and the Intensity of Development)
- CS5 (Inclusive Access)
- CS7 (Design and the Public Realm)
- CS22 (Transport Assessments)
- CS24 (Car / Cycle Parking)
- CS28 (Loss of Open Space)
- CS31 (Additional and Existing Community Facilities)
- CS34 (Pollution and Water Resources)
- CS36 (Biodiversity and Geology)
- CS38 (Trees, Hedges and Woodlands)

### Reading Borough Sites and Detailed Policies Document (2012):

- DM4 (Safeguarding Amenity)
- DM12 (Access, Traffic and Highway-Related Matters)
- DM17 (Green Network)
- SA16 (Public and Strategic Open Space)

### Supplementary Planning Document

- Revised Parking Standards and Design (Oct 2011)
- Sustainable Design and Construction (July 2011)

## 6. APPRAISAL - Planning Applications

- (i) Legal context

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that applications for planning permission be determined in accordance with the development plan unless material considerations indicate otherwise.

- (ii) Main Issues

- 6.1 The main issues are considered to be:
- (i) The principle of additional classroom accommodation;
  - (ii) Loss of open space
  - (iii) The effect upon visual amenity and the public realm
  - (iv) Impact on neighbouring amenity
  - (v) Traffic generation and parking
  - (vi) Trees

- (vii) Environmental Issues
- (viii) Other Matters

(i) The principle of additional classroom accommodation

6.2 A rising population in Reading has seen increasing demand for primary places and as a result Reading Borough Council needs to increase the number of primary school places in a number of schools within the borough.

6.2.1 Policy CS31 (Additional and Existing Community Facilities) of the Reading Borough Core Strategy states that “Proposals for new, extended or improved community facilities will be acceptable, particularly where this will involve co-location of facilities on a single site.” The site is within an existing school site and would provide extended and improved community facilities which would meet an identified need within the Borough. As such it is considered that the general principle of increased classroom provision is in accordance with policy CS31 of the Reading Borough Core Strategy.

(ii) Loss of open space

6.3 The expansion of the school complies with Policy CS31 of the Core Strategy and the relevant national planning policy considerations above, and would help to meet the Council’s statutory duty to provide a school place for every child. Development Plan Policy CS28 also needs to be considered as it is opposed to the loss of all open space.

6.3.1 The proposed modular buildings are on an area of the existing hard play space and the amendments to the existing staff car park would require the loss of some of the existing grassed open space to the front. The proposed off-site car park would be on the adjacent recreation ground however the basketball court where the off-site car park is proposed is to be relocated to the north of the existing courts.

6.3.2 The proposed works facilitate a permanent extension to the school to allow an increase in the number of pupils from 461 (including the existing bulge classes) to 630. Sport England do not object to the amended location of the off-site car park as the basketball court is to be re-provided.

6.3.3 The National Planning Policy Framework (NPPF) is clear that schools are an important aspect of sustainable development. Paragraph 72 states:

The Government attaches great importance to ensuring that a sufficient choice of school places is available to meet the needs of existing and new communities. Local planning authorities should take a proactive, positive and collaborative approach to meeting this requirement, and to development that will widen choice in education. They should:

- give great weight to the need to create, expand or alter schools; and
- work with schools promoters to identify and resolve key planning issues before applications are submitted.

6.3.4 DCLG issued a joint statement by the Secretary of State for Local Government and the Secretary of State for Education in 2011 entitled ‘Policy Statement - Planning for Schools Development’, which is material to the consideration of this application. This states, inter alia:

6.3.5 The Government believes that the planning system should operate in a positive manner when dealing with proposals for the creation, expansion and alteration of state-funded schools, and that the following principles should apply with immediate effect:

- There should be a presumption in favour of the development of state-funded schools, as expressed in the National Planning Policy Framework.
- Local authorities should give full and thorough consideration to the importance of enabling the development of state-funded schools in their planning decisions.
- Local authorities should make full use of their planning powers to support state-funded schools applications.
- Local authorities should only impose conditions that clearly and demonstrably meet the tests set out in Circular 11/95.
- Local authorities should ensure that the process for submitting and determining state-funded schools' applications is as streamlined as possible.
- A refusal of any application for a state-funded school, or the imposition of conditions, will have to be clearly justified by the local planning authority.
- Appeals against any refusals of planning permission for state-funded schools should be treated as a priority.
- Where a local planning authority refuses planning permission for a state-funded school, the Secretary of State will consider carefully whether to recover for his own determination appeals against the refusal of planning permission.

6.3.6 Taking all these factors into account, it is considered that the loss of some of the hard play area and the off-site car park would be acceptable in this case.

(iii) The effect upon visual amenity and the public realm

6.4 The majority of the proposed extensions are single storey other than the two storey modular unit which is located at the rear of the existing school. The existing school is set back from Church End Lane and the proposals will be screened by existing hedging along Church End Lane. The proposed modular units would be visible from the recreation ground and the two storey modular from Calder Close however some screening will be provided by existing trees and hedging along these boundaries.

6.4.1 The proposed modular units will be rendered and will include timber cladding to improve their appearance. The two storey units will be located adjacent to the existing two storey school and the single storey unit adjacent to the existing single storey part of the school. The single storey extensions to the front of the existing school have flat roofs (to match the existing school) and the materials and detailing will be similar to the existing school.

6.4.2 The proposed additional car park will be visible from Church End Lane, however additional landscaping is proposed and this will reduce the visual impact of the car park on the surrounding area. The re-provided basketball court will be set further back from the road between the school and the existing courts.

- 6.4.3 The effect on visual amenity is not considered to be significant and overall the proposed new buildings and arrangements are considered acceptable in terms of design and appearance and in accordance with policy CS7 of the Core Strategy.
- (iv) Impact on neighbouring amenity
- 6.5 Public consultation on the expansion of the School has been undertaken by the applicant, which included two public exhibitions. Following feedback from the exhibitions the two storey modular unit was moved approximately 8.5m further away from the boundary with neighbouring properties.
- 6.5.1 The nearest residential properties are along Church End Lane, Neath Gardens and Calder Close. The proposed off-site car park is within the vicinity of the existing car park in the recreation ground. The car park will be used by school staff during the week and at limited other times. Reading Borough Council Parks department will also have access for maintenance to the recreation ground. Although it was not initially proposed to open the car park out of school hours it will be available for clubs/organisations using the recreation ground.
- 6.5.2 The proposed two storey modular units will be to the rear of the site and are closest to residential properties on Calder Close and there are rear windows. However the modular unit is located to the east of Calder Close and both the unit and the properties are angled away from each other which mitigates any concerns with regards to overlooking. There is also a distance of approximately 8.5m from the side elevation of this modular building to the closest residential property on Calder Close (and no first floor side windows are proposed) and although this modular will be visible it is unlikely to impact neighbouring properties in terms of loss of light, loss of outlook or overbearing effects due to the distances between the properties and the proposal.
- 6.5.3 However, the proposal will facilitate an increase in the number of pupils at the school which could increase the potential for noise associated with it. However, in the context of the established school use it is unlikely that any additional noise would result in significant harm to neighbouring occupiers in terms of noise or disturbance.
- 6.5.4 Environmental Protection has advised that a Noise Assessment of any proposed plant/equipment will be required and this can be dealt with by way of a condition.
- 6.5.5 External lighting is proposed and will comprise typically LED wall mounted and under canopy luminaires to illuminate all final exits from the buildings. The staff car park (on and off-site) will have column mounted LEDs. To ensure there is no harm to neighbouring properties from artificial lighting a condition will be imposed requiring details of external lighting to be submitted to, and approved by, the local planning authority. Conditions requiring the submission of a Construction Method Statement and restricting hours of construction work and prohibiting bonfires are also recommended.

- (v) Traffic generation and parking
- 6.6 Further to the comments provided by Transport the applicant is undertaking additional surveys. This additional information has not yet been submitted and an update will be provided.
- 6.6.1 Transport have requested a contribution of £5,000 be provided to enhance the parking restrictions along Church End Lane and as a result of the additional pedestrian movements alongside the additional vehicle movements and parking demand surrounding the school, pedestrian crossing facilities should be enhanced. A contribution of £40,000 is required to improve the pedestrian crossing facilities which would facilitate the provision of a controlled zebra crossing and another at grade uncontrolled crossing. This will be dealt with by way of a legal agreement.
- (vi) Trees
- 6.7 The proposals involve the removal of a number of trees and the submitted Tree Survey demonstrates that these trees are of sufficiently low quality to justify their removal either in arboricultural grounds or to allow for development. The proposal will provide a minimum of 13 new trees to be planted within the school grounds which is positive along with additional hard and soft landscaping. These plans will be in conjunction with comments from Ecology in regards to biodiversity enhancements. Indicative plans have been provided however the location of the trees will need to be formally agreed with the school. This can be dealt with by way of conditions.
- 6.7.1 The proposed off-site car park would not impact on adjacent trees however soft landscaping around the proposed car park would be considered appropriate (subject to agreement with Parks). This matter can be dealt with by way of a condition.
- (vii) Environmental Issues
- 6.8 In relation to sustainability, the Council's policy requirement is that major non-residential developments meet a BREEAM score of 62.5% (halfway between 'Very Good' and 'Excellent'). The applicant states that they will not be able to undertake a formal BREEAM assessment given the cost implications and the requirement of the school to open in time for the 2015-2016 academic year. They have, however, submitted a Sustainability Statement to support the application which states that although BREEAM would not be sought, 'the intention remains to create sustainable school buildings that will comply with the principles of sustainable construction, design and energy efficiency'. The key points contained within the statement are that the development would:
- include a commitment to low carbon design to reduce energy requirements
  - adopt the principles of BREEAM
  - use daylighting to reduce artificial lighting/energy use
  - include a natural ventilation system
  - improve biodiversity as part of the landscaping proposals
  - include conservation measures such as bat and bird boxes
  - incorporate sanitary fittings with low water usage.

6.8.1 The development would not comply with specific requirements as set out in Policy CS1 of the Core Strategy (or the Council's adopted Sustainable Design and Construction SPD). However, it would meet the objectives of this policy by providing a sustainable building, subject to the development being carried out in accordance with the principles as set out in the Sustainability Statement, which is proposed as a condition.

(viii) Other Matters

6.9 In determining this application the Council is required to have regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sexual orientation. There is no indication or evidence (including from consultation on the application) that the protected groups have or will have different needs, experiences, issues and priorities in relation to the particular planning application.

6.9.1 In terms of the key equalities protected characteristics it is considered there would be no significant adverse impacts as a result of the development.

6.9.2 The Community Infrastructure Levy (CIL) was implemented by the Council from April 1st 2015. Although the proposed scheme would be CIL liable development, because education facilities attract a zero CIL charge in the Borough there would be no CIL payable for this scheme.

## 7. CONCLUSION

7.1 Subject to the outstanding matters above being resolved the proposed development is considered acceptable in planning terms and for the reasons set out in the report above.

Case Officer: Claire Ringwood

## Appendix A - Transport comments

It is proposed that the School would expand from 420 to 630 permanent pupil places (a 1 form entry increase; 210 additional permanent spaces, 30 children per year group) with progressive entry from September 2019. It is expected that the full 630 spaces would be taken up by September 2025 year. This equates to 169 spaces over the current number on roll.

The School needs to expand in order for it to meet the current shortfall and anticipated increase in pupil numbers arising principally from new residential developments in Central West Reading identified in the Reading Housing and Economic Land Availability Assessment (May 2017). The proposed new residential sites would be located within a two mile walking radius of the School.

The school day begins at 8.55am and ends at 3:10pm for years R to 2, and 3.15pm for years 3 to 6. A morning club is available before school from 8.00am. An after school club runs from 3:15 pm until 4.15 pm on Wednesdays only. A nursery school also operates from the site.

The School is located on Church End Lane in a residential area with a good network of footways. It is north of The Meadway and south of Norcot Road; both local distributor roads with primary bus routes, 30mph speed limits and street lighting. The area of Church End Lane surrounding the main pedestrian and vehicular entrance to the School is subject to a 20mph zone. Parking restrictions including zig-zag lines and double yellow lines are in place in the vicinity of the School.

The School has three pedestrian accesses into the School site from Church End Lane and from a footpath connecting Church End Lane with Calder Close. There is also a walking route in the form of an unmade path from Teviot Road linking into the footpath between Church End Lane and Calder Close. There is another pedestrian entrance into the nursery site. Vehicular access into the staff car park is from Church End Lane, separate from the pedestrian entrance. Teviot Road and Calder Close are subject to 30mph limits.

A scoping Note had previously been provided and it had been confirmed that a Transport Statement would be sufficient. I have reviewed the Transport Statement and I comment as follows:

### Trip Rates

An assessment has been undertaken that assigns the trip rate mode of the existing pupils to the proposed increase in pupil numbers based on where those children would be travelling from. This is an acceptable methodology but can it be confirmed that all the children identified within the residential site allocation would be required to attend Moorlands or whether the allocation of pupils will be reviewed to ensure that pupils could actually attend a school within a closer proximity to their place of residence.

If the children would be allocated their closest school (subject to parent choice) I would be happy to use the overall travel percentage by car which would be lower than that currently assessed.

The result of the surveys currently provided identifies increases per mode as stipulated in Table 4 below (Taken from the Transport Statement).



**Table 4: Projected pupils per mode for new pupils.**

	Walk	Car	Bus	Cycle or Scoot	Park and Stride	Total
Projected new pupils per mode*	55	93	34	5	23	210

\*Figures have been rounded.

It has been stated that a person dropping the child at School may return home, or travel onwards as part of a linked trip e.g. a parent driving to work. It has therefore been stated that the vehicles associated with pupils travelling by car could generate two two-way trips; one in the morning and one in the afternoon as these vehicles will not remain on the school site during the day. When combined, the projected number of new two-way trips (car and park and stride) associated with additional pupils in each of the AM and PM peaks is 116 (93+23).

A bulge class is currently accommodated at the school and cars associated with the bulge year children can be removed from this new demand. Based on the current modal split this equates to a reduction of 6 vehicles, taking the new demand from 116 down to 110.

A total of 22 new staff in 14 full time equivalent posts will be associated with the school expansion, again with growth in numbers over time. Table 5 below (Taken from Transport Statement) identifies the projected number of new staff at 2025 per mode based on a recent staff survey.

**Table 5: Staff Trip Generation**

Trip generation by mode for staff (2017)	Walk	Cycle	Car Alone	Car Share	Bus	Other	Total
Current staff modal split %	12.8	0.0	79.4	2.6	2.6	2.6	100%
Projected new staff per mode	2.82	0.00	17.47	0.57	0.57	0.57	22

The vehicles associated with new staff would result in new one-way trips in the morning and afternoon, as the staff would park on site. Therefore, it can be expected that there would be up to 18 one-way vehicular trips on the highways in the morning and afternoon/evening associated with new staff.

From the calculations above, the applicant has projected that there would be an additional 220 one-way trips by car associated with pupils and 18 new one-way trips by car in both the morning and afternoon/evening associated with staff. This results in a total of 238 vehicle movements in both of these periods.

The applicant has deemed this a robust methodology as it does not include the following caveats that are likely to reduce new trips by car:

*Breakfast / after school clubs*

The existence of breakfast and after school clubs is likely to space out the arrival/departure of pupils and further lessen the traffic at peak times. However, as has been stated the breakfast club starts at 8am and an after school club only occurs on a Wednesday. It has also not been confirmed how many children can currently be accommodated at these clubs and whether this is to be expanded / increased following the expansion of the school.

### *Sibling data*

The “car” and “park and stride” modes assume that one child is travelling with one driver, the school survey used by the applicant collected data per child and not per car. Children within one family who would naturally travel together in one car have therefore been counted separately in these calculations. The trip generation is therefore an overestimate of car trips associated with pupils.

It has been confirmed that there are currently 118 pupils at the School with at least one sibling also attending; following the current modal split, 38% of these children would arrive by car and a further 6% by park and stride; the applicant has therefore stressed that car sharing within families would reduce the cars associated with these children from around 52 to around 26. However, I do not understand how this conclusion was obtained and further clarification should be provided.

The assessment I have undertaken has established that 28% of the school currently has a sibling, I am therefore happy for this percentage to be reduced from the proposed projected number of pupil trips as these would already be on the network. See Table below:

	Walk	Car	Bus	Cycle / Scoot	Park and Stride	Total
Projected New Pupils Per Mode	55	93	34	5	23	210
Projected New Pupils Per Mode Minus Those With Siblings	40	67	24	4	17	151

Given the above the proposal would still generate 74 vehicle movements associated with the proposed increase in pupil numbers.

### *Travel Plan*

The new intake of pupils will be phased over a number of years which will enable the School Travel Plan to have time to support children and their parents to change towards more sustainable modes. The measures included within the Travel Plan are deemed acceptable and the timescales sufficiently spread leading up to the opening of the expansion so as to not be too daunting a task to implement. One option not included within the Travel Plan is to review the use of cycle / scooter parking and to provide additional parking should it be required.

Although these points may help to reduce the overall numbers this has not been fully assessed and therefore I cannot fully take this into account, my own assessment has also identified that substantial trip numbers would still be generated as detailed in the table above.

As requested by officers automated traffic counts (ATCs) for speed and volume were undertaken due to the existing pressures within the surrounding area especially at the Church End Lane / The Meadway signalized junction and the Church End Lane / Norcot Road priority junction. The surveys were undertaken from 15th to 22nd of November on The Meadway and Church End Lane.

Of note, The Meadway demonstrated a two-way 24 hour mean average speed of 26mph (the posted limit is 30mph) and a two-way, 5 day, 24 hour average daily flow of just under 15,000 vehicles. Multiple controlled crossings are located along this road to help pedestrians to cross, including crossings close to the junction with Church End Lane.

Church End Lane demonstrated a two-way 24 hour mean average speed of 21.75mph, lowering to 20.15mph from 8-9 AM and 20.3mph from 3-4 PM (around School start and end times). These speeds are very close to the 20mph posted zone limit. The average two-way daily flow was much lower than The Meadway at 5,450 vehicles.

It has been stated at Paragraph 4.4.4 that ‘*assuming all vehicles associated with new pupils and staff travel along Church End Lane, up to an additional 238 two-way trips a day (116 in the AM and PM school peaks) would be generated by the expanded School*’, however this is in conflict with Paragraph 3.1.8 and Table 6. These state the following ‘*it is projected that there 3.1.8.would be an additional 220 one-way trips (110 two-way trips) by car associated with pupils and 18 new one-way trips by car in both the morning and afternoon/evening associated with staff. This results in a total of 238 vehicle movements in both of these periods, as can be seen in Table 6*’.

**Table 6: Projected Trip Generation**

<b>Combined projected trip generation due to new expansion per peak</b>	
<b>One way trips associated with pupils</b>	220
<b>One way trips associated with staff</b>	18
<b>Total trips per peak</b>	<b>238</b>

This would therefore need to be clarified. I would reiterate the point at Paragraph 3.1.3 that states that ‘if accompanied, the person dropping the child at School may return home, or travel onwards as part of a linked trip e.g. a parent driving to work’ the highlighted section therefore confirms that two-way trips could be generated during the drop-off and pick of children. It would therefore not be as simple and doubling or halving the travel modes etc.

Irrespective of the above the assessment undertaken represents a c.4% increase in daily traffic along Church End Lane. At the School peaks where these trips would likely be concentrated the new trips (128 increase in vehicle trips) represent a 26% increase in the AM and 32% increase in the PM over the existing average flow (489 vehicles in the AM and 396 in the PM).

When based against my assessment for sibling data these new trips could be reduced to 102 which would represent a 21% increase in the AM Peak and 26% in the PM Peak.

These calculations are only based on one-way trips and therefore two-way trips would significantly increase any impact on the network. However, regardless of this these increases still represent a material increase in vehicle flows within these

peak periods and will impact the Church End Lane / The Meadway signalized junction and the Church End Lane / Norcot Road priority junction. As a result of these increasing these aforementioned junctions should be fully assessed.

#### Parking

Approximately 16 parking spaces are currently provided on the school site, one of which is accessible; only 5 of these spaces are marked out. Additionally, when the ground is dry enough, vehicles also park in tandem on a grass mat area behind this car park. During a site visit on 15.11.17 the applicant has claimed that 22 vehicles were parked in total. These spaces are for staff and visitors only; pupil drop off and pick up is not normally permitted on site.

The Councils Parking standards requires a maximum provision of 1 space per FTE member of Staff and therefore equates to a provision of 14 additional spaces, 3 of which should be accessible. A motorbike space is also required.

The applicant has however expressed that there is currently pressure on the parking provision at the existing School resulting in the need for a number of staff to park on the highway. It is therefore agreed that in order to relieve this parking pressure, and bring the parking provision closer to the projected provision for a 3FE School, additional formal parking spaces will be provided. 22 formally marked and surfaced spaces are proposed (to match the current informal provision) on site and 24 spaces will be delivered off site, within a new parking area accessed through the adjacent recreation ground car park. This provision has been deemed acceptable. It is also proposed that 2 motor bike spaces will also be provided which exceeds the standard by 1 space.

The existing public car park can accommodate 15 cars with the extra space accommodating recycle bins. The proposed access from within this car park would reduce this parking further, although it is noted that the submitted drawing illustrates a provision of 15 spaces. The retention of the car parking spaces is due to the extension of the hardstanding area to re-provide for the bins currently located within the car park. In the circumstances there are no objections given the current parking numbers are retained.

The proposed staff car park on the adjacent Recreation Ground would be accessed through the public car park via a controlled barrier providing fob/ card reader access to enter and an induction loop release on exit, using a power supply from the existing school site. The car park will be used by school staff during the week only and at limited other times. RBC Parks department will also have access for maintenance to the Recreation Ground. It is not proposed to open the car park out of school hours for public use, which could cause management difficulties for the school.

All the illustrated car parking spaces have been illustrated to the required standards.

Car park surveys have been undertaken of the surrounding area and this has identified that there is an increase in on street parking around the drop off and pick up times at the school. Overall this is well distributed throughout the survey area and it is identified that on street parking capacity is still available to accommodate any additional short term parking. However, it is noted that there is substantial capacity on Church End Lane where I would anticipate any additional parking to be located given the distances that pupils are expecting to travel and the desire for parents to park as close to the school as possible. This would generate increased parking on both sides of the carriageway which would

detrimentally impact the flow of vehicles and also the visibility of pedestrians crossing Church End Lane. A contribution of £5,000 should be provided to enhance the parking restrictions along Church End Lane.

As a result of the additional pedestrian movements alongside the additional vehicle movements and parking demand surrounding the school pedestrian crossing facilities should be enhanced. A contribution of £40,000 is required to improve the pedestrian crossing facilities which would facilitate the provision of a controlled zebra crossing and another at grade uncontrolled crossing.

The Councils standards require 3 cycle spaces for staff and 16 for pupils. It has been stated that the School already has an under utilised covered cycle shelter directly in front of the School reception and for this reason, the expansion does not propose to add staff cycle parking. However before this can be agreed evidence is required to demonstrate this underutilization.

The development proposes to increase cycle and scooter provision above the 16 spaces required for pupils. It is proposed that 15 racks (providing 30 spaces) would be provided for bicycles, in addition a scooter rack or pod will also be provided. Given that this is in excess of the Councils standards this is acceptable however a revised drawing will be required demonstrating that this cycle parking is to be covered and the spaces to the required spacings etc. I am however happy for this to be dealt with by way of a condition.

#### Access

Access arrangements to the school are to remain as existing and these are therefore deemed acceptable. Tracking diagrams have also been submitted to demonstrate that a fire appliance can access and egress the rear of the site.

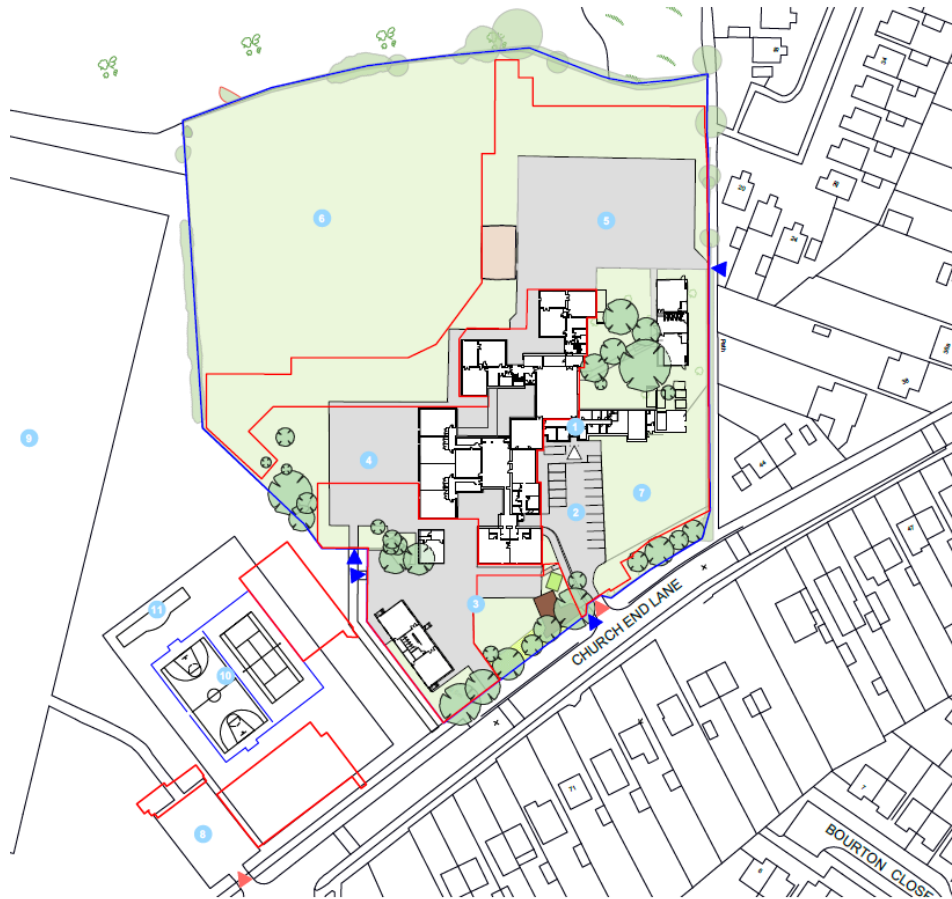
Please ask the applicants agent to submit suitably amended plans / information to address the above points prior to determining the application.

#### S106

A contribution of £5,000 should be provided to enhance the parking restrictions along Church End Lane.

As a result of the additional pedestrian movements alongside the additional vehicle movements and parking demand surrounding the school pedestrian crossing facilities should be enhanced. A contribution of £40,000 is required to improve the pedestrian crossing facilities which would facilitate the provision of a controlled zebra crossing and another at grade uncontrolled crossing.

# Existing Site Block Plan



- Existing Site Key:**
- Site Ownership Boundary
  - Development Site Boundary  
School site area c. 1650sqm  
Playing Field site area c. 1258.5sqm
  - ▼ Pedestrian Entrance
  - ▼ Vehicular Entrance
  - ▽ Main Entrance
  - 1 Main Entrance & Admin Facilities
  - 2 Staff & Visitor Parking
  - 3 Early Years Play Space
  - 4 Key Stage 1 Playground
  - 5 Key Stage 2 Playground
  - 6 School Playing Fields
  - 7 Overspill Staff Parking
  - 8 Public Car Park
  - 9 The Meadow Recreation Ground
  - 10 Tennis Courts
  - 11 Skate Park



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NO.	REVISION	DATE	BY
01	Issue for Planning	28/01/18	RS
02	For submission to Planning	28/01/18	RS
03	Finalised and approved. Plan for a new layout	28/01/18	RS

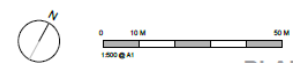
Hampshire County Council		1422 Planning Services, Three Moates House, 21 High Street, Alton, Hampshire GU34 5AL, UK (01463 57070)	
PROJECT <b>Moorlands Primary School                  Expansion to 3FE                  Church End Lane</b>		DRAWING CONTENT <b>Existing Site Block Plan</b>	
SCALE 1:500 @ A1 1:1000 @ A3	DATE 28.01.18	DRAWN RS	CHECKED HD
PROJECT NO. <b>E03153-A-1005</b>		DRAWING NO. <b>P3</b>	

# Proposed Site Block Plan



- Proposed Site Key:**
- Site Ownership Boundary
  - Development Site Boundary  
School Site area c. 9550sqm  
Playing Field site area c. 1328.5sqm
  - ▼ Pedestrian Entrance
  - ▼ Vehicular Entrance
  - ▼ Main Entrance

- 1 Refurbished Main Entrance & Admin Facilities
- 2 Staff & Visitor Parking (parking reconfiguration to provide 22 no. spaces)
- 3 Early Years Play Space
- 4 Key Stage 1 Playground
- 5 Key Stage 2 Playground, extended
- 6 School Playing Fields
- 7 New off-site staff car parking (24no. spaces)
- 8 Public Car Park
- 9 The Meadoway Recreation Ground
- 10 Fenced Courts
- 11 Skate Park
- 12 Proposed Year R Extension
- 13 Proposed Key Stage 1 teaching block
- 14 Proposed Admin Extension
- 15 Proposed Key Stage 2 teaching block
- 16 Relocated basketball court



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Aerial View facing North



KS2 Block view facing North



Aerial View facing North West



Aerial View facing South

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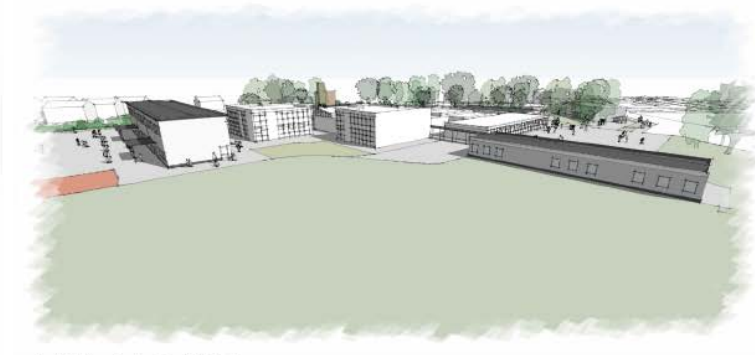
View Towards Entrance and Admin Area



View Facing Year 2 Classrooms



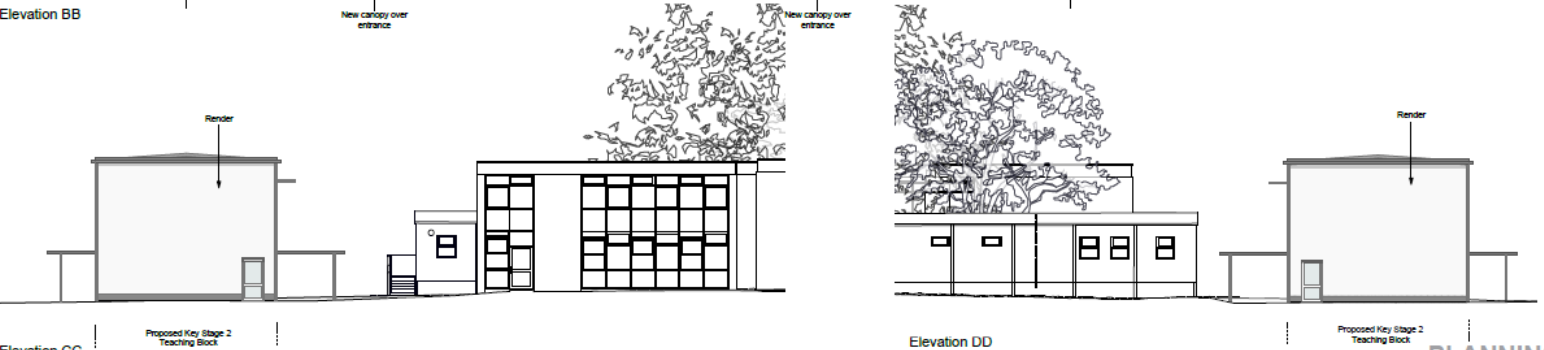
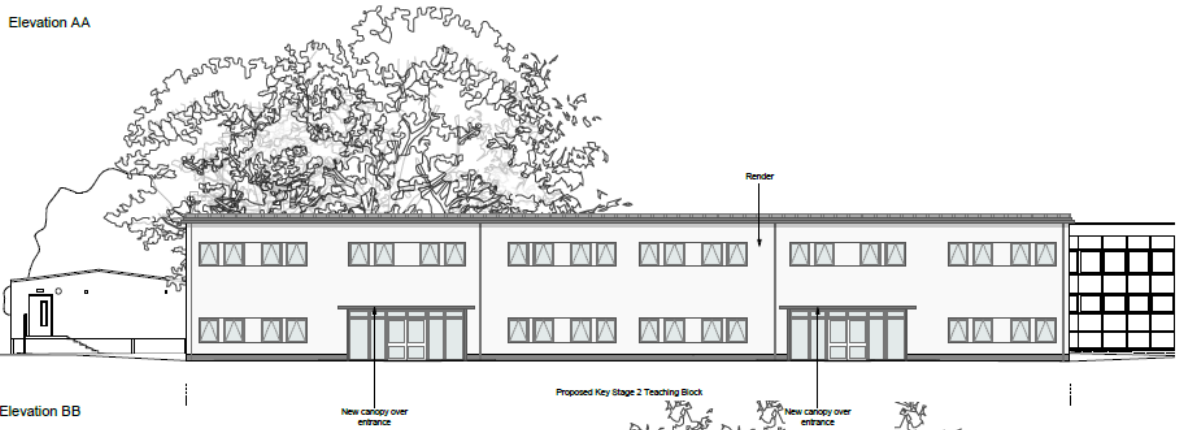
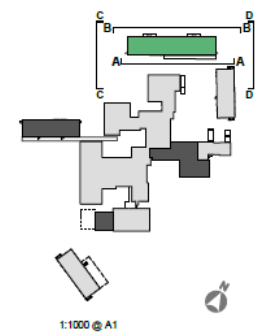
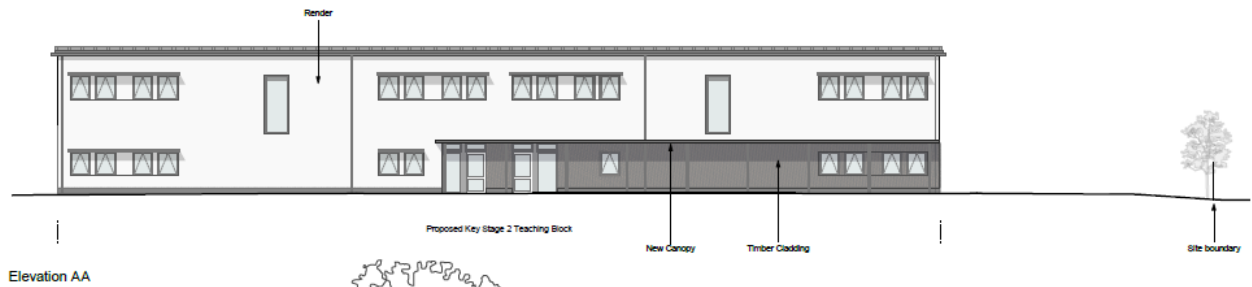
View facing Year R Classrooms



Aerial View facing South East

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	<b>Hampshire County Council</b> Moorlands Primary School Expansion to 3FE Church End Lane		<b>Hampshire County Council</b> Planning Services Three Moors House, 25 High Street, Winchester SO20 0LJ, Wiltshire, Hampshire SO20 0LJ, UK Tel: 01962 547001		<b>Proposed Visualisations</b> Sheet 2		<b>PROJECT NO.</b> E03153-A-1801	<b>SCALE</b> P1				



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Rev	Description	SN	SD
1	Issue for Planning		

**PLANNING**

**Hampshire County Council**

**PROJECT**  
Moorlands Primary School  
Expansion to 3FE  
Church End Lane

**DATE**  
1:100 @ A1  
1:200 @ A3

**DATE**  
26.01.18

**DESIGN**  
RS

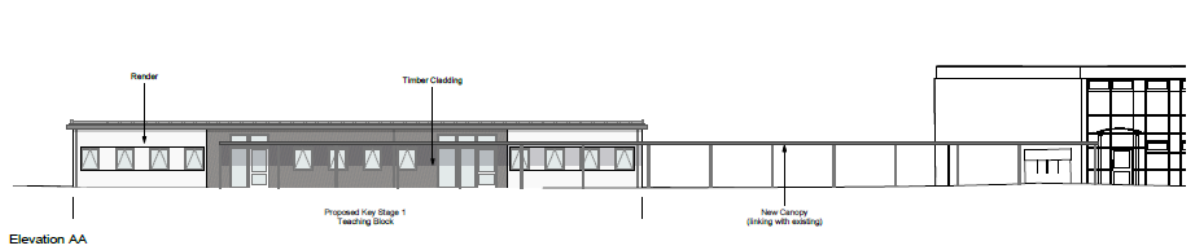
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HD

**SCALE**  
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1:200 @ A3

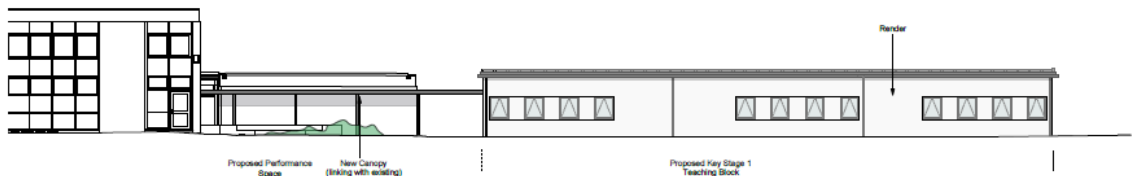
**PROJECT CONTENTS**  
Proposed KS2 Teaching Block Elevations

**REVISIONS**  
E03153-A-3005

**REVISIONS**  
P1



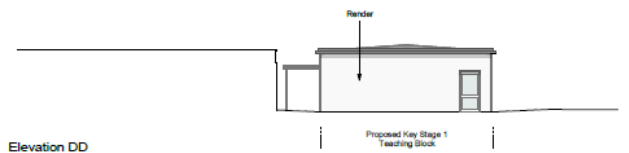
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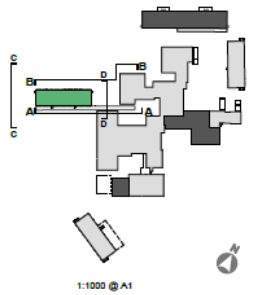
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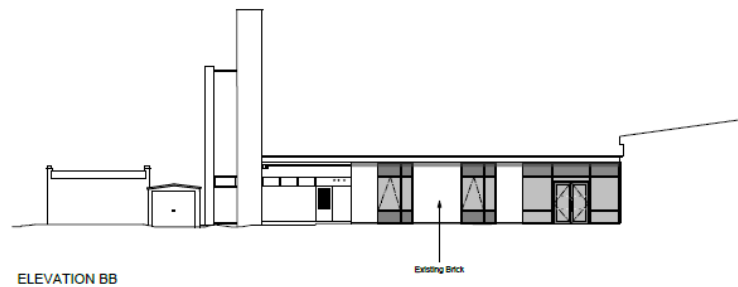
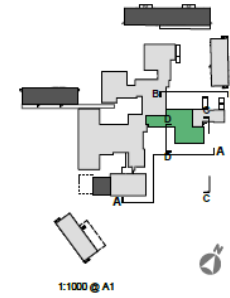
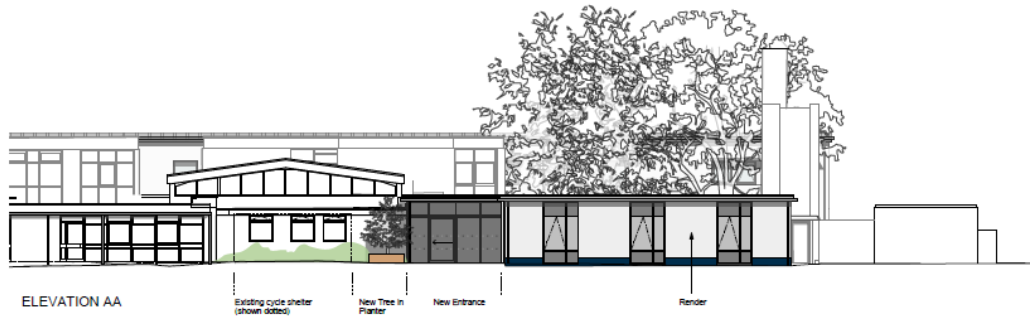
Elevation CC



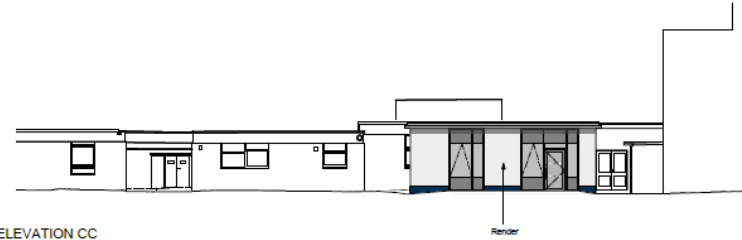
Elevation DD



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	Rev	Description	Date	By											
01	Issue for Planning	28/01/18	RS												
02	Issued for circulation	28/01/18	RS												



ELEVATION BB



ELEVATION CC



ELEVATION DD

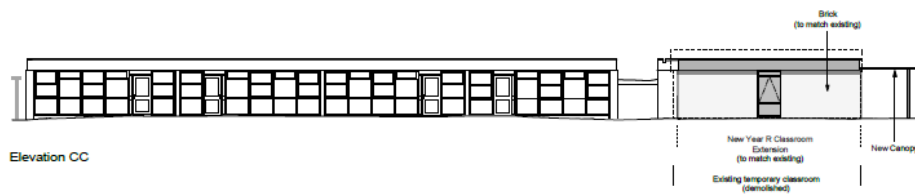
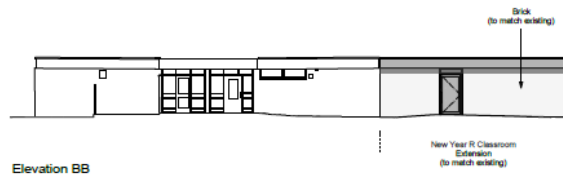
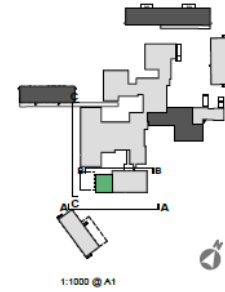
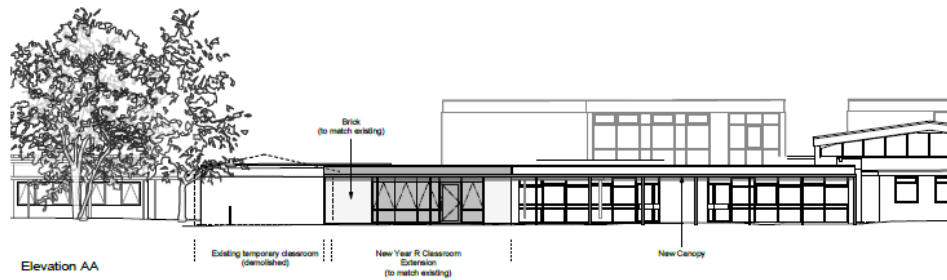
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No	Description	Date	By
1	Issue for Planning	28/01/18	RS

		HCC Property Services, Three Mirrors House, 25 High Street, Alton, Hampshire GU34 5JL, UK (01463) 847801	
<b>Proposed:</b> Moorlands Primary School Expansion to 3FE Church End Lane		<b>Project Code:</b> Proposed Entrance and Admin Elevations	
SCALE: 1:100 @ A1 1:200 @ A3	DATE: 28.01.18	DRAWN: RS	CHECKED: HD
OFFICIAL NO: <b>E03153-A-3001</b>		REVISION: <b>P1</b>	

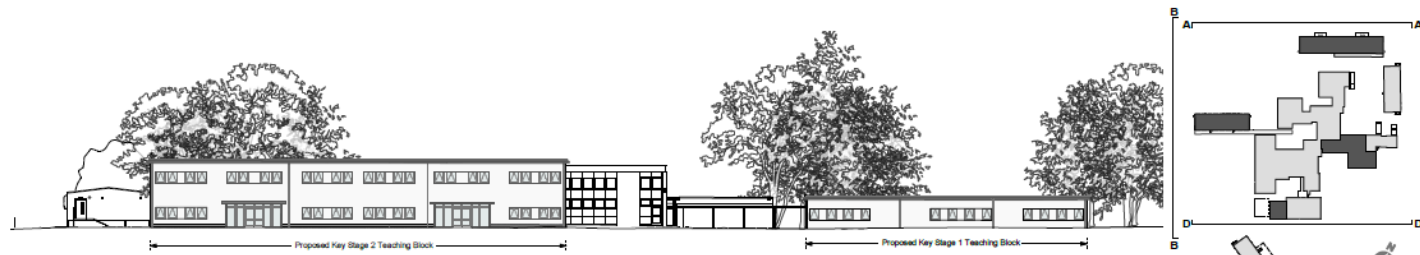


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2. This drawing is published from the drawing.			2018/01/18	RB
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No.	Description	Date	By
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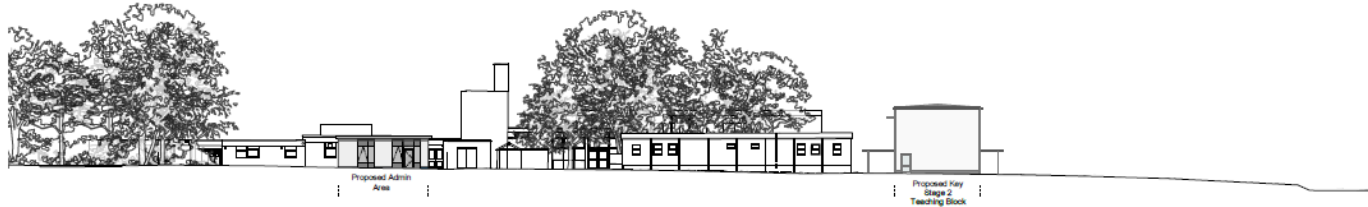
		HCC Property Services, Three Milnes House, 75 High Street, Winchester SO3 1RL, Wiltshire, UK (SO3) 01963	
<b>Moorlands Primary School</b> <b>Expansion to 3FE</b> <b>Church End Lane</b>		<b>PROPOSED</b> <b>Proposed Year R Classroom</b> <b>Elevations</b>	
SCALE 1:100 @ A1 1:200 @ A3	DATE 28.01.18	DRAWN RB	CHECKED HD
Drawn by <b>E03153-A-3003</b>			REVISION <b>P2</b>



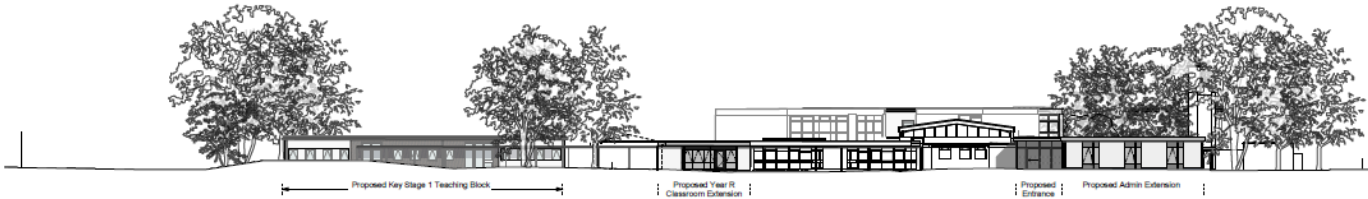
Proposed North-West Elevation (A-A)



Proposed South-West Elevation (B-B)



Proposed North-East Elevation (C-C)



Proposed South-East Elevation (D-D)

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No.	Description	Date	By
1	Issue for Planning	28.01.19	RB
2	Issue for Planning		
3	Issue for Planning		
4	Issue for Planning		
5	Issue for Planning		
6	Issue for Planning		
7	Issue for Planning		
8	Issue for Planning		
9	Issue for Planning		
10	Issue for Planning		

<p><b>Hampshire County Council</b></p>	<p>MOORLANDS PRIMARY SCHOOL Expansion to 3FE Church End Lane</p>		<p>HCC Projects Section, Three Moors House, 25 High Street, Winchester SO3 0AL, Tel: 01962 542000</p>	
	<p>PROJECT: Moorlands Primary School Expansion to 3FE Church End Lane</p>		<p>DRAWING CONTRACT: Proposed Site Elevations</p>	
<p>SCALE: 1:200 @ A1 1:400 @ A2</p>	<p>DATE: 28.01.19</p>	<p>DRAWN: RB</p>	<p>CHECK: HD</p>	<p>DRAWING NO: E03153-A-3006</p>
				<p>REVISION: P2</p>

**PLANNING**

**WHITLEY**





## COMMITTEE REPORT

BY THE DIRECTOR OF ENVIRONMENT AND NEIGHBOURHOOD SERVICES

READING BOROUGH COUNCIL

ITEM NO. 19

PLANNING APPLICATIONS COMMITTEE: 27<sup>th</sup> June 2018

Ward: Whitley

App No.: 180691

App Type: FUL

Address: Green Park Village

Proposal: A planning application for a 2 Form Entry Primary School, associated playing space, car parking, pedestrian and cycle routes, services & infrastructure, landscaping and other associated works.

Applicant: St. Edwards Homes Limited

Date valid: 25/4/18

Major Application: 13 week target decision date: 25/7/18

Planning Guarantee: 26 week date: 24<sup>th</sup> October 2018

### RECOMMENDATIONS

GRANT Planning Permission subject to conditions and informatives and subject to the satisfactory completion of a S.106 legal agreement.

Or

REFUSE permission should the legal agreement not be completed by 25<sup>th</sup> July 2018 unless a later date is agreed by the Head of Planning Development & Regulatory Services.

The Section 106 Legal Agreement to secure the following:

#### *Employment Skills and Training*

- Preparation of Construction skills ESP

#### *Transport*

- Drop-off/ pick-up bays on Flagstaff Road to be available for school related use during specific hours.
- Travel Plan

#### *Phasing Plan*

- Regarding the ongoing marketing suite use once the school has opened and the phasing of works to complete the school once the marketing suite goes in 2021.

#### *Transfer of School Site*

- Obligations of relevance from the original S106 (2011 for 10/01461/OUT - para 8.1-8.4) regarding transfer of the school site from the developer to the Council or nominee, no service charges to apply to the school, rights of way and access to the school, etc

#### *Community Use*

- Community use agreement to deal with users of hall, parking, pitches, hours, numbers etc

### CONDITIONS TO INCLUDE:

1. Time limit for commencement - 3 years
2. Approved Drawings and documents.
3. Materials to be submitted and approved.
4. DC2 Vehicle access provided in accordance with approved plans.
5. DC4 Vehicle parking plans to be approved.

6. Bicycle & scooter parking - submission and approval of plan, and provided prior to occupation.
7. Bin storage - submission and approval of plans and provided prior to occupation.
8. Car Parking Management Plan prior to occupation.
9. Roads serving the school to be provided prior to occupation.
10. CO2 Construction Method Statement/ Construction Environmental Management Plan to be submitted and approved prior to commencement of development (including demolition) including control of noise and dust.
11. DC10 Delivery and Servicing Plan to be submitted and approved prior to occupation and in accordance with approved thereafter.
12. The hours of noisy construction, demolition and associated deliveries.
13. Sound level of plant to be at least at 10db below the existing background sound level.
14. No bonfires
15. In accordance with FRA
16. Sustainable Drainage Scheme in accordance with approved details to be completed prior to occupation.
17. Whole life maintenance plan for drainage to be submitted to and approved by the Local Planning Authority prior to occupation
18. L2a Landscaping - Hard and soft landscaping details to be submitted to and approved in writing and implemented thereafter.
19. L2b Landscaping Implementation.
20. L3 Standard Landscaping Maintenance.
21. L5 Landscape Management Plan Details (for larger schemes) - to be carried out as approved.
22. No amplified sound or music shall be played within the school buildings outside specified hours
23. Control of noise from the sports pitches
24. Submission of detailing elevations of sports pitches includes details of fencing materials
25. Hours of use of the sports pitches
26. Hours of use of the hall
27. Detailed lighting scheme.
28. No floodlighting of sports pitches
29. (i) The development as built, shall meet a minimum of BREEAM Very Good standard with a minimum score of 62.5 points.  
(ii) No part of the development shall be occupied until a post-construction review demonstrating compliance with a minimum BREEAM Very Good score of 62.5 points has been submitted and approved by the LPA.
30. No development shall commence until details of the measures to be incorporated into the development to demonstrate how 'Secured by Design (SBD)' accreditation for schools will be achieved have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details, and shall not be occupied or used until the Council has acknowledged in writing that it has received written confirmation of SBD accreditation.

**INFORMATIVES TO INCLUDE:**

1. Terms and conditions.
1. Building control approval.
2. Pre-Commencement conditions.
3. Construction and demolition nuisance law.
4. S106
5. CIL
6. IF3 Highways i) The attention of the applicant is drawn to Section 59 of the Highways Act 1980, which enables the Highway Authority to recover expenses due to damage caused by extraordinary traffic; ii) Any works affecting the Highway shall be in accordance with Reading Borough's Council's document "Guidance Notes for Activities on the Public Highway within the Borough of Reading". The applicant should note that compliance with this document is mandatory and licences to work on the Highway will only be issued if the

requirements contained within it are met. A copy can be obtained from the Council's website.

7. EA advice - The Green Park area has been granted planning permission to raise the levels of the land, which we agreed at the time would result in the site effectively being in Flood Zone 1, and not Flood Zone 3 and Flood Zone 2 as currently shown on our Flood Map for Planning. This is as a result of a floodplain compensation scheme which involves extensive ground lowering and raising in the Green Park area, a flood storage area and a bypass channel, which over compensates for any built development proposals and allows for the wider development to go ahead without the need for further floodplain compensation
8. Royal Berkshire Fire and Rescue advice - Recommend fitting sprinklers as a requirement, for both the students and staff, to make economic sense and to protect the school and local community from needless damage.
9. Positive and proactive.

## 1. INTRODUCTION

- 1.1 The application area forms part of the overall Green Park Village Development (GPV) identified as Phase 2B. The site is 0.94ha, broadly square in shape, and is located at the corner of Longwater Avenue and what will be one of the main entrances to GPV via Flagstaff Road, currently under construction (which will be the road to access the approved Green Park Station). It is located opposite the site of the Extra Care Building, which is largely complete (approved under Phase 2A). To the north and east of the application area is the Phase 1 residential area, now almost complete, which comprises a mix of three storey houses and apartment blocks up to 4 storeys. To the west will be the site of new public realm 'Market Square'.
- 1.2 The application area is currently part vacant land and partly the site of the marketing suite and its parking area. Work has commenced on the following:
  - Phase 1 (97 houses, 11 apartments);
  - Phase 1C (road);
  - Phase 2A (129 Extra Care and 8 private apartments);
  - Phase 2C (30 lakeside apartments);
  - Phase 3A (68 houses, 6 apartments); and
  - Phase 5 (54 houses, 29 apartments).
- 1.3 The original outline permission for GPV (10/01561/OUT) provided for a one form entry school on this site. At the pre-application stage the Council advised the applicant that as the proposal was to be a two form entry school, it would require a new full application to be submitted, rather than a reserved matters application.
- 1.4 The area is within the Settlement Boundary and within Flood Risk Area 1, following the completion of engineering works to raise ground levels and remodel the surrounding hydrology, but not subject to any other specific environmental designations.
- 1.5 It should be noted that the original school formed Phase 2b of the overall Green Park Village. The overall area of this Phase included part of a swale and bank adjacent to Longwater Avenue. This area has been excluded from the application area for the school, as the applicant has advised that it is difficult for it to form part of the school grounds given its gradient. It would remain landscaped as part of the overall flood strategy.

## Layout of GPV at Outline Stage

School site



## Proposed School Site



- 1.6 Pre-application discussions have taken place since early 2016, and more recently to establish design and transport principles.
- 1.7 The application is being referred to Planning Application Committee as it is a major application.
- 2.0 **PROPOSAL AND SUPPORTING INFORMATION**
- 2.1 The application comprises the following:
  - Two storey building for a two form entry primary school of 2,264sqm, with a curved arrangement consisting of three main square accommodation blocks, connected by wedge shaped link sections. The school buildings would comprise:

- main hall and changing rooms; four key stage 1 classrooms; eight key stage 2 classrooms; two reception classes.; nursery accommodation; kitchen; staffroom, offices, stores, breakout areas etc.
  - Multi-use games areas
  - All-weather playing pitch
  - Other play areas
  - 20 car parking spaces (for staff) accessed from Main Street to the north (deliveries and refuse collection from the staff car park).
  - 7 parking bays for drop off and pick up, 4 of which will be available for school trip buses (on Flagstaff Road)
  - 30 cycle spaces (including scooter provision) for students and 20 for staff
- 2.2 The proposed scheme would be for 446 no. children - 26 nursery places and 60 per year group (2 classes) and 41 FTE staff. A breakfast and after school club are planned. It is anticipated that it will open in the summer of 2019.
- 2.3 At present there is a Marketing Suite and associated parking on part of the school site. The applicant has confirmed that as the school will be occupied incrementally, an artificial pitch will be provided which will meet the needs of the initial 2 years of intakes whilst the Marketing Suite remains. It is then intended in August 2021 following demolition of the Marketing Suite to extend the artificial pitch to its intended full size. This has been agreed in principle with all interested parties
- Community Infrastructure Levy (CIL)
- 2.4 The proposed scheme would not generate CIL as the Council's Charging Schedule includes for zero charge for this type of use.
- 2.5 The following plans and supporting documents were submitted and have been assessed:

*Received 25<sup>th</sup> April 2018:*

- Site Location Plan - Drawing no: 27310 A-2FE-01-200 Rev P2
- Masterplan - Proposed Block Plan - Drawing no: 27310 A-2FE-01-100 Rev P2
- Proposed Site Layout - Drawing no: 27310 A-2FE-02-100 Rev P3
- Proposed Ground Floor Plan - Drawing no: 27310 A-2FE-03-200 Rev P6
- Proposed First Floor Plan - Drawing no: 27310 A-2FE-03-201 Rev P6
- Proposed Roof Plan - Drawing no: 27310 A-2FE-03-202 Rev P3
- Proposed GA Sections Sheet 1 - Drawing no: 27310 A-2FE-04-200 Rev P4
- Proposed GA Sections Sheet 2 - Drawing no: 27310 A-2FE-04-201 Rev P1
- Proposed GA Elevations - Drawing no: 27310 A-2FE-05-200 Rev P2
- Drainage Schematic - Drawing no: 4160914-SK1200 Rev I2
- Proposed Utilities Layout - Drawing no: 4160914-SK1100 Rev I1
- School Car Share Bays - Drawing no: 4160914-SK05 Rev I3
- Staff Car Park Swept Path Assessment - Drawing no: 4160914-SK06 Rev I3
- Refuse Vehicle Swept Path Assessment - Drawing no: 4160914-SK07 Rev I3
- Delivery Vehicle Swept Path Assessment - Drawing no: 4160914-SK08 Rev I4
- Fire Tender Swept Path Assessment - Drawing no: 4160914-SK09 Rev I2

*Other Documentation:*

- Archaeological Mitigation Report, prepared by Oxford Archaeology
- Air Quality Statement, ref: 442984, prepared by RSK, dated 5<sup>th</sup> April 2018
- BREEAM Pre-Assessment Report, Rev D, prepared by Energist, dated February 2018
- CIL Planning Application Additional Information Requirements Form

- Contaminated Land Planning Statement for Phase 2B at Green Park Village, prepared by Ramboll
- Design and Access Statement, prepared by Broadway Malyan, dated March 2018
- Ecological Statement, Phase 2b, Green Park Village School, prepared by CSA Environmental
- Energy Statement, Rev B, prepared by Energist, dated 22<sup>nd</sup> March 2018
- Flood Risk Assessment [and associated figures and appnedices]Green Park Village School, Ref: HH4160914/KJ/012, Issue 2, prepared by Glanville, dated 29<sup>th</sup> March 2018
- Noise & Vibration Report, dated 14/3/2018, prepared by Energist UK
- Planning Statement, prepared by Nexus Planning, dated March 2018
- School Travel Plan, prepared by Glanville, Ref: HH4160914/DK/008, Issue 3: 29<sup>th</sup> March 2018
- Sunlight and Daylight Impact Assessment, Rev C, prepared by Energist
- Transport Statement, Ref; HH4160914/DK/005, Issue 3: 29March 2018, prepared by Glanville

### 3. PLANNING HISTORY

- 85/TP/690 - Business uses including light industrial, warehousing and ancillary offices together with associated service areas, roads, aprons and car parking areas. Land north of Foudry Brook. Approved 26/07/1995.
- 85/TP/691 - Business uses including light industrial, warehousing and ancillary offices together with associated service areas, roads, aprons and car parking areas. Land north of Foudry Brook. Approved 26/07/1995.
- 07/00572/SCO - Request for a Scoping Opinion in respect of development relating to approximately 17,000 sq m of B1 floorspace, 737 residential units and community facilities to include a one form entry primary school. Observations Sent 02/07/2007.
- 07/01275/OUT - A planning application for mixed-use development comprising: "Phase 1 (submitted in full with no matters reserved and as defined on Plan Ref. PA-P1-002): the construction of housing - 46 houses and 22 apartments (Class C3), local retail (Use Classes A1, A2, A3, A4, A5), management suite, village hall, engineering and infrastructure works including reconfiguration of the lake, lakeside access, car parking, pedestrian and cycle routes, services and infrastructure, landscaping and other associated works; and subsequent phases (submitted in outline with all matters reserved except for details of the main access proposals): the construction of housing - 669 dwellings (Class C3), extra care housing with ancillary community uses (Class C2), 16,000 square metres office space (Class B1), one-form entry primary school including nursery (Class D1), health surgery (Class D1), sports pitches, children's play facilities, engineering and infrastructure works including reconfiguration of the lake and vehicular access, lakeside access, car parking, pedestrian and cycle routes, services and infrastructure, landscaping and other associated works." Approved 31/03/2009.
- 10/00587/SCO - Request for a Scoping Opinion in respect of development relating to 730-750 new homes including an 80 unit Continuing Care Retirement Community scheme with extra care ancillary community facilities, 8 family homes for disabled persons, 16,000 sq m of use class B1 floor space, a One Form Entry Primary School with sports pitches for dual use with the local community, associated local centre, community and recreation facilities, a network of dedicated pedestrian and cycle routes and the provision of more than 8 hectares of open space, fully equipped children's play facilities and sports pitches, in addition to a 4 ha lake. Observations Sent 24/06/2010.
- 10/01461/OUT (102172) - A planning application for mixed-use development comprising: Phase 1 (submitted in full with no matters reserved and as defined in

area on Plan Ref. PL-P1-001) for the construction of housing (Class C3), local retail (Use Classes A1, A2, A3, A4, A5), management suite, village hall, engineering and infrastructure works including reconfiguration of the lake, lakeside access, car parking, pedestrian and cycle routes, services & infrastructure, landscaping and other associated works; and Subsequent phases (submitted in outline with all matters reserved except for details of the main access proposals) for the construction of housing (Class C3), extra care housing with ancillary community uses (Class C2), offices (Class B1), one-form entry primary school Class (Class D1), health surgery (Class D1), Nursery (Class D1), sports pitches, children's play facilities, engineering and infrastructure works including reconfiguration of the lake and vehicular access, lakeside access, car parking, pedestrian and cycle routes, services & infrastructure, landscaping and other associated works - Approved 1/7/2011

- 151068/FUL - Temporary Marketing Suite - Approved 3/3/16 - temporary until 3/3/2021, with condition that *"The following shall take place no later than 3 September 2021:*
  - i) The marketing suite building, access road and car park and all associated structures, hard surfacing and waste materials shall be removed from the site.*
  - ii) The ground to the front of units 40-44, forming part of detailed Phase 1 of permission 102172, shall be reinstated in accordance with the plans approved under permission ref. 102172.*
  - iii) All land forming part of the site that are located within the Phase 2a area of the overall Green Park Village scheme of permission 102172 shall be cleared and levelled."*

#### 4. CONSULTATIONS

##### (i) Statutory

###### *Environment Agency*

- 4.1 No objections subject to a condition regarding the scheme being undertaken in accordance with the FRA.

###### *West Berkshire*

- 4.2 The LPA does not wish to comment on this application.

- 4.3 However, the Education Department stated "The proposed school site is well within the Reading border and is unlikely to impact significantly on West Berkshire schools. We import pupils from across the Reading border, but this tends to be around Tilehurst and the schools are in close proximity to each other. I don't expect that the proposed school will impact on this cross border movement significantly as the school is not within Tilehurst and won't have those historic associations."

###### *Wokingham Borough Council*

- 4.4 No objection

##### (ii) Non-Statutory

###### *Berkshire Archaeology*

- 4.5 The proposals submitted under 180691 are accompanied by the Archaeological Mitigation Report (OA Nov 2016) which details the results of archaeological field work previously completed within the proposal area.

4.6 The results of this work demonstrated that there has been previous significant disturbance within the application area, with the archaeologically relevant deposits surviving only in one trench. In addition no archaeological features or finds were recorded within this trench.

4.7 On the basis of this work I can confirm that the archaeological investigations have been completed in the application area and there are no archaeological issues with these proposals.

*Berkshire Fire and Rescue*

4.8 There is a possible requirement for hydrant provision on this site, however until we are provided with a more detailed site plan we are unable to comment further. Access requirements for Fire Fighting are to meet the functional requirements the Building Regulations 1991 and the relevant provisions of the Berkshire Act. It should be noted that any gates required for emergency access should provide a minimum 3.1 m clear opening.

4.9 The layout plans provided have not been reviewed for fire safety provisions. This is the responsibility of your Buildings Regulations Department or Approved Inspector, in consultation with this Authority as part of a Building Regulations submission.

4.10 This Authorities overall strategic aim is to improve the safety of those who live work and travel in the county of Berkshire. School fires are a major national problem. Each year more than 2,000 schools in the UK suffer serious arson attack. Figures from insurers clearly show losses averaging almost £100m per annum over the last few years. Sprinklers are an effective way of preventing the spread of fire and experience has shown by those enlightened education departments who have fitted sprinklers that only minor disruption occurs if a fire is extinguished by a sprinkler system.

4.11 We would therefore recommend you to consider fitting sprinklers as a requirement, for both the students and staff, to make economic sense and to protect the school and local community from needless damage.

*Burghfield Parish Council*

4.12 No objection.

*Ecology- RBC*

4.13 There are unlikely to be any objections to this application on ecology grounds.

*Education - RBC*

4.14 The initial scheme outlined the requirement for a 1FE primary School, based on the Pupil Product Ratios of 0.3 per dwelling for Primary. In addition to this, other developments have been notified to RBC, such as Royal Elm Park Development which alone is anticipated to generate a further 1FE. Also in Green Park Village, a block originally intended to be office accommodation is now proposed to be residential accommodation. These developments would indicate the requirement for a second form of entry.

*Environmental Protection and Nuisance - RBC*

4.15 Noise generating development: A noise assessment has been submitted which identifies the noise limit which plant should meet in order to meet the Council's criteria at the nearest noise sensitive receptor. The applicants are not at a detailed enough design stage to be able to propose the exact plant to be installed. A noise condition is recommended.



- 4.16 *Air Quality*: The air quality assessment shows that air quality as a result of the development will remain below air quality objective limits and therefore no mitigation measures are required.
- 4.17 *Contaminated Land - high risk sites*: The contaminated land statement discusses the remediation that has already been carried out on the site and that the school site is now considered to be low risk and no further remedial works are required.
- 4.18 *Construction and demolition phases*: We have concerns about potential noise, dust and bonfires associated with the construction (and demolition) of the proposed development and possible adverse impact on nearby residents (and businesses). Fires during construction and demolition can impact on air quality and cause harm to residential amenity. Burning of waste on site could be considered to be harmful to the aims of environmental sustainability. I was unable to find the dust mitigation measures proposed which were referred to in the air quality assessment, so I have suggested the following conditions, which includes submission of proposed dust control strategies for during construction.
- 4.19 Conditions for Hours of construction and demolition working and no bonfires on site are recommended.
- Joint Emergency Planning Unit<sup>1</sup>*
- 4.20 I have reviewed this application with respect to AWE Burghfield and would advise that it is outside the DEPZ for the site and therefore I have no adverse comments to make.
- Leisure - RBC*
- 4.21 The proposed 2 Form Entry Primary School within Green Park is welcomed, particularly the provision of new sports, recreational and community facilities which will serve not only the residents of Green Park Village but the wider community within Reading. We therefore have no objections to the proposal.
- Natural Environment - RBC*
- 4.22 The site is at a main entrance to GP, leading to the western lake housing (Phase 6), the Extra Care Home (Phase 2B) and GP Station. It is therefore important that the frontage of the site provides the green frontage consistent with Phase 2A opposite. The layout shown on the approved Masterplan for 10/01461/OUT (included in the DAS) allowed for a buffer on the frontage between the road and school buildings. However the proposed layout now shown brings the school buildings much closer to the road frontage between the GP entrance and Market Square, not allowing for tree planting. The lack of softening on the frontage is exacerbated by the significant wide expanse of unbroken building.
- 4.23 The only softening is a small low level landscape bed hence the proposal is not consistent with other Phases which allow tree planting on the frontage. If the current layout is acceptable in planning terms, can the inclusion of a tree be considered in the 'front entrance palette' (upright form)?
- 4.24 In terms of tree species proposed, these are Oak, Hornbeam, Hazel and Hawthorn which are all native so in biodiversity terms are positive. However, the latter two are very small trees and will therefore provide no wider landscape value which is unfortunate given the limited number of trees on site.
- 4.25 The DAS mentions the importance of the boundary planting in providing a screen into and out of the school. However, the palette proposed is of hedges (Hawthorn

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<sup>1</sup> Bracknell Forest, Royal Borough of Windsor and Maidenhead and West Berkshire

and Hazel) and low level shrub planting. It is therefore not clear how the screening element will be provided.

- 4.26 The landscape strategy includes a potential habitat garden/area, which would have the potential to provide the ecology suggestions of ephemeral, invertebrate, bird and bat habitats (ref 5.6 of the application statement). However, whilst the picture palette shows a bug house, there is no mention of other biodiversity enhancements with emphasis being put on growing vegetables and herbs. Further details on this space should be provided (subject to ecology comments) and other landscape provision is required prior to a decision.

*Planning Officer Note:* The agent advised that amended information would be submitted with regard to trees on the frontage and the species proposed. Natural Environment responded further as follows:

- 4.27 The response to the lack of softening on the frontage is a very architect led response. 'Softening' by building design is very different to softening with planting which is important, the inclusion of which would be consistent with other Phases. It is stated that softening will be dealt with by increasing the number of trees shown, however none are currently shown on the frontage. It is further stated that '*If this can be accommodated within the frontage design we would seek to specify Acer platanoides or Tilia cordata to mirror Phase 2C opposite. We would note these are to be upright form*'. Can tree planting be accommodated? If it can be, it should be noted that the Tilia cordata and Acer platanoides proposed (as per Phase 2A - not 2C) are not upright forms, as is indicated. However, there are upright varieties of these species available.
- 4.28 It would be useful for landscaping to confirm at what height the hedge will be allowed to grow and thereafter a min maintenance height.
- 4.29 In relation to the 'Habitat garden', given its name and the ecology suggestions, we should expect some of this space to be used for biodiversity enhancements, Details can be agreed at a later stage.

*Planning Officer note:* Amended details were provided which showed more trees and identified relevant tree species. The Natural Environment Officer confirmed that these were an improvement and that the scheme was acceptable.

#### *ONR*

- 4.30 I have consulted with the emergency planners within West Berkshire Council, which is responsible for the preparation of the Burghfield off-site emergency plan required by the Radiation Emergency Preparedness and Public Information Regulations (REPPIR) 2001. They have provided adequate assurance that the proposed development can be accommodated within their off-site emergency planning arrangements.

The proposed development does not present a significant external hazard to the safety of the nuclear site. Therefore, ONR does not advise against this development.

#### *SUDS*

- 4.31 Response awaited. This will be reported in an update report.

#### *Thames Valley Police - Crime and Design*

- 4.32 No objections to the development in principle. However there are some aspects of the design that would require attention/clarification to create as safe a development as possible.

- It appears that the main entrance provides access into a corridor, I would recommend the inclusion of a secured lobby where the identity of visitors can be verified before allowing them through to visit staff or attend the interview room.
- Boundary treatment and access control gates

4.33 In addition, I note that there is limited information within Design and Access Statement relating 'Safety & Security'. Therefore, opportunities to promote community safety remain. To ensure that these opportunities are not missed I request that a condition be included [achieving Secured by Design accreditation for schools]. I feel that attachment of this condition would help the development to meet the requirements of: The National Planning Policy Framework 2012 (Part 7, Sect 58; 'Requiring good Design' and Part 8, Sect 69; Promoting Healthy Communities') where it is stated that development should create 'Safe and accessible environments where crime and disorder, and the fear of crime do not undermine quality of life or community cohesion', and Reading Core Strategy Policy CS7: Design and the Public Realm and Planning policy RC5

*Thames Water*

4.34 We're writing to tell you that we won't be pursuing a build over agreement for 180691 Green Park Village Longwater Avenue. Reading. Although the area itself has no mapped sewers, it is very likely not to have a public sewer within 3m. If this is incorrect and the site owner finds there is a public sewer within three metres of their building work, or a lateral drain within one metre, they must make sure they comply with our specifications. These can be found at [www.developers.thameswater.co.uk/domestic-and-small-commercial/building-near-pipes/building-over-or-near-a-sewer/getting-approval](http://www.developers.thameswater.co.uk/domestic-and-small-commercial/building-near-pipes/building-over-or-near-a-sewer/getting-approval).

*Transport - RBC*

4.35 The site established outline consent under planning consent number 10/01461/OUT in July 2011 for a mixed-use development consisting of dwellings, extra care housing, offices, local retails, one form entry primary school, nursery, community facilities and associated infrastructure works.

4.36 The site is located on the northern side of the junction of Longwater Avenue and Flagstaff Road (formerly known as Station Road), and opposite the Care Home currently under construction. It is situated 450m southeast of the proposed Green Park railway station and approximately 120m from bus stops on Longwater Avenue which form part of the Greenwave bus route.

4.37 The initial scheme outlined the requirement for a 1FE primary School to serve pupils living within the Green Park Village development. Since the outline application was approved in 2011, Reading Borough Council has requested that the school is expanded to a two-form entry establishment to accommodate the additional pupils from other developments coming forward such as Royal Elm Park Development.

4.38 The number of pupils who live within Green Park Village has been calculated using factors provided by RBC. The projected pupil numbers identified indicates that is likely to be a total of 417 pupils who live within the Green Park Village. This equates to 93% of the total 446 student places within the proposed school. It is anticipated that there will be a total of 41 FTE staff members.

*Staff Parking*

4.39 RBC parking standards are based on a zonal system depending on where in the Borough the site is located. The proposed Site falls within Zone 3 which is classed as a 'Secondary Core Area'. The car parking standards are expressed as maximums

and for Land Use Class D1: Primary Education is based on 1 car parking space per 1FTE staff and 1 drop-off space per 10 pupils.

- 4.40 Based on the proposed staff numbers, the development would be required to a total of 41 staff parking spaces. However, the site cannot accommodate this level of parking and a total of 20 staff parking spaces will be provided.
- 4.41 Whilst it is reasonable to assume that a couple of the teachers will also live within the Green Park Village development, it is evident that proposed parking provision is below the requirements of the current parking standards (once fully occupied). The application will therefore be accompanied by a Travel Plan to minimise car use and encourage sustainable travel to the site.
- 4.42 The school will initially fill the nursery and the reception classes with the number of staff and students increasing year by year as they progress through the school. Therefore, the school will need to appropriately manage the car parking as the school grows and implement travel plan measures such as car sharing, public transport incentives to ensure the school can operate efficiently once it is fully occupied. The Travel Plan measures are discussed later in this report.
- 4.43 The Transport Statement confirms that the parking on surrounding roads will be strictly managed to reduce the potential for parking to overflow onto residential roads, as identified within the S106 agreement for the original application. It is anticipated that this will be enforced by the Green Park Village parking enforcement team which currently provide parking management for the residential areas.
- 4.44 The staff car park will be situated to the rear of the school site and accessed via a 4.8m wide access road which connects to Champlain Street. Vehicular access to the staff car park will be restricted to staff vehicles at all times, no pupil or parent access will be allowed even during the breakfast and afterschool clubs.
- 4.45 In terms of layout, the staff car parking spaces do comply to the standard dimensions of 2.5m x 5m but there is not the full 6m turning space to the front of all spaces. The Transport Statement indicates that the school should consider managing the parking by ordering the way in which the bays should be filled up so that each space can be fully utilised. This seems unnecessary complicated and could be resolved by shifting the spaces south towards the sports pitch. This would ensure that the full 6m turning space is available for all spaces.

#### *Pupil Drop off*

- 4.46 The Council's parking standards state that a maximum 1 drop-off space per 10 pupils are required for primary schools. For a 446 pupil primary school this equates to a maximum 44 drop-off spaces.
- 4.47 The proposed Primary School at Green Park Village was originally planned to be a one-form entry school and no pupil drop off parking provision was included within the original development masterplan. Therefore, drop off/collection spaces in accordance with the Council's standards is unachievable.
- 4.48 The Transport Statement indicates that 7 parking bays will be provided on Flagstaff Road adjacent to the Market Square. During school peak hours, these bays will be restricted for school car sharing and for drop-off / pick-up activity with appropriate signage setting out the parking restrictions. It is anticipated that these bays will be managed and enforced by the Green Park Village parking enforcement company. However, these bays were originally approved as part of the Market Square

development to provide a loading bay for the retail units within Phase 2C. Condition 2 of planning permission 162050 states the following;  
*“The loading bay as shown on the approved plan, and as referred to in the amended Design and Access Statement is to be provided and ready for use prior to the occupation of the retail units and apartments.*

*Reason: To ensure that the retail units have an off-road loading bay to minimise stopping on the highway in accordance with Adopted Sites and Detailed Policies Document (2012, revised 2015) Policy DM12: Access, Traffic and Highway-related Matters.”*

- 4.49 Therefore, it is clear that these bays were specifically for servicing/delivery of the retail units and apartments. It is not clear how the mixed uses of these bays will operate together given that the demand for these bays are likely to coincide with each other. The applicant is requested to address this concern.
- 4.50 In accordance with the projected pupil numbers set out within Table 2, it is anticipated that 93% or 417 pupils will live within Green Park Village whereby pupils will be encouraged to walk/cycle to school. However, it is acknowledged that initially the number of children travelling from outside the Green Park Village development might outnumber those from the adjacent properties. This is due to the school opening before families start moving into the neighbouring development.
- 4.51 I am concerned that the development does not provide adequate drop off spaces for the size of the school which will lead to on-street parking on Flagstaff Road which is main vehicular route for buses and cars to Green Park Station.
- 4.52 As previously stated, the school will initially fill the nursery and the reception classes with the number of staff and students increasing year by year as they progress through the school. It is understood that the school will be willing to consider staggering the end times of the school day according to school year to minimise any potential impact on the local highway network. This will be considered as the school grows and the actual impact on the highway network is identified but there is no firm commitment to do this. The Travel Plan should therefore commit to annual pupil/parent surveys to establish the demand for these spaces to identify a trigger point for reviewing school hours.
- 4.53 In line with a recent school application within Reading, the school should also commit to operating a managed drop-off/collection area for those parents happy to have their children quickly disembarked by staff/volunteers. A managed collection/drop-off arrangement allows a reduced parking provision for the pick-up / collection from the maximum standards as it will keep vehicles moving and remove the need for them to park for longer periods. This will help reduce any demand for on street parking on the surrounding highway network and improve the efficiency of the drop off spaces.

*Community use*

- 4.54 The school hall and sports pitch may be used for community events. It is considered that the main use of the school hall would potentially be used by Green Park Village residents for village events and meetings. The sports pitch will not be a full size pitch as it will cater for 5-a-side matches and will not be floodlit. Consequently, it would only potentially be used for local leagues or for residents of Green Park Village outside of peak hours or at weekends.
- 4.55 Any community use of the proposed school and sports pitch will be outside of normal school hours when there is no demand for staff parking. However, the

applicant is requested to confirm that staff parking area will be available for community use outside of school hours to ensure sufficient car parking is provided.

*Servicing and Deliveries*

- 4.56 It is proposed that all servicing and delivery associated with the school will occur to the rear of the building within the staff car park. Delivery vehicles will access the site via Longwater Avenue, Maine Street and Champlain Street.
- 4.57 A tracking diagram has been provided illustrating that a refuse vehicle and fire tender can manoeuvre around the staff car park. A swept path assessment showing a 10m Rigid heavy goods vehicle accessing the car park has also been undertaken. In order to ensure refuse vehicles and goods vehicles can easily access the staff car park, the management company must ensure that no on-street parking occurs on the access road. The applicant has confirmed that on-street parking will be managed and enforced by the Green Park Village parking enforcement company.

*Traffic Generation*

- 4.58 It was agreed at pre-application stage that the morning peak hour would have the worst impact on the highway network as it would occur at the same time as office workers travelling to Green Park.
- 4.59 It is anticipated that 93% or 417 pupils will live within Green Park Village upon full occupation of the school. It was agreed at pre-application stage that these trips would not be assessed given that the trips would be coming from within the Green Park Village development and pupils would be encouraged to walk, scoot and cycle to and from the school site through the Travel Plan measures. It should be noted that no pupil vehicular trips were included within the assessment in original outline permission.
- 4.60 In order to accurately calculate the number of vehicle trips from pupils outside of the Green Park area, it has been assumed that some families will have more than one child at the school. It has also been assumed that 10% of pupils will attend both the Breakfast Club and the After School club. Given this, it is anticipated that the pupils will result in a marginal increase in vehicular trips in the am peak.
- 4.61 Paragraph 5.31 of the Transport Statement states that due to the proximity of the sustainable modes of travel, and on the basis that a strong School Travel Plan will be implemented, it is considered that the staff modal split would be as follows:

*Table 9: Staff Modal Splits*

Mode	Proportion	Number of staff
Car Driver	25%	10
Car Share	30%	12
Walking	5%	2
Cycling	5%	2
Train	10%	4
Bus	25%	10

- 4.62 The original school application identified that fifteen out of the eighteen staff drove to the site during the AM highway peak hour of 08:00 to 09:00. However, the proposed staff modal splits within Table 9 of the Transport Statement significantly reduce the number of single occupancy vehicle trips to site given the limited parking on-site.

- 4.63 When compared to the one-form entry school, the assessment determines that the staff vehicular trips will increase before 8am but will decrease in period between 8am-9am, despite a doubling in staff numbers. I find it unrealistic to assume that that staff vehicular trips will reduce in the period between 8am and 9am. However, the limited parking within the site will significantly influence mode of travel by staff and the development is unlikely to significantly increase the staff vehicular trips during the am peak hour above the permitted use.
- 4.64 The combined staff and pupil vehicle trips from the proposed two-form entry school development will result in a net increase in trips when compared to the former school application. However, it is considered that the junctions within Green Park would not experience a significant adverse impact on their operation as a result of the increased flows and is acceptable.

*Cycle Parking*

- 4.65 The RBC cycle parking standards are minimum standards and are applicable irrespective of zoning. RBC's SPD document states that cycle parking should be provided for primary schools at a ratio of 1 space per 5 FTE staff for staff cycling and 1 space per 15 pupils for Years 1 to 3 and 1 space per 10 pupils for Years 4 to 6. This equates to a minimum cycle parking provision of 9 spaces for staff and a minimum 38 spaces for the proposed maximum roll of 446 pupils.
- 4.66 To encourage both staff and students to either cycle or use their scooters, 40 student cycle parking spaces will be provided and a further 10 cycle parking spaces will be provided for staff. It is proposed that these cycle spaces will take the form of Sheffield cycle stands and will be located adjacent to the car park. We will however require full details to demonstrate that this cycle parking is covered with a 1m spacing between the stands but I am happy for this to be dealt with by way of a condition.

*Travel Plan*

- 4.67 A Framework Travel Plan has been submitted and is deemed appropriate as the travel patterns will not be established until the building is occupied. Following the opening of the school, it is proposed that a hands-up Travel Survey will be undertaken by pupils to identify the baseline pupil modal split. Teachers and parents will be asked to complete an online survey. A full travel plan will therefore need to be submitted within 6 months of occupation (within the second term of the first academic year).
- 4.68 The Action Plan within Section 8 sets out the Measures & Initiatives of the Travel Plan. I have however reviewed the detail within the plan and I comment as follows:
- Prior to the opening of the new school in the summer of 2019, a Travel Plan Co-ordinator (TPC) will be appointed. This is acceptable.
  - Pupil Information sheets and home school agreements highlighting travel options and parking restrictions/enforcement will be undertaken prior to the school opening. This is acceptable.
  - For clarification travel surveys must be undertaken with 6 months (clear timescale opposed to second term). Travel surveys must be repeated annually to ensure that the modal splits accurately reflect the expansion. The monitoring process should commence 12 months following approval of the initial Travel Plan.
  - Promotion of sustainable travel for staff should be prior to school opening. The TPC should offer all staff members a form of personalised travel planning in their induction outlining the possible modal options on offer to

them from their home location. This will encourage sustainable travel options from the offset to reduce the reliance on driving to the site.

- The TPC will actively promote car-sharing. The initial travel survey should also determine staff members' willingness to car share to identify suitable car sharers.
- Public transport incentives should also be investigated and promoted to staff, this could be in the form of discounted travel.
- The Green Park Village walking Bus should be set up and trialled within 3 months of opening. This should be reviewed annually to determine whether demand has increased.
- In line with recent school application within Reading, the school should also commit to trailing a managed drop-off/collection area for those parents happy to have their children quickly disembarked by staff/volunteers to relieve pressure on parking.
- The Travel Plan should commit to annual pupil/parent surveys the establish problems associated with pupil drop off/collection to identify a trigger point for reviewing school hours.

### (iii) Public Consultation

4.69 A number of site notices were displayed and a notice was put in the local paper.

4.70 One letter of support was received as follows:

We fully support the application. The residents of Green Park Village have to travel a lot for a school. This school will help the Green Park Village and will reduce the traffic on A33.

## 5. RELEVANT PLANNING POLICY AND GUIDANCE

5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that proposals be determined in accordance with the development plan unless material considerations indicate otherwise. Material considerations include relevant policies in the National Planning Policy framework (NPPF) - among them the 'presumption in favour of sustainable development'.

5.2 The following national and local planning policy and guidance is relevant to this application:

### *National*

National Planning Policy Framework (NPPF)

National Planning Policy Guidance

### Reading Borough Local Development Framework - Adopted Core Strategy (2008, altered 2015)

CS1: Sustainable Construction and Design

CS2: Waste Minimisation

CS3: Social Inclusion and Diversity

CS4: Accessibility and Intensity of Development

CS5: Inclusive Access

CS6: Settlement Boundary

CS7: Design and the Public Realm

CS9: Infrastructure, Services, Resources and Amenities

CS20: Implementation of Reading Transport Strategy

CS22: Transport Assessments



CS23: Sustainable Travel and Travel Plans  
CS24: Car / Cycle parking  
CS29: Provision of Open Space  
CS30: Access to Open Space  
CS34: Pollution and Water Resources  
CS35: Flooding  
CS36: Biodiversity and Geology  
CS38: Trees, Hedges and Woodland

Reading Borough Local Development Framework - Sites and Detailed Policies Document (2012, altered 2015)

SD1: Presumption in Favour of Sustainable Development  
DM1: Adaptation to Climate Change  
DM3: Infrastructure Planning  
DM4: Safeguarding Amenity  
DM12: Access, Traffic and Highway-related Matters  
DM16: Provision of Open Space  
DM18: Tree Planting  
DM19: Air Quality

SA1: South Reading Development Principles  
SA2: South Reading Strategic Development Sites  
SA11: Settlement Boundary

Emerging RBC Local Plan - Submission Draft Reading Borough Local Plan, March 2018

CC1: Presumption in Favour of Sustainable Development  
CC9: Securing Infrastructure  
TR3: Access, Traffic and Highway-Related Matters  
TR4: Cycle Routes and Facilities  
TR5: Car and Cycle Parking and Electric Vehicle Charging  
OU1: New and Existing Community Facilities

Supplementary Planning Documents

Revised Parking Standards and Design (Oct 2011)  
Employment, Skills and Training (2013)  
Planning Obligations under S106 (April 2015)  
Sustainable Design and Construction (July 2011)  
South West Reading Planning Brief (April 2000)

Other Guidance Documents

- Site Layout Planning for Daylight and Sunlight: A Guide to Good Practice Second Edition, by Paul Littlefair BRE, 2011
- BS8206 - Part 2: 2008 Code of Practice for Daylighting
- Guidance Notes for the Reduction of Obtrusive Light, Institution of Lighting Professionals (2012)
- Acoustic Design of Schools; Performance Standards, Building Bulletin 93, DFE & EFA (February 2015)

6. APPRAISAL

(i) Principle of Development - Location

- 6.1 Paragraph 34 of the NPPF requires developments to be sited in sustainable locations and the Core Strategy identifies that Green Park is such. Green Park Village (referred to as Green Park 3 at that time) was identified as one of four

sustainable locations for future development at that time. This will rely on the continued high level of public transport accessibility, as described below.

6.2 The site is well served by buses, is close to the proposed South MRT, existing and proposed cycling routes which connect to the wider Reading cycle network and an approved railway station. There is a good network of existing and proposed footpaths within GPV, Green Park and beyond.

6.3 The proposed development benefits from a high level of accessibility by a range of sustainable transport modes in accordance with paragraph 17 of the NPPF.

(ii) Principle of development - Use

6.4 The principle of a primary school was established through the outline planning permission (10/01561/OUT) which included a one-form entry school. There is an ongoing need for primary provision specifically within this part of the Borough and during pre-application discussions RBC Education advised the applicant that a two form entry school would meet wider educational needs. Since the original outline permission there has been a range of residential permissions within the local area including the pending issuing of the decision for further residential within GPV within Phase 6, all of which add to the potential primary age pupils in the area.

6.5 The NPPF identifies that in terms of the social role of sustainable development it should support *“strong, vibrant and healthy communities, .....with accessible local services that reflect the community’s needs and support its health, social and cultural wellbeing”*. It places great importance on ensuring that there is a sufficient choice of school places available to meet the needs of existing and new communities (para 72).

6.6 The adopted Core Strategy includes as one of its key social and economic objectives to *“maximise access for all to the necessary education, skills and knowledge to play a full role in society”*. Policy CS3 addresses the need for major developments to *“... demonstrate measures to enhance social inclusion in terms of access to housing, employment, services, community facilities, leisure, health, education, and other services and facilities”* and that *“...All members of the population should be provided with access to good quality health, education and other social facilities...”*. Policy CS31 supports new education development especially where this involves the co-location of facilities on a single site. The overall proposal includes for a hall which will as well as being for school use will be available for the wider community. A condition is recommended requiring the submission and approval of a community use agreement, which would include the use of the hall and the sports pitches.

6.7 Para 13.1.4 of the Sites and Detailed Policies Document (SDPD) states that *“Significant shortfalls have been identified within South Reading in relation to the provision and accessibility to, and quality of, various community facilities including public meeting venues, open spaces, allotments and other recreational areas. Future significant shortfalls in primary and secondary school reception places are also predicted and there is an ongoing need to provide healthcare and other community, leisure and social facilities to serve an expanding population.”* Para 13.5.5 goes on to refer to the shortfalls in various community facilities, in particular within the education sector, which add to the deprivation within the local wards. The provision of a school would contribute to meeting this need.

6.8 The emerging RBC local plan identifies the need for new education provision at both primary and secondary and Policy CC9 gives the highest priority to securing education facilities.

6.9 The proposed use would also have economic benefits with regard to the creation of jobs during the construction period and, once occupied, for a proposed 41 new jobs (full time equivalent).

(iii) Transport and Accessibility

6.10 The submitted Transport Statement, the scope of which was agreed with RBC officers in March 2017, sets out that the proposed scheme would be for a total 446 no. pupils (once fully occupied). This would comprise 26 FTE nursery spaces and two forms of entry (max 30 per form) for each year group from reception to year 6.

6.11 It has been calculated, based on the approved outline scheme for GPV, that there would be a pupil yield from GPV of 417 (=93%) with the remainder from outside the development. The Statement identifies that it is not anticipated that many families would live in the Phase 6A apartments and would therefore not generate a significant demand for school places, however if these were included than 100% of anticipated school pupils would be within GPV.

6.12 There would be a total of 41FTE staff members, with an assumption that a couple of these would live within GPV. The proposed parking provision is below the requirements of the current parking standards (once fully occupied) (41 requirement against 20 proposed). A Travel Plan will therefore be required to minimise car use and encourage sustainable travel to the site. This is included as a recommended S106 obligation.

6.13 Pedestrian access to the school would be via the main entrance which would face directly on Market Square, accessible from Flagstaff Road and the wider network of roads within GPV. An additional pedestrian gate would be provided at the south-eastern corner adjacent to the Longwater Avenue/Flagstaff Road roundabout.

6.14 Vehicular access to the car park would be restricted to staff at all times. Transport has advised that although the layout of the spaces would comply with standards, the full 6m turning space could not be achieved. The applicant's proposal of managing the car park, so that each space could be fully utilised, e.g. a specific order that the spaces should be filled up, appears unnecessarily complicated. An amended layout could be achieved, which removes this conflict, by shifting the spaces south towards the sports pitch. An amended layout has been requested and will be reported in an update.

6.15 The staff car park is also proposed to be used for servicing, delivery and refuse vehicles. Relevant swept path analyses have been included and Transport has confirmed that the arrangement would be acceptable. However, a condition is recommended for the submission and approval of a delivery and servicing plan.

6.16 The scheme includes for 50 no. cycle parking spaces (40 for pupils (20 stands) and 10 for staff) including scooter spaces, which would meet Council standards. A condition is recommended for the submission and approval of the layout of the cycle storage area intended to be adjacent to the car park.

6.17 Although the Transport Statement sets out the assumption that most pupils would walk or come using other sustainable modes, there is also the provision of seven drop-off/ pick up bays on Flagstaff Road, also to be available for visitors (the latter also with access to unallocated Market Square parking). Four of these bays would

be reserved for a school bus at times for school outings. The Transport Statement identifies that these would be controlled (as a private road) through appropriate signage and enforcement. However, these bays were originally approved as part of the Market Square development to provide a loading bay for the retail units within Phase 2C. Condition 2 of planning permission 162050 states the following;

*“The loading bay as shown on the approved plan, and as referred to in the amended Design and Access Statement is to be provided and ready for use prior to the occupation of the retail units and apartments.*

*Reason: To ensure that the retail units have an off-road loading bay to minimise stopping on the highway in accordance with Adopted Sites and Detailed Policies Document (2012, revised 2015) Policy DM12: Access, Traffic and Highway-related Matters.”*

- 6.18 Therefore, it is clear that these bays were specifically for servicing/delivery of the retail units and apartments. It is not clear how the mixed uses of these bays would operate together given that the demand for these bays would be likely to coincide with each other. The applicant is requested to address this concern. This will be reported in an update.
- 6.19 It is recommended, however, that the provision of these and the specific hours and for what use are defined within a S106 obligation, to ensure that the bays are retained for school use during relevant hours.
- 6.20 Notwithstanding the recommended obligation for the bays Transport is concerned that the development does not provide adequate drop-off spaces for the size of the school and that this would lead to on-street parking on Flagstaff Road which will be the main vehicular route for buses and cars to Green Park Station. It is understood that the school would be willing to consider staggering the end times of the school day according to school year to minimise any potential impact on the local highway network, but there is no firm commitment to do this. It is recommended, therefore, that the Travel Plan should therefore commit to annual pupil/parent surveys to establish the demand for these spaces to identify a trigger point for reviewing school hours. In addition, in line with a recent school application within the Borough, the school should also commit to operating a managed drop-off/collection area for those parents happy to have their children quickly disembarked by staff/volunteers. This would allow a reduced parking provision, from standards, as it would keep vehicles moving and remove the need for them to park for longer periods. This in turn would help reduce any demand for on street parking on the surrounding highway network and improve the efficiency of the drop off spaces.
- 6.21 Transport has confirmed that the combined staff and pupil vehicle trips from the proposed two-form entry school development would result in a net increase in trips when compared to the former school application. However, it is considered that the junctions within Green Park would not experience a significant adverse impact on their operation as a result of the increased flows and is acceptable.
- 6.22 The sports pitches and hall are proposed for community use. The submitted Transport Statement includes limited details regarding the anticipated associated transport/ accessibility requirements. The Transport Statement refers in para 4.33 *“for events which take place at the evenings or weekends the school should liaise with the Business Park to identify whether the nearby office car parks could be utilised when not being used by office staff.”* A similar suggestion is made with regard to visitor parking for events at the school. There needs to be clear details regarding the number and hours of use and relevant proposed parking and other

transport provision. As any community use of the proposed school hall and sports pitches would be outside of normal school hours, when officers assume there would be no demand for staff parking. However, confirmation has been sought from the applicant that the staff parking area would be available for community use outside of school hours to ensure sufficient car parking is provided. This will be reported in an update. As stated above a recommended obligation is included within the S106 regarding the submission and approval of a community use agreement, which would include hours of use, numbers of users etc.

- 6.23 The emerging revised Local Plan Policy TR5, states that 10% of spaces should be provided with an active charging point, which would equate to 2 spaces. 1 charging point (2 spaces) is proposed for this scheme.
- 6.24 Bin stores would be provided. A condition is recommended for the submission and approval of further details.
- 6.25 Transport has confirmed that this is a sustainable location and that, subject to conditions and obligations, included in the recommendation above, the proposed scheme would accord with national and local policies CS22, CS23, CS24, and DM12 and emerging policies TR1, TR3, TR4 and TR5.
- (iv) Design
- 6.26 The site location and area for the proposed school was fixed by the outline permission, and so the challenge has been to achieve a suitable design and layout for a two form, as opposed to a one-form entry school. Originally the one form entry school needed to have a specification which accorded with clause 3.1 of the S106 legal agreement, which states *"the specification of the school is to accord with the DfES guidance BB99 (Building Bulletin 99. Briefing Framework for Primary School Projects) current at the date of the agreement"*. The proposed scheme has been developed based on Building Bulletin 103 (current version) and is in line with Education Funding and Skills Agency (EFSA) guidelines.
- 6.27 The DAS explains that the design development took place from 2015 onwards and in August 2017 it was agreed with RBC that a two form entry would be preferable on the site. Prior to submission of the application, work continued on refining the design including internal layouts and the external envelope.
- 6.28 The building is comprised of three square blocks linked by two wedge shape structures, following a curve, facing not Flagstaff Road, the access road to the approved Green Park Station. At the end nearest Market Square is the proposed hall for the school and community access, and this is separated from the teaching blocks and ancillary accommodation by a school entrance. The overall building is two storeys in height and the gross internal area is based on Building Bulletin 103 and is in line with the ESFA guidelines.
- 6.29 To the rear of the school is a proposed MUGA -single netball size; all weather pitch Sport England standard for under 11/12 year olds; and informal play area (hard landscaping) within the constraints of the originally planned 1FE site.



- 6.30 Design is a key element of national planning policy and para 56 of the NPPF states that *“The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.”*
- 6.31 At the local level Core Strategy Policy CS7 states that all developments must be of high quality design that maintain and enhance the character and appearance of the area of Reading in which it is located. The submitted DAS includes an assessment of the proposals under the headings of use, amount, layout, scale, landscape, appearance and access.
- 6.32 The following will examine different components of the overall design.

#### *Layout*

- 6.33 The series of connected buildings are in a curved arrangement, located adjacent to Market Square, and these would positively contribute to defining that public space, by formally providing an edge to it, enclosure, and contributing to an active frontage. The section which would front the square would be the school hall, which would also be accessible to the community. The main entrance to the school and teaching blocks beyond would provide a continuous active frontage to Flagstaff Road to its junction with Longwater Avenue (opposite Green Park Business Park).
- 6.34 The external landscaped areas would be mainly to the north of the school (rear) acting as a buffer to the residential area to the north-east and north-west of the site.
- 6.35 During pre-application discussions officers queried the location of the nursery outdoor space being located to the front of the building, adjacent to the road and requested that the rationale for this would need to be clearly explained. The applicant has identified that alternative locations were tested, however, the proposed location was felt to be the best location, as it provided contiguous play space for nursery/reception separate to that of the main school (alternative locations would fragment the play space for older children on a constrained site), it would enable parental access for am/pm sessions of nursery and reception classes

to function without disruption to the running of the rest of the school. The enclosure to play space provides safe containment of the nursery/reception area.

- 6.36 In terms of the internal layout at the ground and first floor the northernmost block accommodates the hall (double height), kitchen and changing rooms; the central block the reception, offices, nursery and KS1 classrooms to the rear at ground floor and KS2 classrooms at first floor; and the southernmost block reception classrooms to the front and KS1 classrooms to the rear at ground floor and KS2 classrooms at first floor.

*Scale*

- 6.37 The proposed school would be well within the original maximum height parameter for the school as defined at outline stage, i.e. 7.9m compared to an approved maximum of 13m. The surrounding buildings range up to 6 storeys (extra care) and therefore the overall scale and massing would be appropriate in the context.

*Appearance*

- 6.38 The school is proposed to be principally constructed using the same buff multi-stock brick as the surrounding residential apartments. It is proposed that the entrance would have a series of different colour vertical fins with the green fin carried through into the atrium space to emphasise the route into the building, along with a double height glazed corridor. A green totem bearing the school name is also proposed to act as a visual landmark.
- 6.39 The façade of the community hall is proposed to be a combination of strong buff brick surround with bronze look clad facade and glazing. Vertical fins of the same material as the cladding would give emphasis to the height of the hall and provide a civic façade to the Market Square.
- 6.40 A different composite cladding is proposed for the curved connecting blocks with the coloured vertical metal shading fins to distinguish them from the other buildings and to provide a 'fun' element to the elevations, intended to appeal to primary aged children.
- 6.41 The teaching blocks would be similar in appearance to the hall, but simpler i.e. without the vertical fins.



## Illustrative Images to Market Square and the school entrance



View looking North-West across Market Square



View looking North towards main entrance



View looking North-West across Market Square with the school on the right hand side

### *Access*

- 6.42 There would be level access through the school with a lift. Stair and door widths would comply with Part M of Building Regulations.
- 6.43 Officers consider that the scheme provides for a good quality design in accordance with policy CS7.

### (v) Amenity Impacts

- 6.44 With regard to local planning policies the key guiding principles for amenity are set out in Policy DM4: Safeguarding Amenity. The matters identified in the Policy are discussed and assessed in turn below:

### *Sunlight and Daylight*

- 6.45 A Sunlight and Daylight Impact Assessment was submitted which reviews the expected access to natural light and the building's impact on the availability of natural light for neighbouring buildings. It concludes that the overall design and layout allows for good levels of natural daylight for the majority of teaching rooms;



that it will not affect the natural daylight of surrounding development; outdoor space with sufficient levels of direct sunlight and a passive design to use naturally occurring solar gains.

- 6.46 In summary out of the 15 windows assessed 13 of them would have sufficient access to natural light. The Nursery would experience the lowest levels of daylight and sunlight, due in part being opposite the Extra Care Building, but mostly as a result of the covered play space on the ground floor, which is an educational requirement. The proposed scheme has exceeded recommendations for both sunlight and daylight for a greater proportion of areas than those that do not.

*Privacy and Overlooking*

- 6.47 There would be some overlooking of the school from adjacent developments, in particular the upper floors of the Extra Care building, but this would be a minimum of 33 m away (front face to front face). The flat block to the north would be at ca 15m. These relationships are largely the same as approved at outline, and it is not considered that there would be significant issues with regard to privacy and overlooking.

*Visual Dominance*

- 6.48 The proposed scheme would be well within the height parameters as set at outline stage and would not be visually dominant, especially when viewed in the context of the extra care building opposite.

*Noise and Disturbance*

- 6.49 The national and local policy framework seeks to ensure that developments are not subject to noise which gives rise to significant adverse effects which would impact on health and quality of life. Adequate mitigation measures should be provided to minimise the impact of such pollution (para 123 NPPF, Core Strategy Policy CS34: Pollution and Water Resources).
- 6.50 A Noise and Vibration Report was submitted which concludes that based on future predicated noise levels from road traffic, i.e. once GPV and the Station are fully operational, that the levels are likely to be higher than would be acoustically acceptable for natural ventilation via openable windows. Therefore alternative means of ventilation may need to be considered. The Environmental Protection and Nuisance Officer recommends a condition controlling the noise level of plant.
- 6.51 The approved outline permission includes conditions regarding hours of use of the sports pitches and the community hall, to protect amenity of neighbouring residences, which remains relevant for this scheme.

*Artificial Lighting*

- 6.52 A section entitled 'Lighting Strategy' is included in the DAS. This refers to lighting design being produced in accordance with the Department for Education and Employment's Lighting Design for Schools. The types of lighting shown in the DAS appear to be a suitable approach, but it is recommended that a condition be included for the submission and approval of a detailed lighting scheme.

*Dust*

- 6.53 The Environmental Protection and Nuisance officer has recommended that a condition be included for a dust management plan to be submitted and approved.

*Crime and Safety*

- 6.54 National Planning Policy Framework 2012 (Part 7, Sect 58; 'Requiring good Design' and Part 8, Sect 69; Promoting Healthy Communities') where it is stated that development should create '*Safe and accessible environments where crime and*

*disorder, and the fear of crime do not undermine quality of life or community cohesion’.*

- 6.55 The design of the school has been led from meeting requirements of BB103, rather than specific secure by design guidance. However, TVP has confirmed that the overall design, layout, security appear to be an acceptable approach, but recommend a condition be attached that Secure by Design accreditation is achieved.
- 6.56 In conclusion on residential amenity matters, the original outline permission clearly found the location and use acceptable with surrounding residential development. Although the proposal is for increased activity on the site, the overall principle for the use, subject to suitable conditions, is still considered to be acceptable with respect to residential amenity issues.

(vi) Landscape & Open Space/Leisure

- 6.57 The landscape strategy has been based on principles set out in the Department for Education and Skills document ‘Schools for the Future - Designing School Grounds’: inspiration and variety, access, choice and versatility, security and safety, the natural environment and management and maintenance. The Strategy includes for a variety of external spaces; design of flexible play space to consider the sun’s movement; key social nodes with larger hard landscaped areas to provide adequate functional space and seating opportunities; a habitat garden, and tree planting for privacy.
- 6.58 Additional tree planting was requested at the front of the school to soften the appearance as well as suggestions as to amended tree species and further details of the hedge planting. Amended details were submitted, which the Natural Environment Officer has confirmed are acceptable. The overall principles of landscaping are, however, considered acceptable subject to further detail and recommended conditions for the submission and approval of hard and soft landscaping detail.
- 6.59 With regard to the provision of open space the key policy framework at the local level is set out in the Core Strategy, policies CS29 & CS30, supported by the Open Spaces Strategy (2007) and in the Sites and Detailed Policies Document, Policy DM16. Combined, these require that all new development should make provision for open space needs through on or off-site provision or through contributions.
- 6.60 The original S106 legal agreement for the one form entry included the requirement for *“a shared use sport facility to be located within the School site to include three sports pitches of approximately 300sqm each which facility may be used for sports purposes for the benefit of the community, which are appropriate in the residential area to include changing and lavatory facilities.”* The overall provision is for an all-weather 3G pitch of 38x55m (2090sqm), and a MUGA 32.5x21.5m (699sqm). This meets relevant education related requirements for school sites and would provide new sports, recreational and community facilities, which would serve Green Park Village Residents and the wider community in Reading. This accords with policy and is supported by Leisure. The use of the pitches would be defined within a community use agreement which is a recommended condition.
- 6.61 The landscaping scheme seeks to maximise opportunities on a constrained site and deliver a range of functions. The overall scheme is considered to accord with relevant policies CS7, CS29, CS38, DM16 and DM18.

(vii) Ecology

- 6.62 NPPF paragraph 118 requires local planning authorities to aim to conserve and enhance biodiversity when determining planning applications. The local policy (Core Strategy policy CS36) also requires development to retain, protect and incorporate features of biodiversity or geological interest found within sites.
- 6.63 The submitted Ecological Statement concludes that the original Phase 2b area contained habitats of low ecological value, although there were a small amount of woody vegetation and ditch line, which have since been lost or altered. The Statement identifies that measures to increase the ecological value of proposed habitats have been incorporated into the landscape design where possible. However, although the Landscape Strategy does include a potential habitat/garden area, which would have the potential to provide ecological habitats, there is no other mention of biodiversity enhancements.
- 6.64 A condition is recommended requiring further detail to be submitted and approved as part of a detailed landscape scheme.
- 6.65 The proposed scheme is considered acceptable and accords with policy CS36, subject to the above condition/s.

(xiii) Environmental Effects

*Air quality*

- 6.66 The site is not within an Air Quality Management Area and the submitted air quality Statement shows that the air quality as a result of the development will remain below air quality objectives.

*Contamination*

- 6.67 The previous use of the site and its potential for contamination was considered at the outline stage. The contaminated land statement discusses the remediation that has already been carried out on the site and that the school site is now considered to be low risk and no further remedial works are required.

*Flood Risk and Sustainable Drainage*

- 6.68 As advised by the EA Green Park was granted permission to raise levels of the land resulting in the site being in Flood Zone 1. The wider development can therefore go ahead without the need for further floodplain compensation measures. The EA has recommended a condition that the development is to be in accordance with the submitted Flood Risk Assessment.
- 6.69 A Surface Water Drainage Strategy forms part of the Flood Risk Assessment submission. This sets out that the drainage on the site would be designed to accommodate rainfall intensities for a 100yr +40% climate change storm event. The discharge of surface water is not expected to exceed that which was allowed under the original permission.
- 6.70 Conditions are recommended with respect to SUDS.
- 6.71 It is considered that the development proposals would comply with relevant standards for flood risk and sustainable drainage in accordance with Policy CS1, CS35 and DM1.

(ix) Sustainability

- 6.72 As part of the environmental strand of sustainable development the NPPF, supported by local policies CS1, CS2, SD1, DM1, and DM2 requires development to *“mitigate and adapt to climate change including moving to a low carbon economy.”* The submission information includes an Energy Statement and BREEAM Pre-Assessment Report.
- 6.73 Policy CS1 and supporting SPD requires a minimum of 62.5% BREEAM score to be achieved. The submitted report demonstrates that a score of 68.3% could be achieved. With regard to energy requirements, the measures identifies to be incorporated area: Energy efficient building fabric and insulation to all heat loss floors, walls and roofs; high efficient double glazing; high efficient heating and mechanical ventilation systems; led lighting; occupancy sensors to WCs, and heating and lighting sub-metering in line with BREEAM requirements.
- 6.74 Policy DM2 ‘Decentralised Energy’ states that any non-residential development of 100sqm or more shall consider the *“inclusion of a CHP plant, biomass-fuelled heating system or other form of decentralised heating provision within the site unless it can be demonstrated that the scheme is not suitable or feasible for this form of provision.”* A number of energy options were explored, but only solar PV panels were considered as feasible option for the site.
- 6.75 Together these measures would deliver a total reduction in CO2 emissions of 23.7% which would meet council requirements.

(x) Infrastructure Provision (Section 106 and Community Infrastructure Levy)

- 6.76 Policies CS9 and DM3 allow for securing the necessary contributions to ensure that the impacts of a scheme are properly mitigated. These are considered to meet the relevant legal tests as set out in the CIL regulations and would mitigate the effects of the scheme effectively.
- 6.77 Reading’s Economic Development Plan sets out the need to harness the unique opportunities provided by major developments in the South Reading Corridor. South Reading includes some of the more deprived wards in Reading, including with regard to the level of skills and employment. This proposed development would provide an opportunity to develop Employment, Skills and Training Plans which would benefit residents. An Employment, Skills and Training Plan for construction is a recommended obligation within the Section 106 agreement.
- 6.78 The proposal includes for parking bays to be utilised for drop-off and pick up as well as for school buses for trips. As these lie outside the red line application area of the school an obligation is recommended to specify specific hours of use for the school and shared servicing and delivery use, and that these are retained, for specific hours, for those uses.
- 6.79 An obligation is recommended regarding Travel Plan requirements based on Transport comments above.
- 6.80 The application site currently includes the current marketing suite building and associated parking spaces. It is proposed to retain the marketing suite until 2021 (when the temporary permission elapses), which would involve phased completion of the external space of the school to coincide with the phased filling of the school. The relevant phasing is included as a recommended obligation within the S106.

6.81 The overall proposal includes for the community use of the hall, external sports pitches and parking area. This is included as an obligation to ensure that the community use benefits are realised and retained.

(xi) Equality

6.82 In determining this application the Council is required to have regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sexual orientation. There is no indication or evidence (including from consultation on the current application) that the protected groups have or will have different needs, experiences, issues and priorities in relation to this particular planning application.

6.83 In terms of the key equalities protected characteristics it is considered there would be no significant adverse impacts as a result of the development.

## 7. CONCLUSION

7.1 The principle of development has been established by the outline planning consent and the proposed scheme would meet a specific need for education and community facilities in an area of Reading with deprivation.

7.2 The scheme would be in sustainable location, support the creation of community, and create jobs during the construction phase

7.3 There would be no significant environmental effects.

7.4 The principle of the proposals and the detailed aspects of the development would comply with the objectives of national planning policies and the adopted local policies.

Case Officer: Alison Amoah

# APPENDIX 1: PLANS

## Proposed Site Plan



## Proposed Elevations



1 Proposed South West Elevation - Market Square  
1:200



2 Proposed North West Elevation  
1:200



3 Proposed South East Elevation  
1:200



4 Proposed North East Elevation  
1:200



# Roof

